

Southeastern Pennsylvania Interstate System Resources and Constraints

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**Prepared by the Delaware Valley Regional Planning Commission
for the Pennsylvania Department of Transportation
and the Pennsylvania Turnpike Commission.**

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Created in 1965, the Delaware Valley Regional Planning Commission (DVRPC) is an interstate, intercounty and intercity agency which provides continuing, comprehensive and coordinated planning for the orderly growth and development of the Delaware Valley region. The region includes Bucks, Chester, Delaware, and Montgomery counties as well as the City of Philadelphia in Pennsylvania and Burlington, Camden, Gloucester, and Mercer counties in New Jersey. The Commission is an advisory agency which divides its planning and service functions among the Office of the Executive Director, the Office of Public Affairs, and three line Divisions: Transportation Planning, Regional Information Services Center which includes the Office of Regional Planning, and Finance and Administration. DVRPC's mission for the 1990s is to emphasize technical assistance and services and to conduct high priority studies for member state and local governments, while determining and meeting the needs of the private sector.



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DELAWARE VALLEY REGIONAL PLANNING COMMISSION

Publication Abstract

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Geographic Area Covered:

The five counties of Southeastern Pennsylvania, including Bucks, Chester, Delaware, Montgomery, and Philadelphia counties.

Key Words:

Interstate, Highway, Design Features, Land Use, Transportation Improvement Projects, Natural Features, Historic Sites, Cultural Features, Wetlands, Floodplains, Transit, Schuylkill Expressway, Pennsylvania Turnpike, Mid-County Expressway, "Blue Route," I-95, Vine Expressway.

ABSTRACT

In 1991, The Pennsylvania Department of Transportation undertook a project to create an operating plan for the entire interstate system in Southeastern Pennsylvania. This document was created to present all data concerning these roadways in a single source. The roads studied were the Schuylkill Expressway (I-76), the Pennsylvania Turnpike (I-76 and I-276), The Mid-County Expressway, popularly known as the "Blue Route" (I-476), Interstate Route 95 (I-95), and the Vine Expressway (I-676).

Five types of data were examined for each of the roadways studied. Included were design features, land use information, Transportation Improvement Program projects, natural, cultural, and historical features, and transit routes which use or intersect the highway. A series of base maps was developed for each Interstate, and all of the data is presented on these maps.

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Introduction

In 1991, the Pennsylvania Department of Transportation (PennDOT) undertook an initiative to develop an overall operating plan for the region's Interstate Highways (Figure 1). The plan, composed of a 14-component Transportation Information Management System (TIMS), will be the basis of a ten to fifteen year strategy for these roads. This Resources and Constraints document was created to assemble, within one source, all information pertaining to the Schuylkill Expressway (I-76), the Mid-County Expressway (I-476), the Pennsylvania Turnpike (I-76 and I-276), the Vine Expressway (I-676), and Interstate Route 95 (I-95).

The five roads comprising the Interstate system in southeastern Pennsylvania total just over 152 miles. Longest is the Pennsylvania Turnpike, at 57+ miles. It extends between the Chester County/Berks County border and the Turnpike Bridge over the Delaware River in Bristol Township, Bucks County. I-95, at slightly more than 50 miles, is the next longest. It enters the region in Delaware County at the Delaware state line and continues, somewhat parallel to the Delaware River, through the City of Philadelphia and Bucks County before crossing the Delaware River into New Jersey at the Scudders Falls Bridge, north of Trenton.

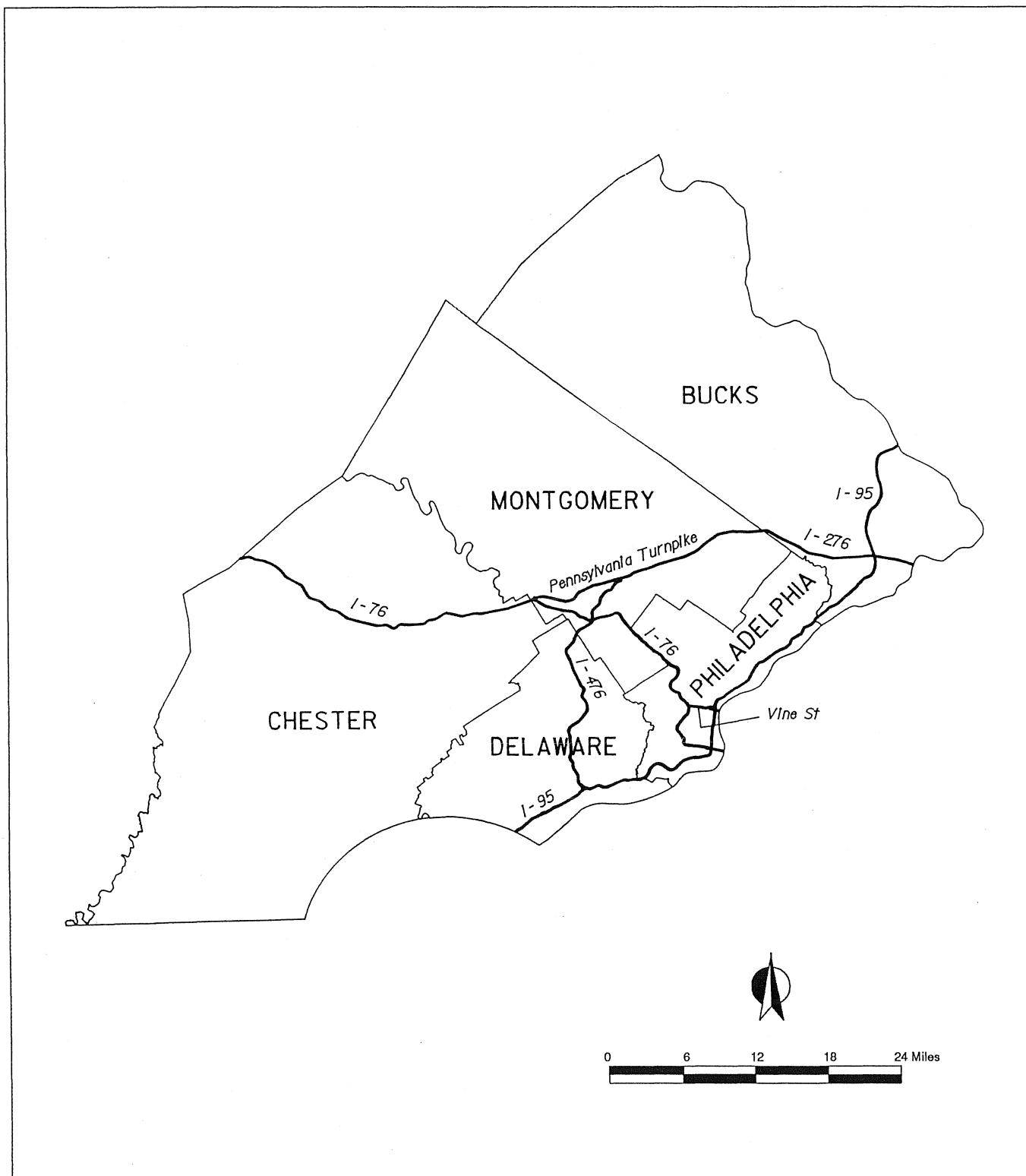
The Schuylkill Expressway and the Mid-County (popularly called "The Blue Route") are 20 and 21 miles long, respectively. The Schuylkill runs between the Turnpike in King of Prussia and the approach to the Walt Whitman Bridge Approach east of the Schuylkill River. Data on the bridge is included in the Schuylkill Expressway section for continuity, as it is also designated I-76. The Mid-County Expressway runs between the Northeast Extension of the Pennsylvania Turnpike (PA Route 9) in Plymouth Meeting, Montgomery County and I-95 in Ridley Township, Delaware County. The shortest road discussed here is the Vine Expressway. Opened to traffic in 1991, it covers almost two miles between the Schuylkill Expressway, just east of the Schuylkill River, and I-95, west of the Delaware River.

It should be noted that the Pennsylvania Turnpike is designated as I-76 west of the Valley Forge interchange in King of Prussia, and as I-276 east of this exit. Therefore, mileage calculations for each facility are different from mileage for each interstate route number. By route number, mileage is as follows:

Interstate Number	Interstate Name	Mileage
76	The Schuylkill Expressway The Pennsylvania Turnpike	20
		25
		Total: 45
95	Interstate 95	51
276	The Pennsylvania Turnpike	33
476	The Mid-County Expressway	21
676	The Vine Expressway	2

INTERSTATE HIGHWAYS IN THE REGION

Figure 1



Report Organization

The report is presented in five sections, one for each roadway. Each section is comprised of an index map, showing the location and sequence of the individual maps for that Interstate (Figures 2-6), followed by five sets of maps. Each set, printed on various colors of paper, focuses on different aspects which may influence improvement strategies. For each roadway, a set is included which details:

- Design Features (white pages),
- Adjacent Land Use (green pages),
- Planned Improvements (yellow pages),
- Natural and Cultural Environment (blue pages), and
- Transit and Traffic Data (ivory pages).

Facilities are documented from the point where they enter the Delaware Valley Regional Planning Commission (DVRPC) region in Pennsylvania, or from where they begin, to the point where they either terminate or exit the state. Each roadway is made up of a series of base maps as follows:

Interstate	Number of Base Maps
The Schuylkill Expressway (I-76)	9
The Mid-County Expressway (I-476)	7
The Pennsylvania Turnpike (I-76 & I-276)	19
The Vine Expressway (I-676)	1
Interstate 95 (I-95)	17

Maps show an area of just over one-half mile on either side of each Interstate, and represent approximately 2.8 miles of roadway. The scale of these maps is 1" = 2000', the same as the United States Geological Survey (USGS) and DVRPC 7 1/2 minute quadrangle maps, as well as the popular book atlases of the region published by Alexandria Drafting Company. There are five sets of maps for each Interstate, each of which uses the same base map. Ramps and interchanges are shown, along with all state highways, roads which cross the Interstate, and selected additional roads. Also included are freight railroads, passenger railroads and stations, large streams, important sites, and county and state boundaries.

Design Features Series

The design features series of maps show the as-built conditions of the individual interstates. The major characteristics referenced include:

- number of lanes in each direction
- median type
- span length and deck width of bridges
- distance between supports of overpassing structures
- general information regarding additional physical constraints which may affect planned improvements.

Adjacent Land Use Series

The adjacent land use series identify the uses of parcels of land immediately adjacent to the interstate highways in southeastern Pennsylvania. Standard land use categories are cited. Vacant land totaling ten or more contiguous acres is identified.

Planned Improvements Series

The planned improvement series shows the location of planned and programmed transportation improvements to the interstate system or to roadways immediately adjacent. Throughout the region, there are 22 projects which belong in this category. They are divided among the various interstate roadways as follows:

Interstate	Number of Projects
The Schuylkill Expressway (I-76)	2
The Mid-County Expressway (I-476)	6
The Pennsylvania Turnpike (I-76 & I-276)	1
The Vine Expressway (I-676)	no projects
Interstate 95 (I-95)	14

Project implementation schedules are given for all projects, including those already underway. Public transportation improvement projects are not included in this document.

Natural and Cultural Environment Series

The natural and cultural environment series shows significant natural and historic features in the vicinity of the interstate highways in southeastern Pennsylvania. The features include:

- location of historic features (source: DVRPC Inventory of Historic Sites)
- 100-year flood plains (source: National Flood Insurance Program)
- wetlands (source: National Wetlands Inventory (NWI) and DVRPC)



Due to the recent completion of the Mid-County Expressway, revised NWI data is not yet available for this area. Therefore, major new wetlands created during the construction of the Mid-County Expressway at its interchanges with US 1 and Baltimore Pike were included based on DVRPC's 1990 aerial photographs. Although there are other, smaller impacts on wetlands along the Mid-County, there is no anticipated impact on general NWI wetlands designations.

Transit and Traffic Data Series

Lastly, public transportation routes and Average Annual Daily Traffic (AADT) volumes are shown in the transit and traffic data series. Included are all bus and rail routes which travel along or intersect each interstate, as well as DVRPC's AADT for the section of roadway shown in each map.

Data Sources

Data shown in this report was gathered from a number of sources. PennDOT District 6-0's right-of-way, bridge, and design units provided structure, pavement marking, and right-of-way plans. The Pennsylvania Turnpike Commission's engineering department provided structure data for the Turnpike, and the Delaware River Port Authority provided structure information for the Walt Whitman Bridge Approach. Consultants involved with the construction of I-476 and I-676 also provided structural and wetlands information. In addition, DVRPC traffic counts, reports, Transportation Improvement Program, and intergraph maps were consulted for this report.

Right-of-Way Data

Research showed significant variations in right-of way area along each interstate. Each roadway's physical setting is unique, accounting for the inconsistencies. Recording this data is beyond the scope of this project. Should this data be required to supplement the information provided in this document, it is recommended that PennDOT plans and county tax maps be consulted for exact ROW dimensions at any specific site.

SECTION ONE

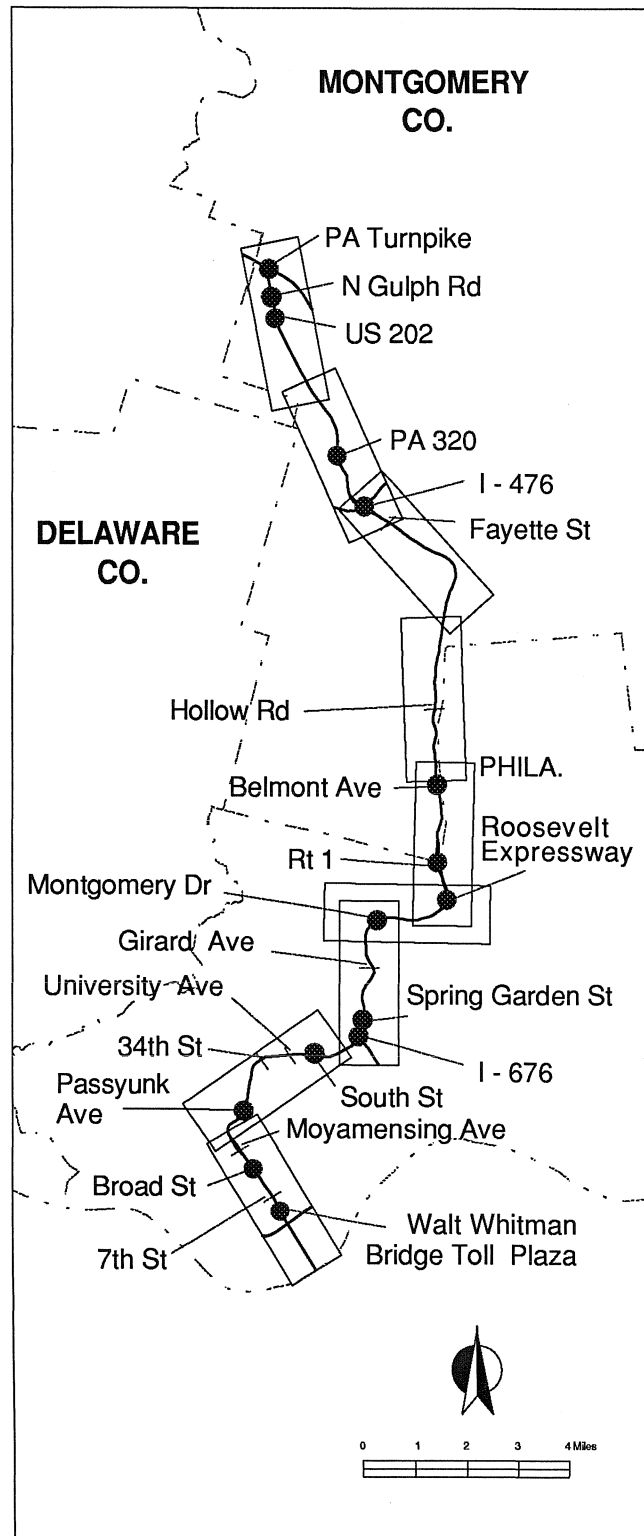


THE SCHUYLKILL EXPRESSWAY



THE SCHUYLKILL EXPRESSWAY

Figure 2



DESIGN FEATURES

These maps inventory current physical attributes, including physical characteristics and structural information, of the Schuylkill Expressway. The roadway itself is two travel lanes by direction west of the US 1 interchange, four lanes by direction between the US 1 interchange and the I-676 interchange, and two lanes by direction east of I-676. Travel lanes are 12' in width, with jersey barrier median and 10' shoulders for most of the length of the road. It should be noted that the road is geographically situated in such a way as to make widening cost-prohibitive.

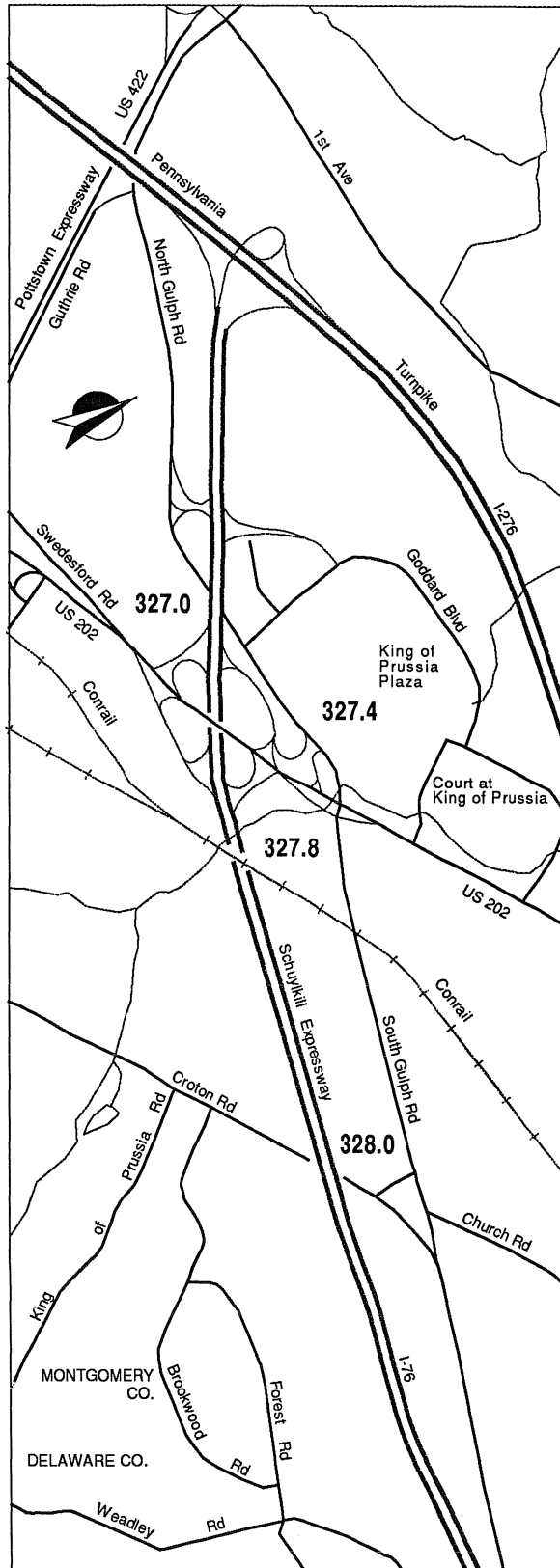
Segment/offset markers on this highway run west to east, and are shown on these maps accordingly. General information about the section of road shown in each map is located underneath the title block. Segment/offset numbers posted on the maps correspond to locations of structures which are described beside each map.

Specific information regarding structures is provided as necessary. Citations are made for either bridges or overpasses. For bridges, structure number, span length, and deck width are given, as well as information regarding the underneath facility. For structures overpassing the Schuylkill Expressway, the distance between piers is given in exact measurements (where available) or is listed according to the location of the piers. Structure number and description of facility carried are also given. Due to the interstate status of this highway, there are no sub-standard weight or clearance structures.

Information on these maps was gleaned from a combination of construction drawings, PennDOT Bridge Unit's structure lists, DVRPC reports, and field observations.



THE SCHUYLKILL EXPRESSWAY



Map 1

Design Features

Scale: 1" = 2000'

Note: Two 12' travel lanes by direction. Shoulder width 10' and variable. Variable ROW. Concrete Barrier median.

327.0 Bridge

Over North Gulph Road SR 3039 LR 201

S#: 15498A

Span Length: 162'

Deck Width: 12'-6"

327.4 Overpass

US 202 SR 202 LR 143

S#: 15503

Distance Between Piers: Full Width

327.8 Overpass

Conrail/PECO

S#: 1637

Distance Between Piers: Full Width

328 Bridge

Over Croton Road SR 3024 LR 46174

S#: 15500

Span Length: 70'-0"

Deck Width: 81'-6"





THE SCHUYLKILL EXPRESSWAY

Map 2

Design Features

Scale: 1" = 2000'

Note: Two EBD and two WBD 12' travel lanes. Shoulder width 10' and variable. Variable ROW. Concrete Barrier median. At I-476 Interchange, EBD and WBD mainlines are separated by a wide grass median.

329.0 Overpass
Weadley Road T-580
S#: 1642
Distance Between Piers:
Full Width

329.4 Overpass
Gypsy Road T-711
S#: 1643
Distance Between Piers:
Full Width

330.0 Bridge
Over S. Gulph Road
SR 3039 LR 201
Gulph Creek, SEPTA
Route 100 and Trinity Ln.
PA 320 SR 0320 LR 225
S#: 15501 S-1645
Span Length: 627'
Deck Width: 71.5'

331.0 Bridge
Matson Ford Road
SR 3016 LR 46140
S#: 15502
Span Length: 60'
Deck Width: 89'-6"

331.4 EBD Bridge
Ramps R (EBD I-76 to
NBD I-476)
and N (EBD I-76 to
PA 23 SR 0023 LR 46138)
S#: 8896
Span Length: 133'
Deck Width: 43.2'

331.4 WBD Bridge
Ramps R&N
S#: 8896
Span Length: 135'
Deck Width: 43.2'

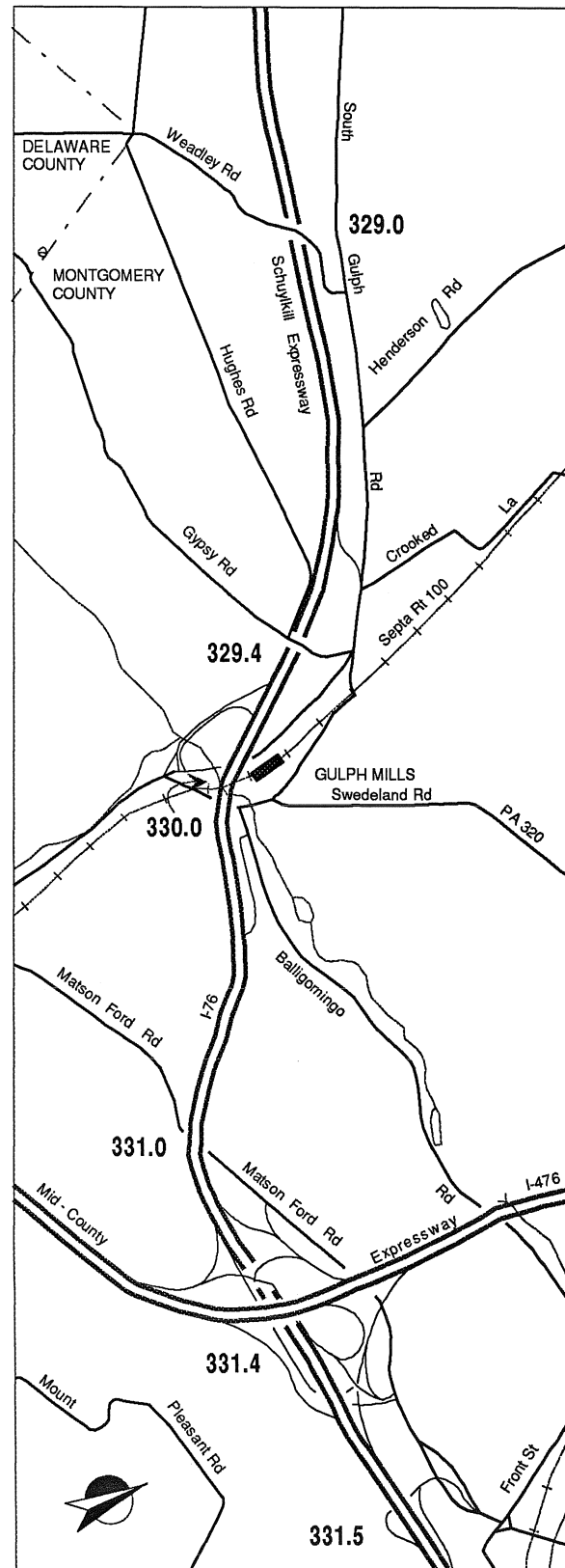
331.4 EBD Overpass
Ramp M (WBD I-76 to
SBD I-476)
S#: N/A
Distance Between Piers:
Full Width

331.4 Dual Overpass over
Dual Roadway I-476 NBD
& SBD Mainline over EBD
& WBD I-76
S#: N/A

Distance Between Piers:
Center Pier in wide grass
median

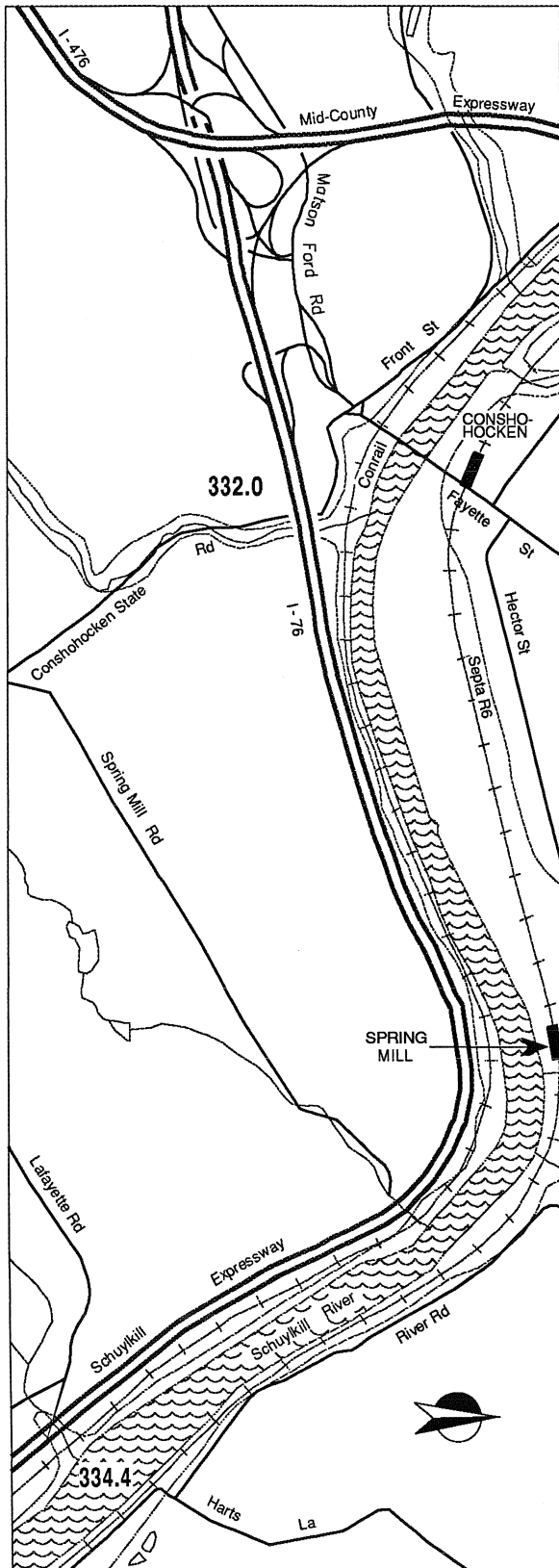
331.4 WBD Bridge
Over Ramp K (SBD I-476
to EBD I-76)
S#: N/A
Span Length:
Deck Width:

331.5 Bridge
Ramp D (I-476)
S#: 15596
Span Length: 46'
Deck Width: 75.5'





THE SCHUYLKILL EXPRESSWAY



Map 3

Design Features

Scale: 1" = 2000'

Note: Two EBD and two WBD 12' travel lanes. Shoulder width 10' and variable. Variable ROW. Concrete Barrier median. At I-476 Interchange, EBD and WBD mainlines are separated by a wide grass median. South side of road bounded by rock or steep hill. Northside bounded by Conrail ROW.

332 Overpass

Spring Garden Street (not shown)

S#: 1727

Distance Between Piers: Full width

332 Bridge

Over Conshohocken State Road PA 23

SR 0023 LR 4618

and Aronimink Creek.

S#: 15597

Span Length: 348'-0"

Deck Width: 71'-6"

334.4 Bridge

Over Waverly Road (not shown)

S#: S-1733 S-15598

Span Length: 118'

Deck Width: 79.8'





THE SCHUYLKILL EXPRESSWAY

Map 4

Design Features

Scale: 1" = 2000'

Note: Two EBD and two WBD 12' travel lanes. Shoulder width 10' and variable. Variable ROW. Concrete Barrier median. South side of road bounded by rock or steep hill. North side bounded by Conrail ROW.

335 Bridge

Over Mill Creek and Mill Creek Road

S#: 15599

Span Length: 329'-0"

Deck Width: 68'-0"

336 Bridge

Over Hollow Road SR 3050 LR 46201 and Creek

S#: 1794

Span Length: 245'

Deck Width: 75'-6"

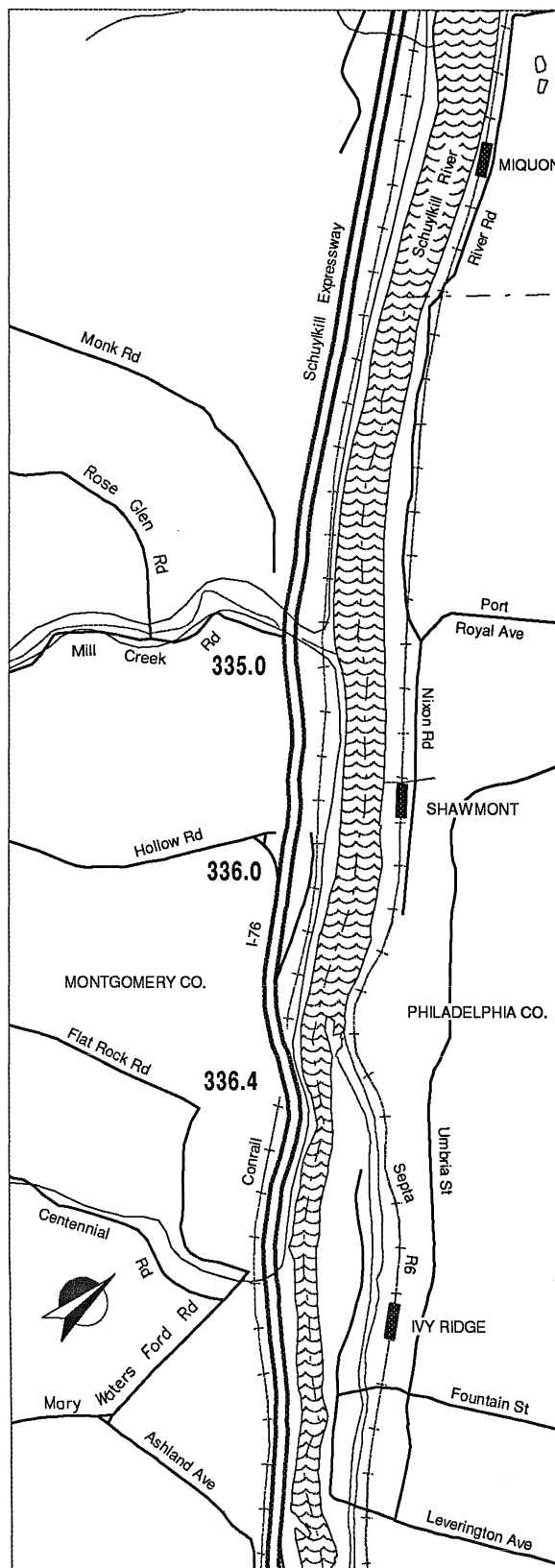
336.4 Bridge

Over Conrail

S#: 15742

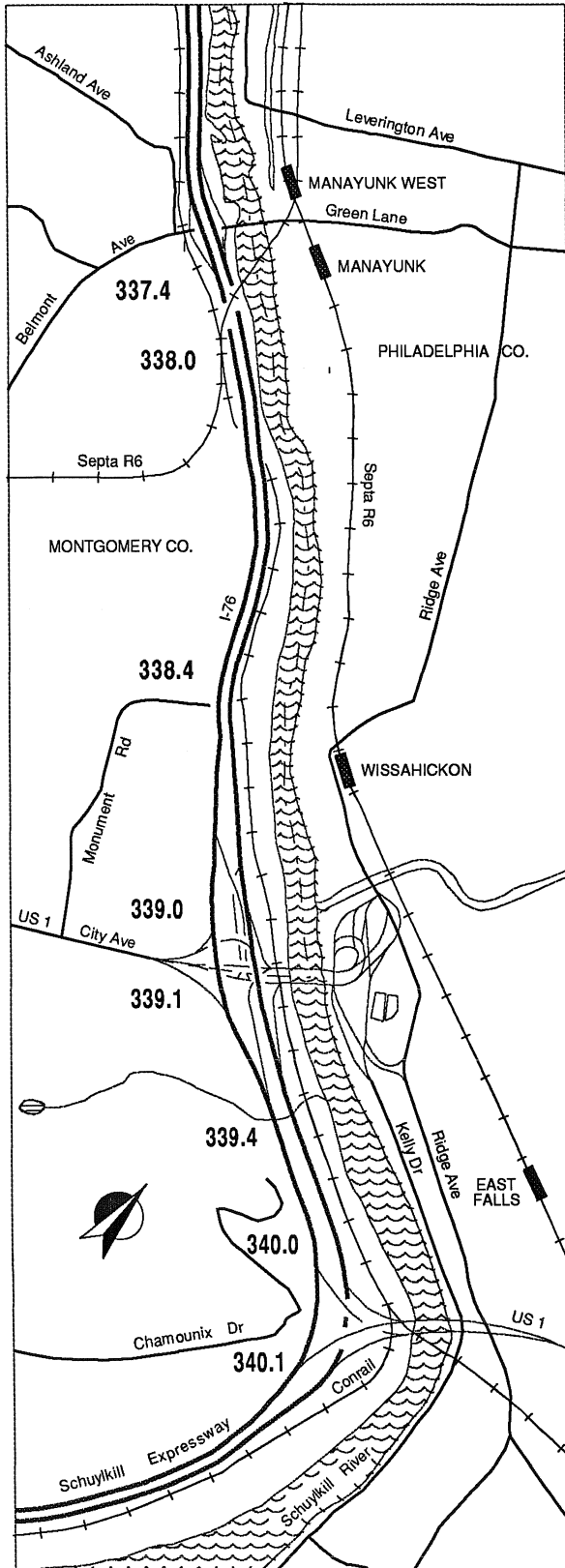
Span Length: 118'-0"

Deck Width: 75'-0"





THE SCHUYLKILL EXPRESSWAY



Map 5

Design Features

Scale: 1" = 2000'

Note: EBD and WBD, two 12' travel lanes. Shoulder width 10' and variable. Variable ROW. Concrete Barrier median. South side of road bounded by rock or steep hill. North side bounded by Conrail ROW. Dual bridges over City Line Avenue. Dual roadway throughout US 1 / I-76 interchange area. East of 40.1, four EBD and four WBD 12' lanes.

337.4 Bridge

Over Belmont Avenue
SR 3045 LR 46200
S#: 1796
Span Length: 156'-0"
Deck Width: 72.1'

338 Overpass

SEPTA R-6 S#: N/A
Distance Between Piers:
Center Pier

338 Bridge

Over Conrail (Penncoyd Viaduct) S#: 16093A
Span Length: 1490'-0"
Deck Width: 76'-0"

338.4 Bridge

Over Righter's Ferry Road
S#: S-1805 S-16094
Span Length: 235'
Deck Width: 76.5'

339 EBD Bridge

Over Ramp G (US 1)
S#: 16095
Span Length: 133'-0"
Deck Width: 42'-6"

339 EBD Bridge

Over City Line Avenue & Ramp C S#: 16096
Span Length: 299'-0"
Deck Width: 42'-0"

339.1 WBD Bridge

Over City Line Avenue & Ramp F S#: 16097
Span Length: 332'
Deck Width: 65'-6"

339.4 EBD Bridge

Over Neil Drive
S#: 1975 , 7585
Span Length: 223'-0"
Deck Width: 56'-0"

339.4 WBD Bridge

Over Neil Drive
S#: 7584, 1974
Span Length: 223'-0"
Deck Width: 54'

340 EBD Bridge

Over Ramp B (EBD I-76 - NBD US 1)
S#: 7592
Span Length: 162'-0"
Deck Width: 43'-0"

340.1 WBD Bridge

Over Ramp B (EBD I-76 to NBD US 1)
S#: 16091
Span Length: 76'-0"
Deck Width: 49'-0"

340.1 WBD Overpass

Ramp A (SBD US 1 to WBD I-76) S#: N/A
Distance Between Piers:
Full Width

340.1 WBD Overpass

Ramp D (SBD US 1 to EBD I-76) S#: N/A
Distance Between Piers:
Full Width





THE SCHUYLKILL EXPRESSWAY

Map 6

Design Features

Scale: 1" = 2000'

Note: Four EBD and four WBD 12' travel lanes Shoulder width 10' and variable. Variable ROW. Concrete barrier median. East of 341.4, three EBD and three WBD 12' travel lanes. North side bounded by Conrail.

340.4 Overpass

Greenland Drive (not shown)

S#: S-8764 S-1978

Distance Between Piers: Full Width

341.4 Bridge

Over Montgomery Drive

S#: 1979 S-15871R

Span Length: 144'

Deck Width: 95.5'

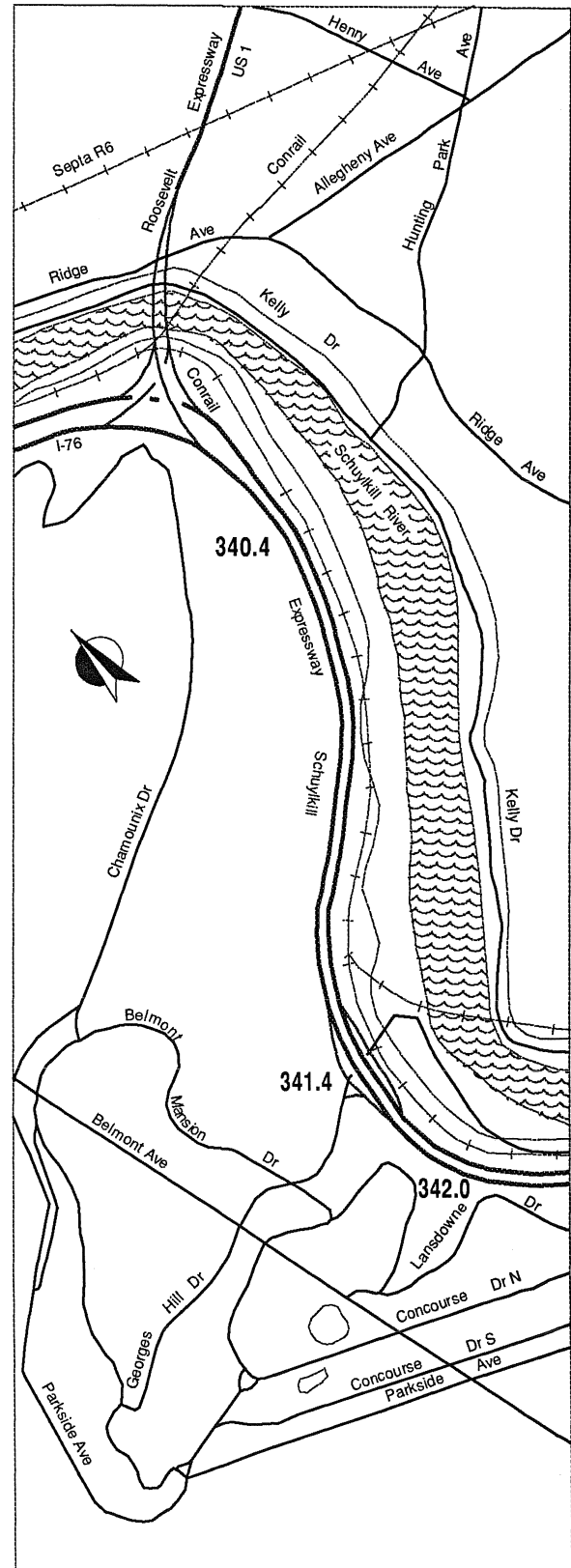
342.0 Bridge

Over Black Rock Road

S#: 1980 S-15872R

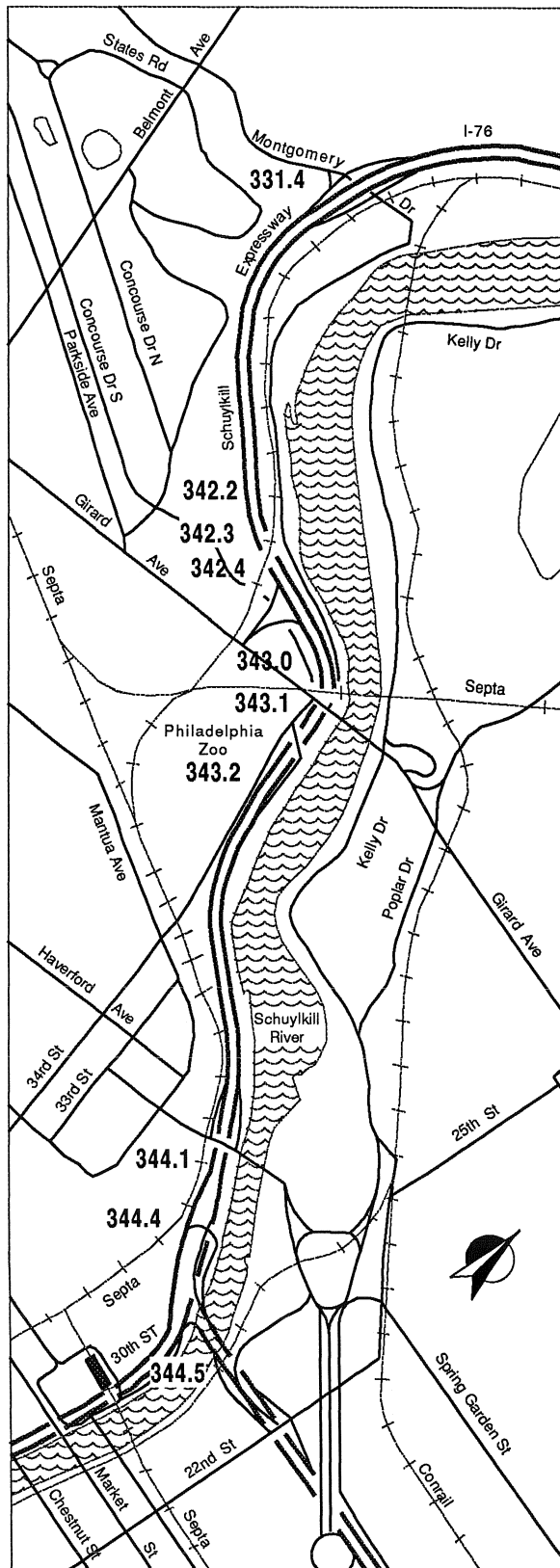
Span Length: 130'

Deck Width: 98.2'





THE SCHUYLKILL EXPRESSWAY



Map 7

Design Features

Scale: 1" = 2000'

Note: Three EBD and three WBD 12' travel lanes. Shoulder width 10' and variable. Variable ROW. Concrete barrier median. North side bounded by Conrail west of 342.2. East of 344.5 (I-676 interchange), two WBD and two WBD 12' travel lanes. Dual grade separated roadway. East of 344.6, main line is lower deck below Schuylkill Avenue in 30th Street Station area, with limited shoulder. One EBD pull-off area.

342.2 Overpass

Girard Avenue US 30
SR 0030 LR 67301
Ramp to WBD I-76
S#: N/A
Distance Between Piers:
Center Pier

342.3 Overpass

Conrail
S#: 2130
Distance Between Piers:
Center Pier

342.4 Bridge

Over Sweet Briar Cutoff
S#: 2135 S# 15878R
Span Length: 54'
Deck Width: 103.4'

343.0 Overpass

Amtrak NE Corridor
S#: N/A
Distance Between Piers:
Center Pier

343.1 Overpass

Ramp from WBD Expressway to 34th Street (Girard Avenue) S#: N/A
Distance Between Piers:
Center Pier

344.1 Overpass

Spring Garden Street
S#: N/A
Distance Between Piers:
Full Width

344.4 Bridge

Over Ramp from WBD Vine Street I-676 to EBD Expressway I-76
S#: 16147A
Span Length: 78'
Deck Width: 51.7'

344.4 Bridge

Over Ramp from WBD Vine Street (I-676) to WBD Expwy. (I-76)
S#: 16146R
Span Length: 46'
Deck Width: 41.6"

344.5 Overpass

Schuylkill Avenue
S#: N/A

Note: Overpass continuous between I-76 at Arch Street and I-76 at Walnut Street
Distance Between Piers:





THE SCHUYLKILL EXPRESSWAY

Map 8

Design Features

Scale: 1" = 2000'

Note: Two EBD and two WBD 12' travel lanes. Shoulder width 10' and variable. Variable ROW. Concrete Barrier median. Over-deck (Schuylkill Avenue) west of Walnut Street. Dual roadway in vicinity of South Street interchange. Three EBD and three WBD 12' travel lanes east of 34th Street. East of 26th Street I-76 is owned and maintained by DRPA. East of Passyunk Avenue, two EBD and two WBD 12' travel lanes.

345.X Overpass

South Street

S#: N/A

Distance Between Piers:
Center Per

345.4 Bridge

Over Schuylkill River and
CSX R.R.

S#: 11167

Span Length: 1641'-0"

Deck Width: 66'-0"

345.4 Bridge

Over Conrail

S#: N/A

Span Length: 73'-0"

Deck Width:

345.4 Bridge

Over Grays Ferry Avenue to
34th Street

S#: 14940

Span Length: 1630'-0"

Deck Width: 64.8'

XXX.X WBD Bridge

Over Schuylkill River bet-
ween 34th Street and Walnut

Street S#: N/A

Span Length: N/A

Deck Width:

W8 EBD Bridge

Over Passyunk Avenue

SR 3019 LR 67310

Span Length: N/A

Deck Width: 30'

W9 WBD Bridge

Over Passyunk Avenue

Span Length: N/A

Deck Width: 30'

Overpass

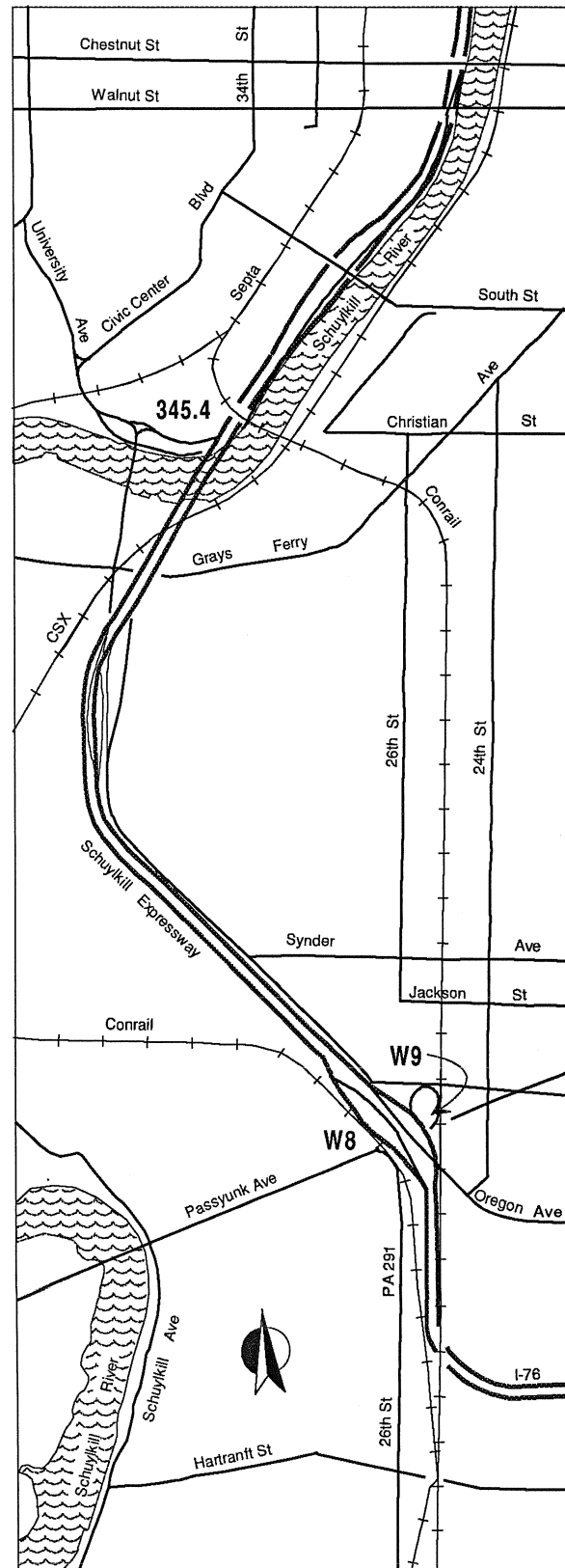
Conrail

E of 26th Street

S#: N/A

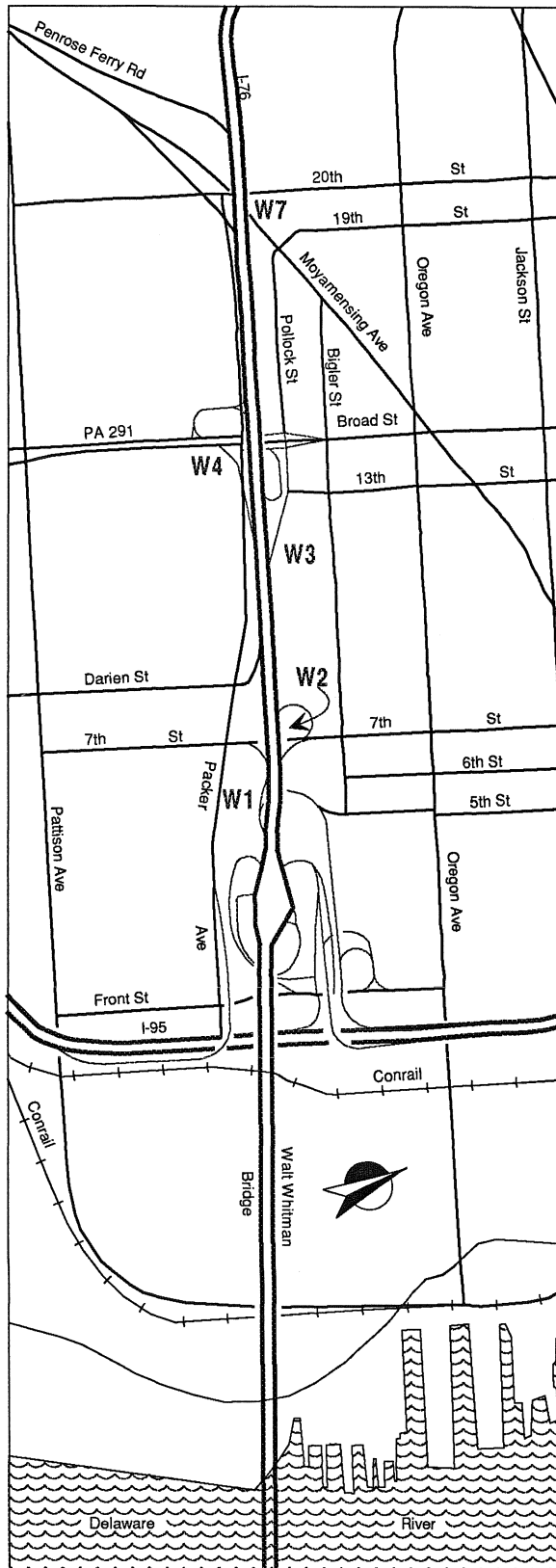
Distance Between Piers:

Full Width





THE SCHUYLKILL EXPRESSWAY



Map 9

Design Features

Scale: 1" = 2000'

Note: Two EBD and two WBD 12' travel lanes. Shoulder width 10' and variable. Variable ROW. Concrete Barrier median. Dual roadway at the Moyamensing Avenue interchange. East of Moyamensing Avenue and west of Walt Whitman Bridge toll plaza, three EBD and three WBD 12' travel lanes.

W7 Bridge

Over Moyamensing Avenue

Span Length: N/A

Deck Width: 28'

W4 Bridge

Over Broad Street US 611 SR 3001 LR 67373

Deck Width: 98'

W3 Bridge

Over 10th Street (not shown)

Deck Width: 116'

W2 Bridge

Over 7th Street

Deck Width: 122'

W1 Bridge

Over Randolph Street

Deck Width: Varies 48' +/-



Print
on
green

ADJACENT LAND USE

The adjacent land use maps identify the land use on parcels immediately adjacent to the Schuylkill Expressway's right of way. The purpose of this inventory is to highlight land use considerations which may impact, positively or negatively, upon large-scale roadway modifications.

Nine different land use categories are used to designate adjacent land uses. These categories are general in nature so as to facilitate their use, but sufficiently specific to allow for a meaningful designation of land use. The nine categories are:

- Agricultural
- Cemetery
- Commercial/Industrial
- Institutional
- Parkland
- Residential
- Transportation
- Vacant (parcels over 10 acres identified as such)
- Water

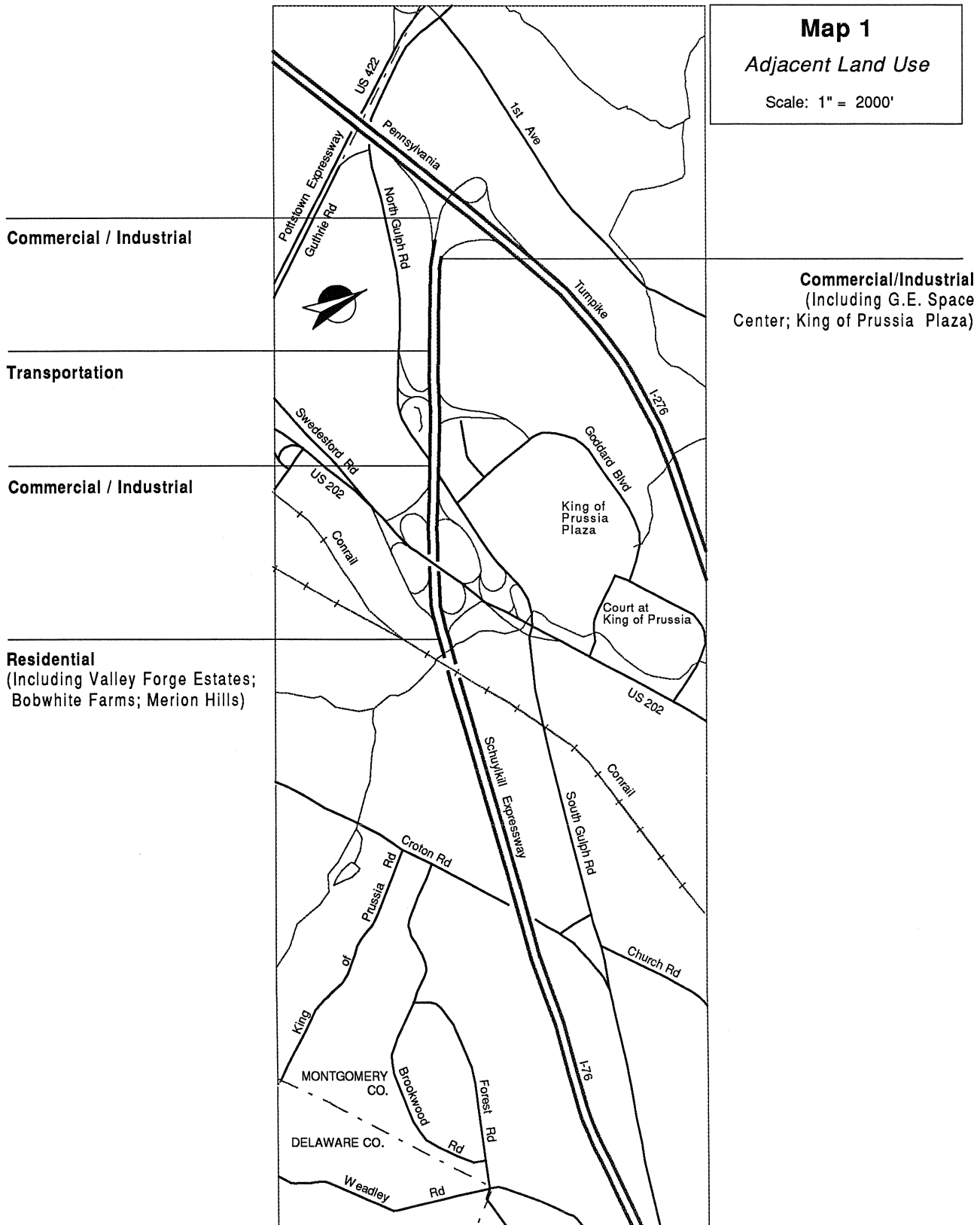
Adjacent land uses are identified on the map along both sides of the highway. The land use noted at the top of each map extends toward the bottom of the page until a different land use is cited under a horizontal line.

Special conditions and anecdotal information that may provide additional insight into the nature of adjacent parcels is provided where appropriate. The presence of special structures, including the proper names of identifiable places located adjacent to the roadway is also noted.

Information presented on this map was assembled from DVRPC in-house data, including aerial photographs, and field views.



THE SCHUYLKILL EXPRESSWAY





THE SCHUYLKILL EXPRESSWAY

Map 2

Adjacent Land Use

Scale: 1" = 2000'

Residential

Vacant

Residential

Vacant

Residential

Cemetery
(Calvary)

Vacant

Commercial / Industrial

Residential

Vacant

Transportation

Vacant

Residential

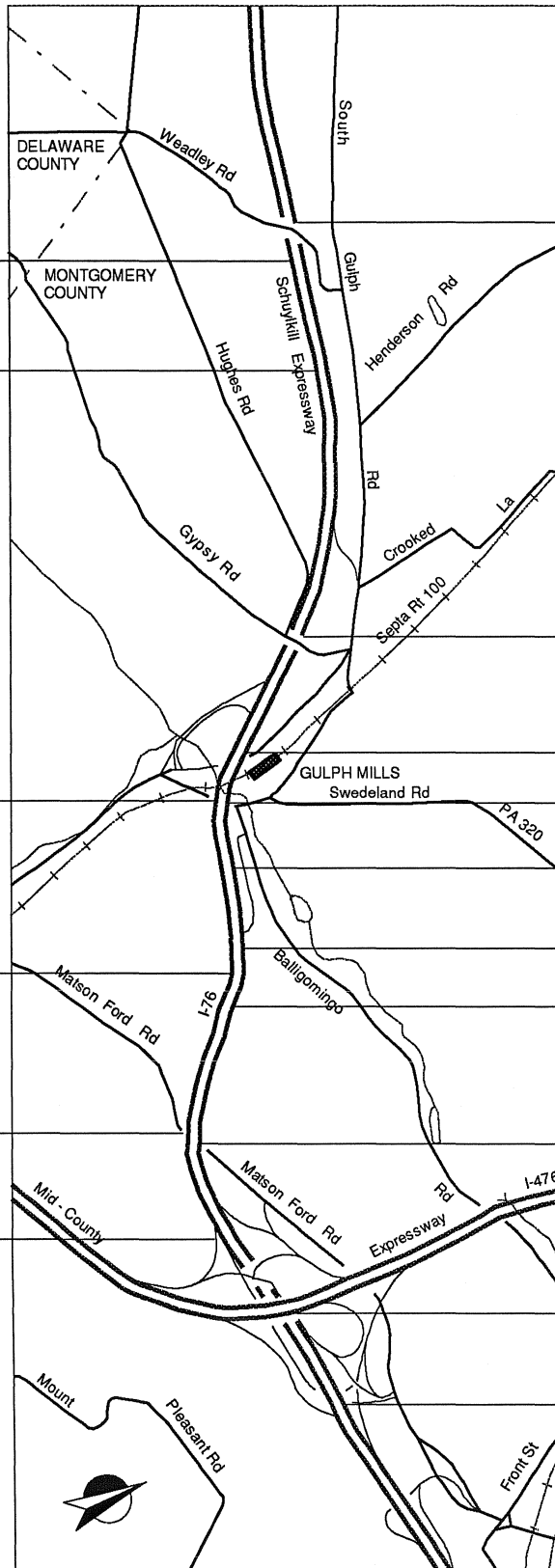
Vacant

Residential

Transportation
(Commercial / industrial
north of Matsonford Road)

Transportation
(Residential north of
Matsonford Road)

Residential





THE SCHUYLKILL EXPRESSWAY

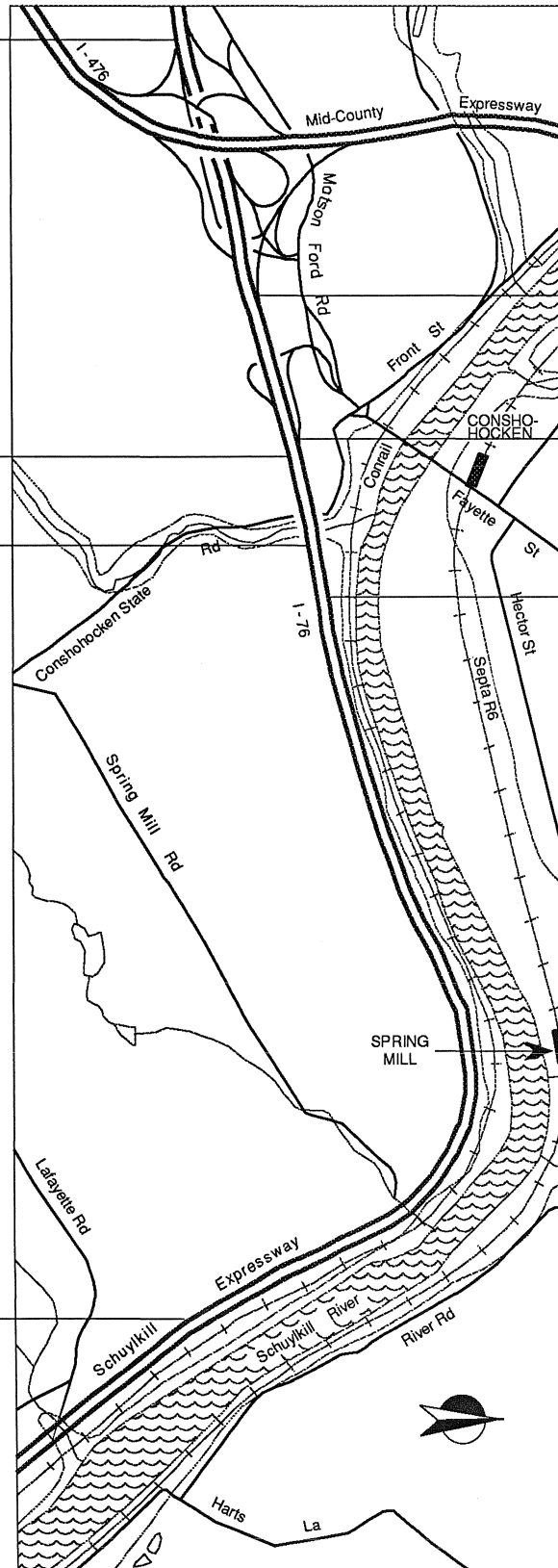
Cemetery

Vacant

Commercial / Industrial

Vacant

Residential



Map 3

Adjacent Land Use

Scale: 1" = 2000'

Transportation
(Residential north of
Matson ford Road)

Residential

Commercial / Industrial

Transportation
(Conrail; Schuylkill River
adjacent to railroad tracks)





THE SCHUYLKILL EXPRESSWAY

Map 4
Adjacent Land Use
Scale: 1" = 2000'

Residential

Vacant

Residential

Vacant

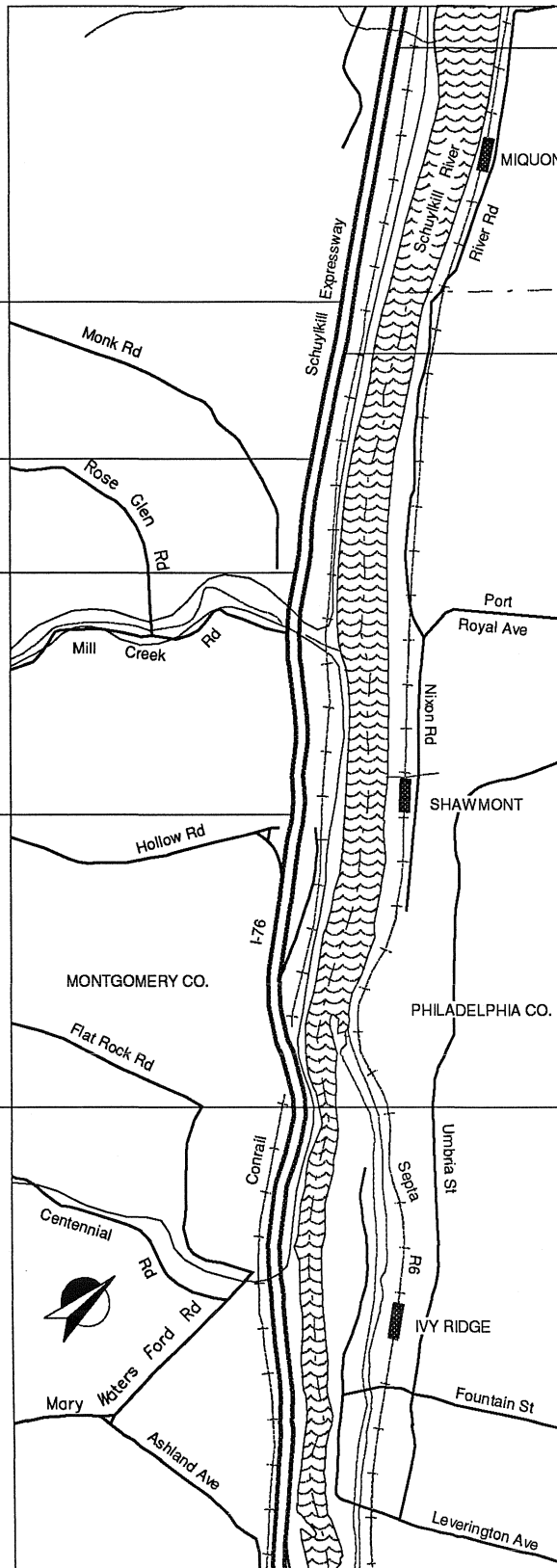
Residential

Transportation
(Conrail; residential adjacent to railroad tracks; transmission towers adjacent to expressway)

Transportation
Commercial / Industrial

Transportation
(Conrail; River Road)

Water
(Schuylkill River)





THE SCHUYLKILL EXPRESSWAY

Transportation
(Residential adjacent
to railroad tracks)

Transportation
(Westminster Cemetery
adjacent to railroad tracks)

Cemetery
(West Laurel Hill)

Commercial / Industrial

Residential

Parkland
(Fairmount Park)

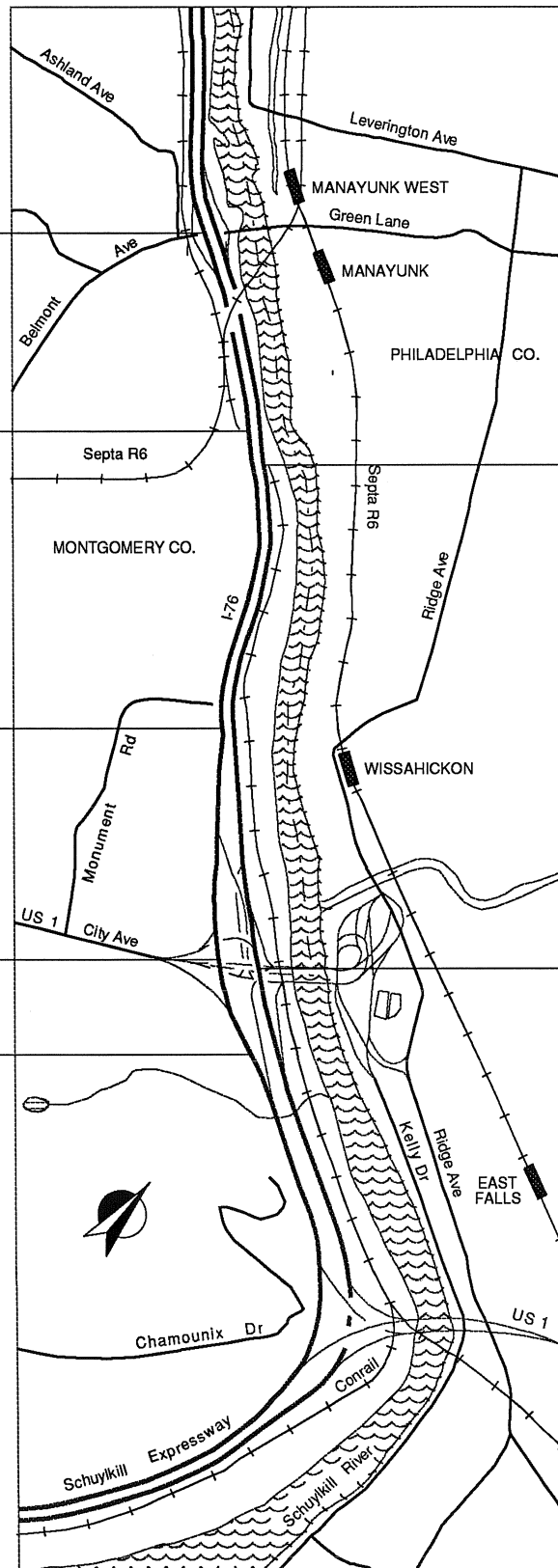
Map 5
Adjacent Land Use

Scale: 1" = 2000'

Water
(Schuylkill River)

Transportation
(Conrail)

Parkland
(Fairmount Park)





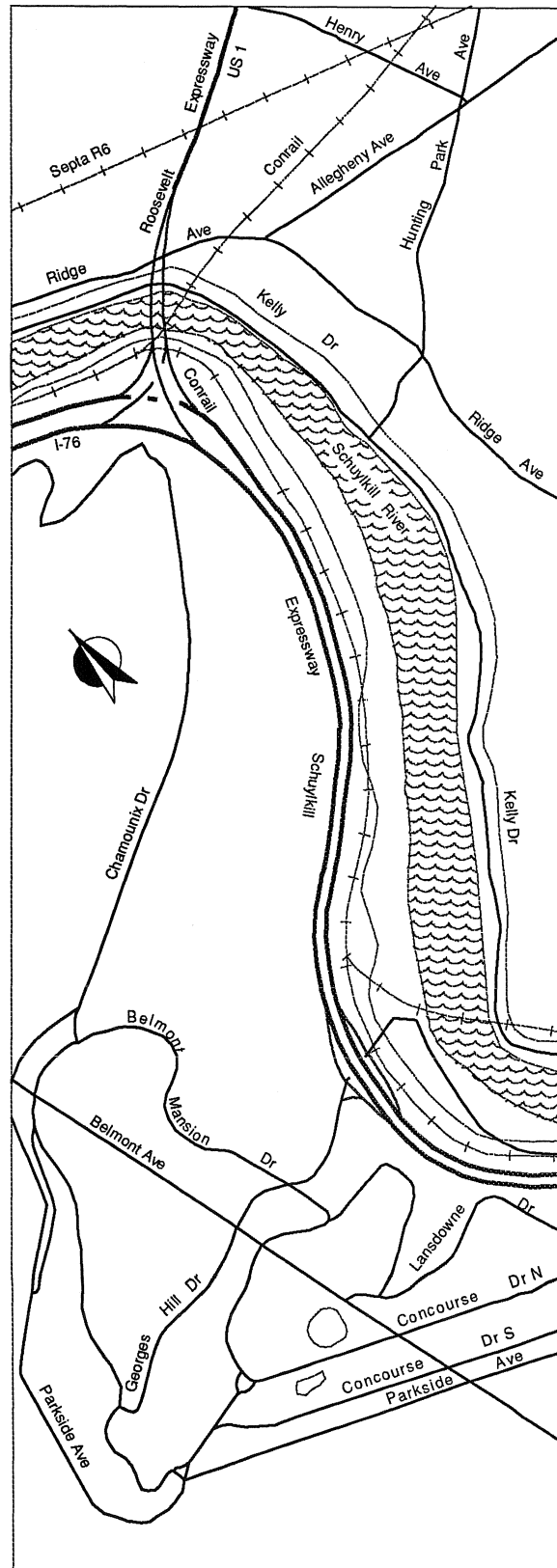
THE SCHUYLKILL EXPRESSWAY

Map 6

Adjacent Land Use

Scale: 1" = 2000'

Parkland
(Fairmount Park)



Parkland
(Fairmount Park; also,
Conrail adjacent to
expressway)





THE SCHUYLKILL EXPRESSWAY

Parkland
(Fairmount Park)

Parkland
(Philadelphia Zoo)

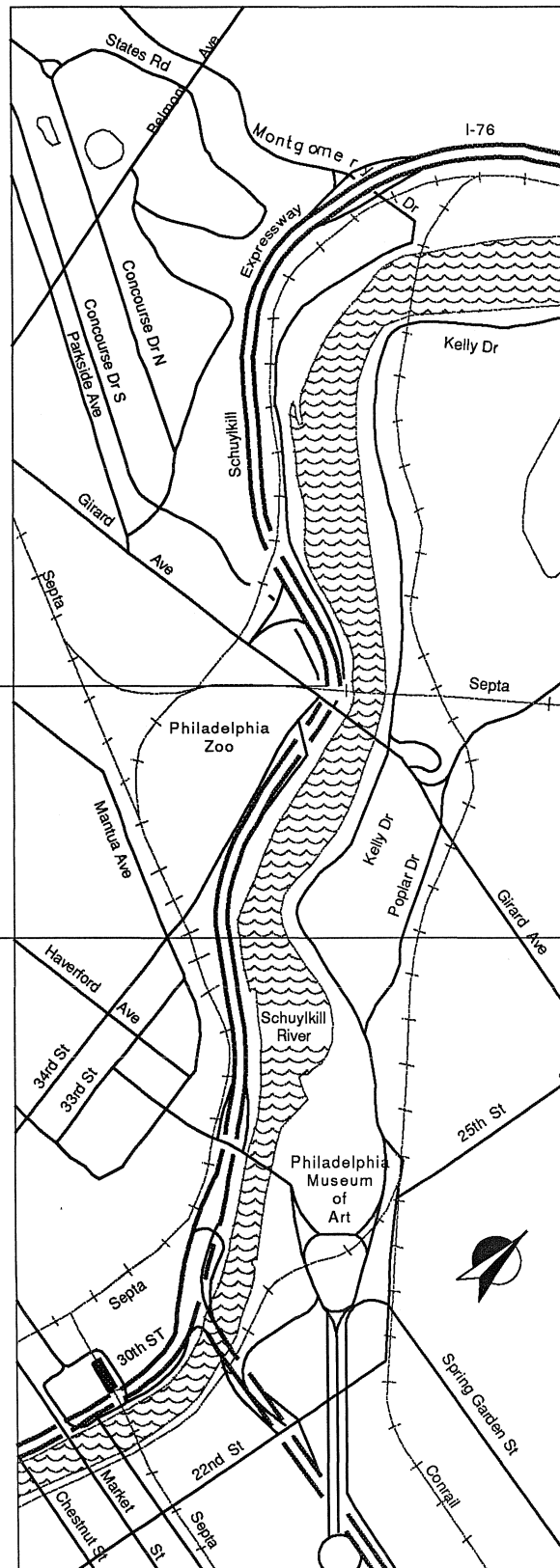
Transportation
(Railroad tracks; 30th Street Station)

Map 7
Adjacent Land Use

Scale: 1" = 2000'

Parkland
(Fairmount Park)

Water





THE SCHUYLKILL EXPRESSWAY

Map 8

Adjacent Land Use

Scale: 1" = 2000'

Transportation

Institutional
(University of Pennsylvania)

Water

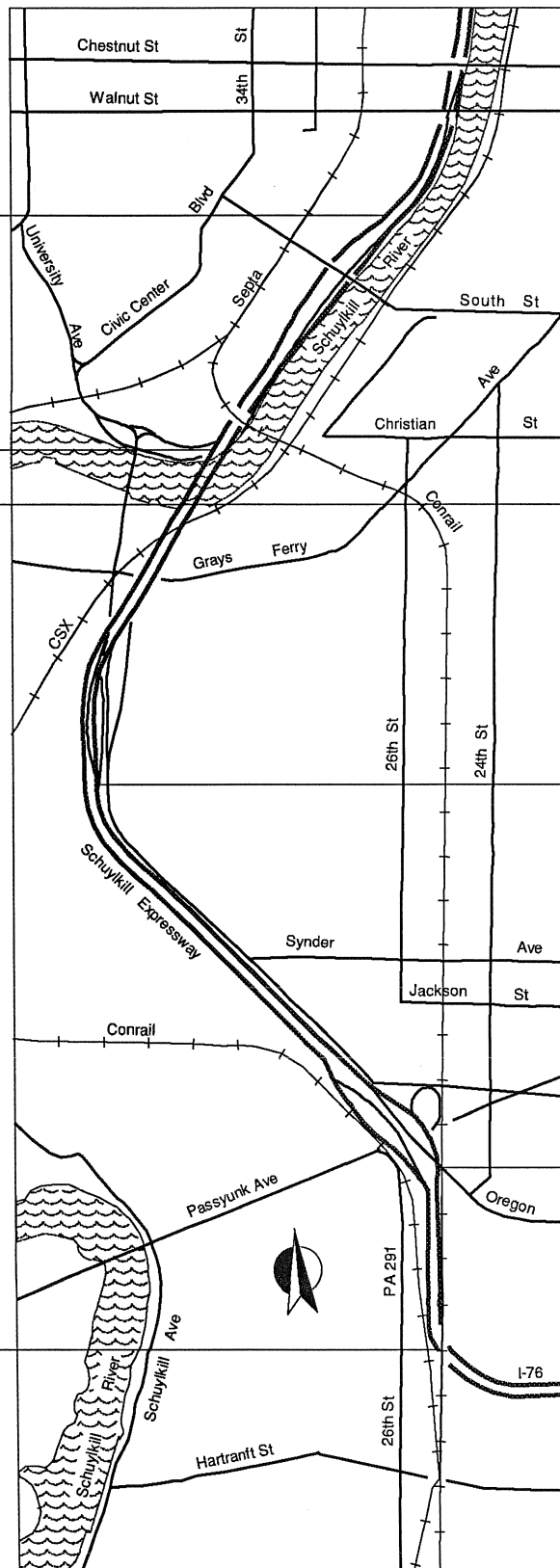
Commercial / Industrial

Commercial / Industrial

Residential
(Including Tasker
Homes; Girard Park)

Institutional
(United States Defense
Personnel Support
Center)

Residential
(Passyunk Homes)





THE SCHUYLKILL EXPRESSWAY

Residential

Transportation
(Packer Avenue; residential adjacent to Packer Avenue)

Transportation
(Veterans Stadium adjacent to Packer Avenue)

Transportation
(Commercial/industrial adjacent to Packer Avenue, including Food Distribution Center)

Commercial / Industrial

Commercial / Industrial
(Packer Marine Terminal)

Water



Map 9
Adjacent Land Use

Scale: 1" = 2000'

Institutional

Commercial / Industrial

Residential

Institutional
(Stella Maris School; ball fields adjacent to expressway)

Residential

Commercial / Industrial

Water



Print

on

yellow

PLANNED IMPROVEMENTS

These maps summarize transportation improvements to the Schuylkill Expressway corridor which are either underway or programmed. Relevant improvements are shown for parallel and intersecting roadways. Projects which are not yet funded are not included in this listing.

Each project can be located on the map by referencing its TIP number, which is placed as closely as possible to actual location. When a project entails a corridor, it is referenced at one point only.

Two official program numbers, the Transportation Improvement Program (TIP; source: DVRPC) number and the Program Management System (PMS; source: Penn DOT) number are provided for reference purposes. In the few cases where a TIP number is not specified, federal funding is not involved and the project is therefore not on the TIP. Cost and estimated let and completion dates are subject to change.

Each project is catalogued according to the following format:

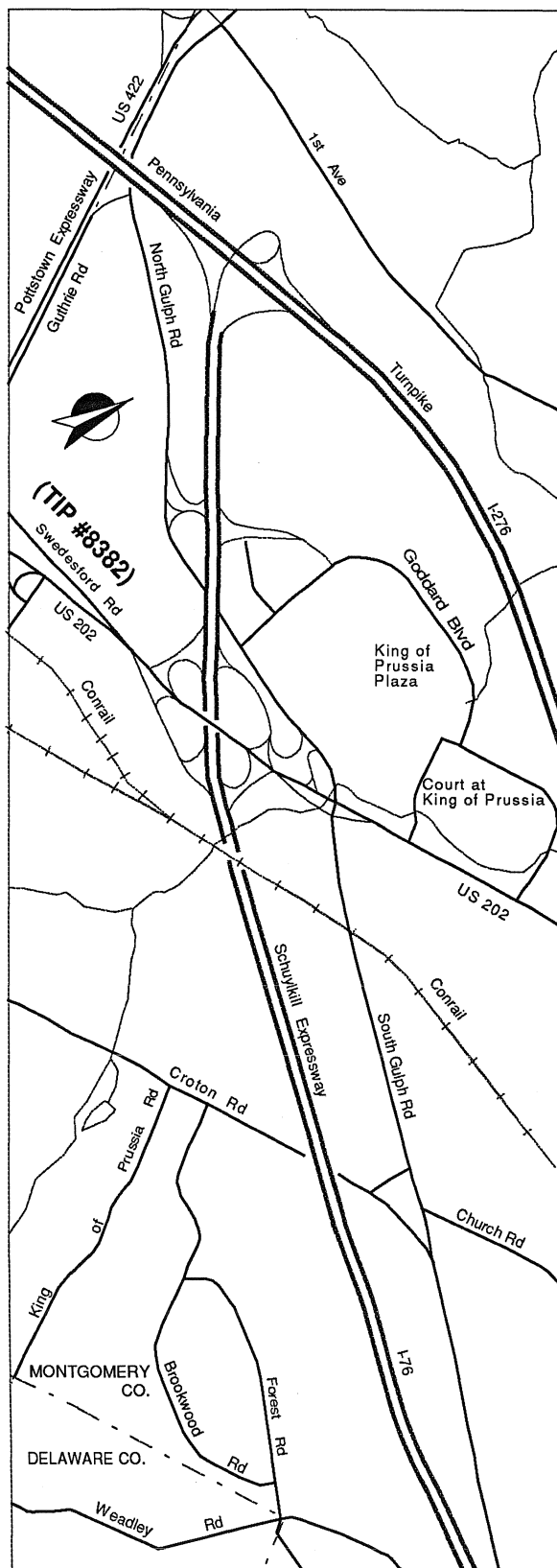
- Project Description
- Project Location
- Limits of Project (if necessary)
- Special Comments (related projects, funding, etc.)
- Transportation Improvement Program and Program Management System Numbers
- Current Estimated Cost
- Actual or Estimated Let Date
- Estimated Completion Date

The Schuylkill Expressway was completely reconstructed between 1983 and 1987, and there are therefore no improvement projects planned or underway. There are, however, two projects on adjacent roadways. Descriptions and locations of these projects can be found on Maps 1 and 7.

Information was gathered for this section from DVRPC's Transportation Improvement Program, and the PennDOT 12-year plan.



THE SCHUYLKILL EXPRESSWAY



Map 1

Planned Improvements

Scale: 1" = 2000'

Widening to 6 Lanes (72 Feet)

US 202

I-76 to PA 252 (Valley Forge Road)

TIP # 8382 PMS # 064C202

\$86,782,000

Let date: Mid 1995

Estimated Completion: 12/96





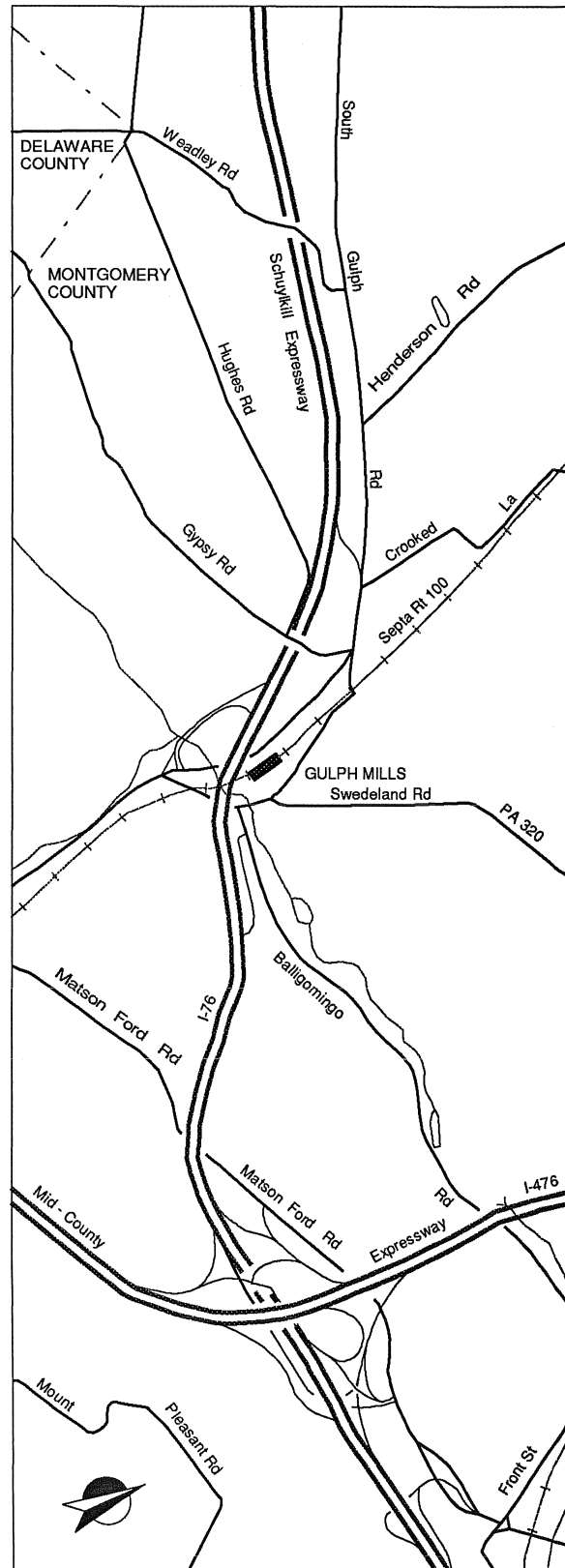
THE SCHUYLKILL EXPRESSWAY

Map 2

Planned Improvements

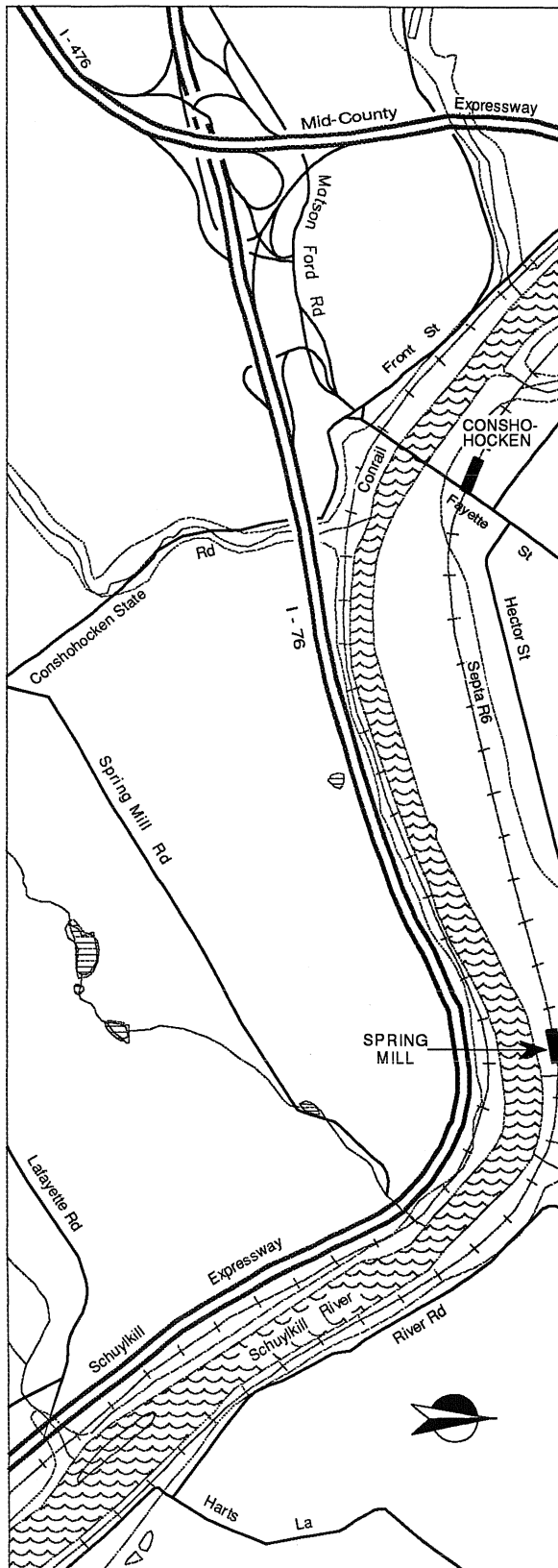
Scale: 1" = 2000'

NO PROJECTS





THE SCHUYLKILL EXPRESSWAY



Map 3

Planned Improvements

Scale: 1" = 2000'

NO PROJECTS





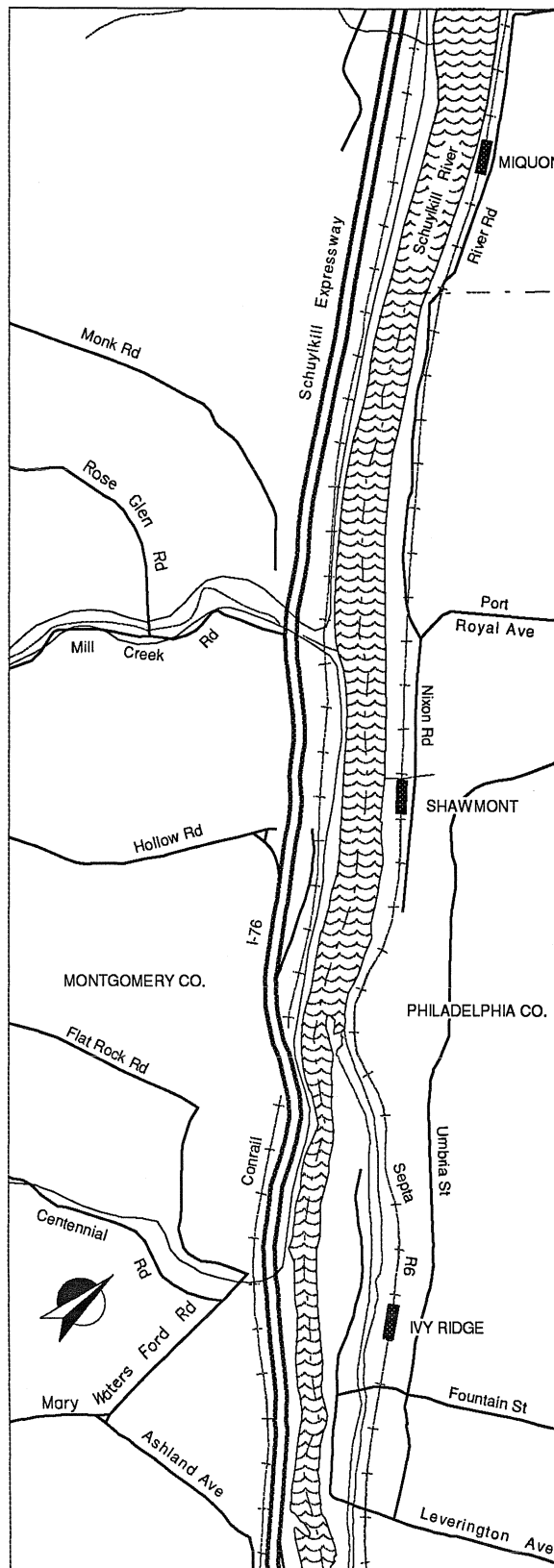
THE SCHUYLKILL EXPRESSWAY

Map 4

Planned Improvements

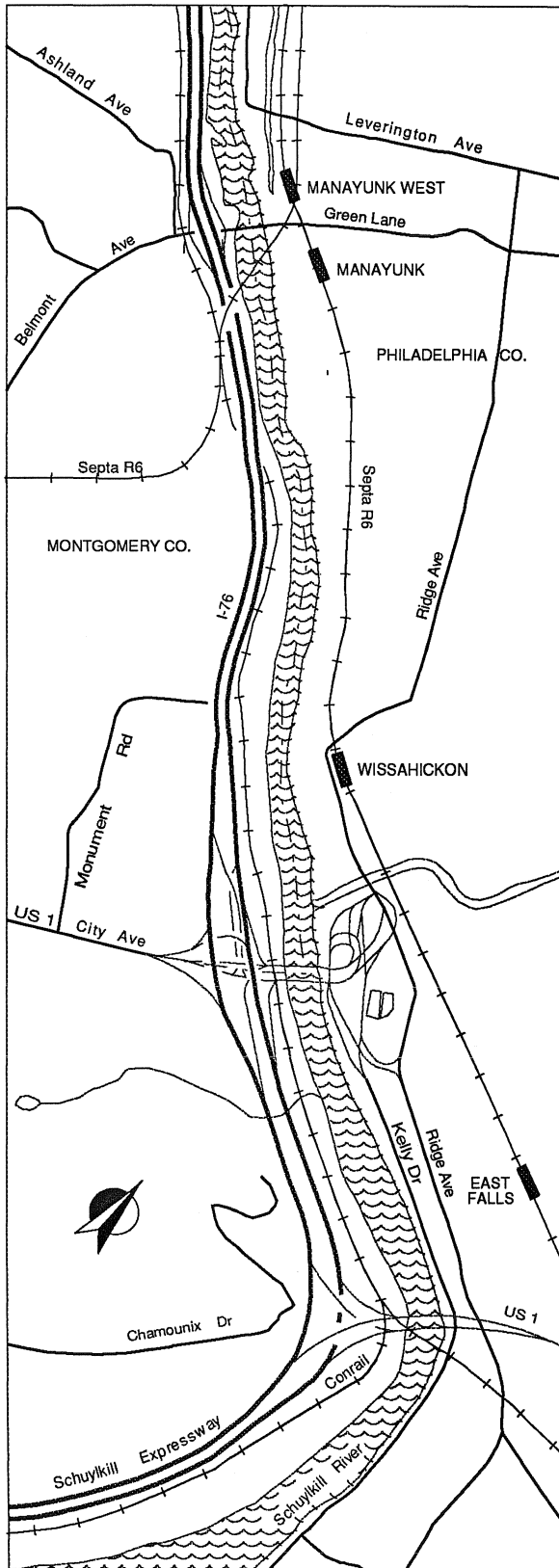
Scale: 1" = 2000'

NO PROJECTS





THE SCHUYLKILL EXPRESSWAY



Map 5

Planned Improvements

Scale: 1" = 2000'

NO PROJECTS





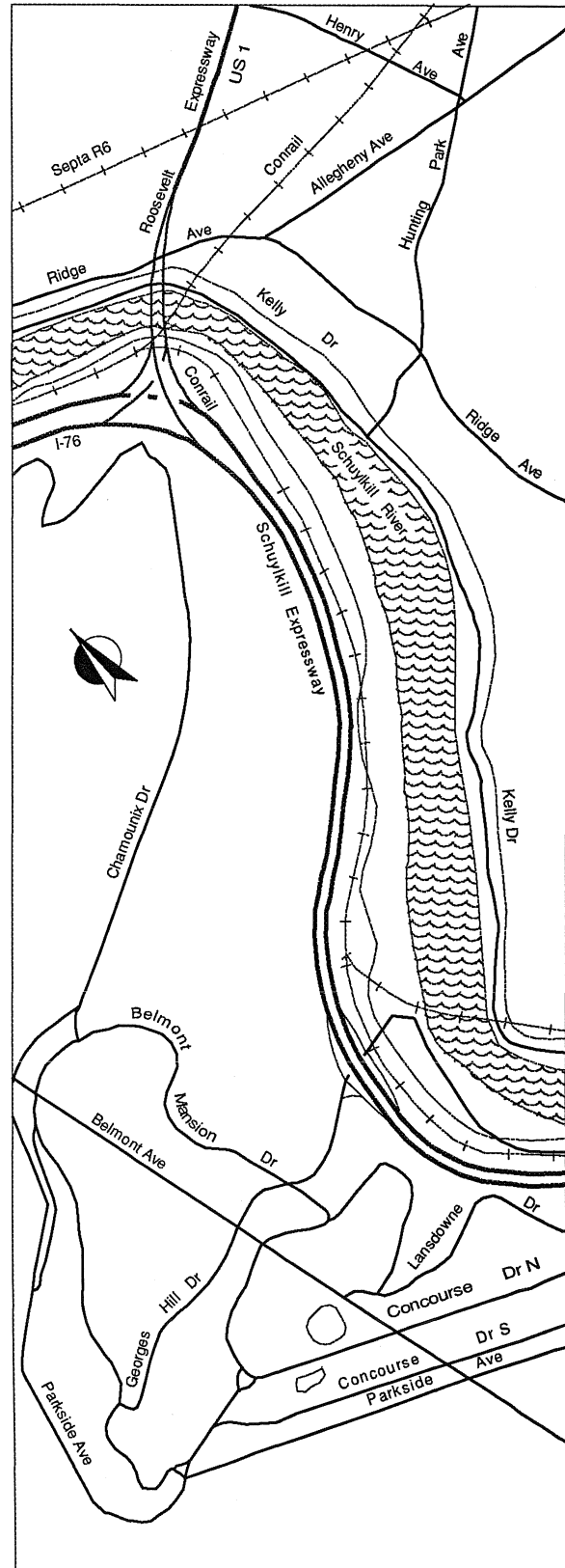
THE SCHUYLKILL EXPRESSWAY

Map 6

Planned Improvements

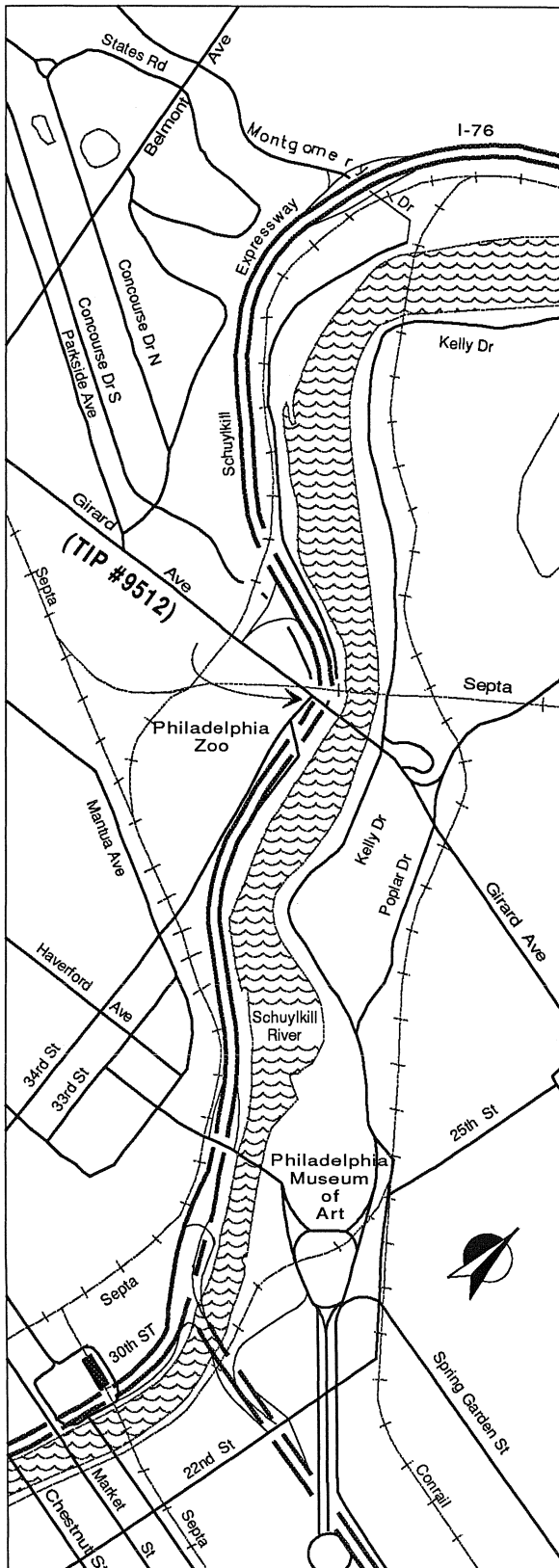
Scale: 1" = 2000'

NO PROJECTS





THE SCHUYLKILL EXPRESSWAY



Map 7

Planned Improvements

Scale: 1" = 2000'

Bridge Deck Replacement and Minor Rehabilitation

US 13/30, Girard Avenue

Over I-76 (Schuylkill Expressway) West

Philadelphia

TIP # 9512 PMS # 065M2018

\$1,734,000

Let date: Early 1992

Estimated completion: 12/93





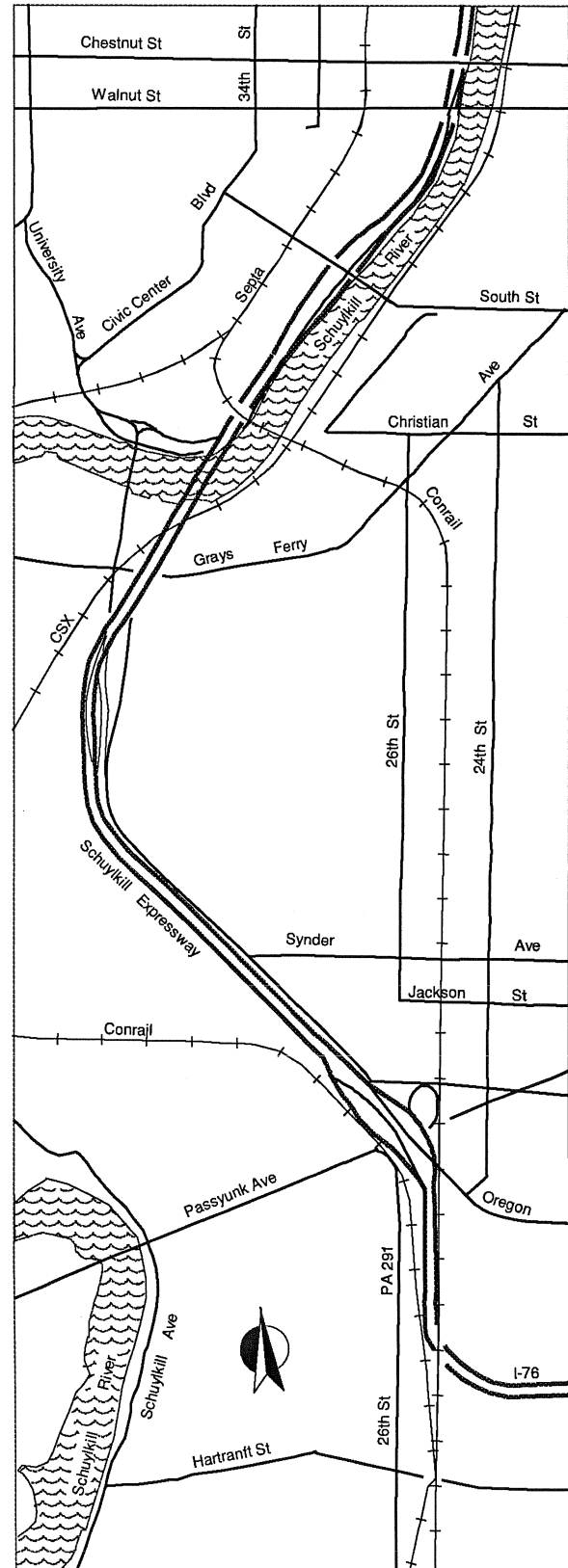
THE SCHUYLKILL EXPRESSWAY

Map 8

Planned Improvements

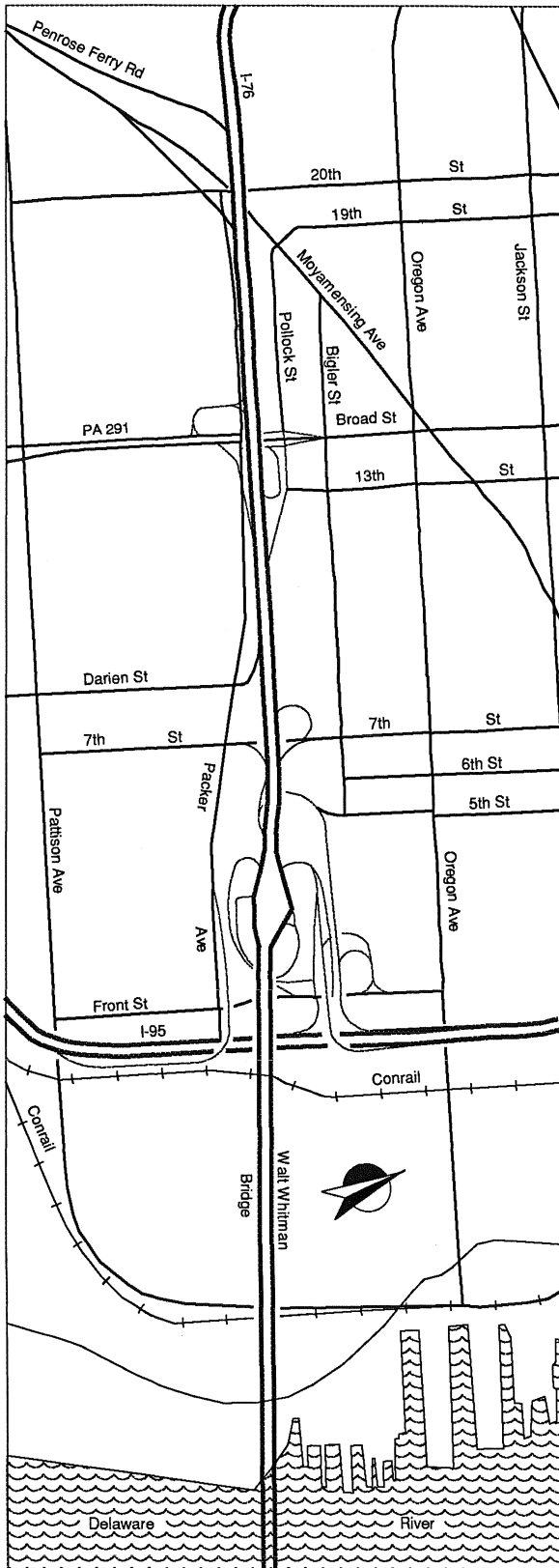
Scale: 1" = 2000'

NO PROJECTS





THE SCHUYLKILL EXPRESSWAY



Map 9

Planned Improvements

Scale: 1" = 2000'

NO PROJECTS



Print
on
Blue

NATURAL AND CULTURAL ENVIRONMENT

The natural and cultural environment maps identify noteworthy features located within close proximity to the roadway. Natural features displayed on the map include wetlands and flood plains. Cultural features are primarily historic.

Most of the wetlands information, including location and general configuration, was derived from the National Wetlands Inventory (NWI) Maps, issued by the United States Department of the Interior. Wetlands are identified by a pattern of horizontal lines. No distinction is made between the various classifications of wetlands (such as estuarine, palustrine, riverine, marine, or lacustrine).

It should be noted that the NWI maps are compiled on a very large scale and are therefore very general. An indication of wetlands on an NWI map suggests that wetlands are probably present to some extent on that particular site. Conversely, an indication that wetlands are not present suggests that the site is probably free of wetlands. However, any specific site which is proposed for development must be surveyed individually before a final determination can be made as to whether or not wetlands are actually present.

The flood plains delineated on the map represent the 100-year flood plain boundaries as identified on the Flood Insurance Rate Maps distributed by the Federal Emergency Management Agency. Flood plain areas are indicated by a pattern of vertical lines. Areas where flood plains and wetlands overlap are indicated by intersecting vertical and horizontal lines. Wetlands which are actually bodies of standing water are already depicted on the map.

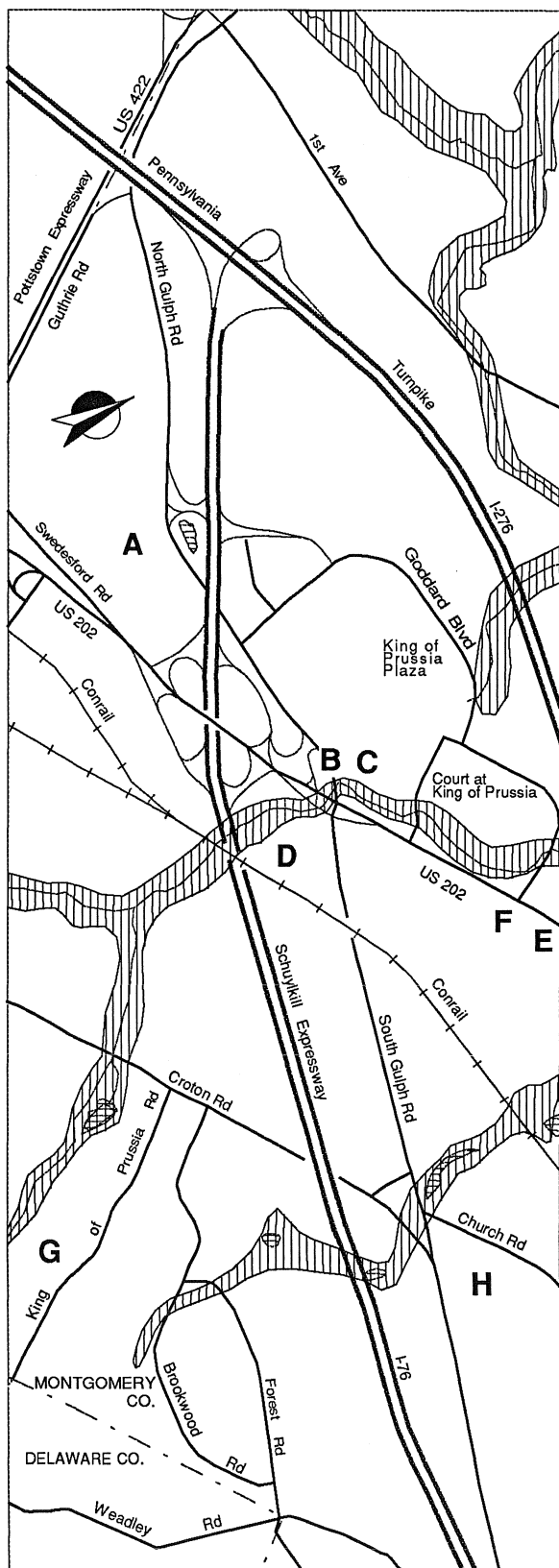
Historical features are also identified. These features include sites, structures, or districts which are significant in terms of American history, architecture, or culture. The general locations of the features are indicated on the map by large, upper-case letters. The site name, the year built, a general description and the type of ownership are provided in the margin text. Sites which are listed on the national Register of Historic Places are identified as such.

Other sites or buildings which have been determined to be eligible for the National Register are also identified. All public agencies are required to safeguard properties on the National Register as well as those which are or may be eligible for it.

Information for this section was gathered from DVRPC reports and field observations.



THE SCHUYLKILL EXPRESSWAY



Map 1

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

- A Valley Forge Club House**
Built 1728. Private.
- B King of Prussia Inn**
Built 1719. Located on "island" in Route 202 at Gulph Road. Commonwealth of Pennsylvania.
- C King of Prussia Store**
Built circa 1800. Private.
- D Peacock Gardens**
Gulph and Swedesford Roads.
Built 1724. Private.
- E Union School Masters Quarters and Addition**
Built circa 1810. Used as station on Underground Railroad, 1830 -1865. Private.
- F Stewart Fund Hall**
Swedesford and Allendale Roads.
Built 1878. Upper Merion Township's first significant public building.
- G L.W. Morrison House**
Built circa 1740. Private.
- H Jonathan Roberts House**
Completed early 1800's.
Upper Merion Park and Historic Foundation.





THE SCHUYLKILL EXPRESSWAY

Map 2

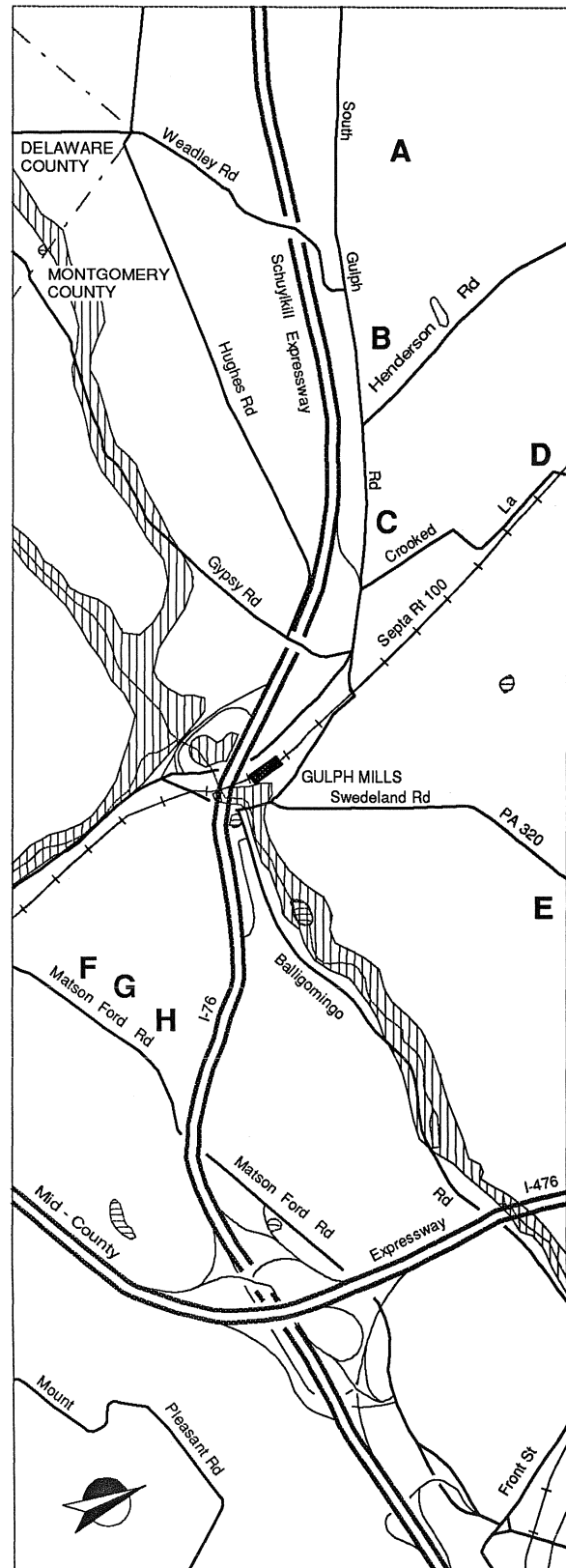
Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

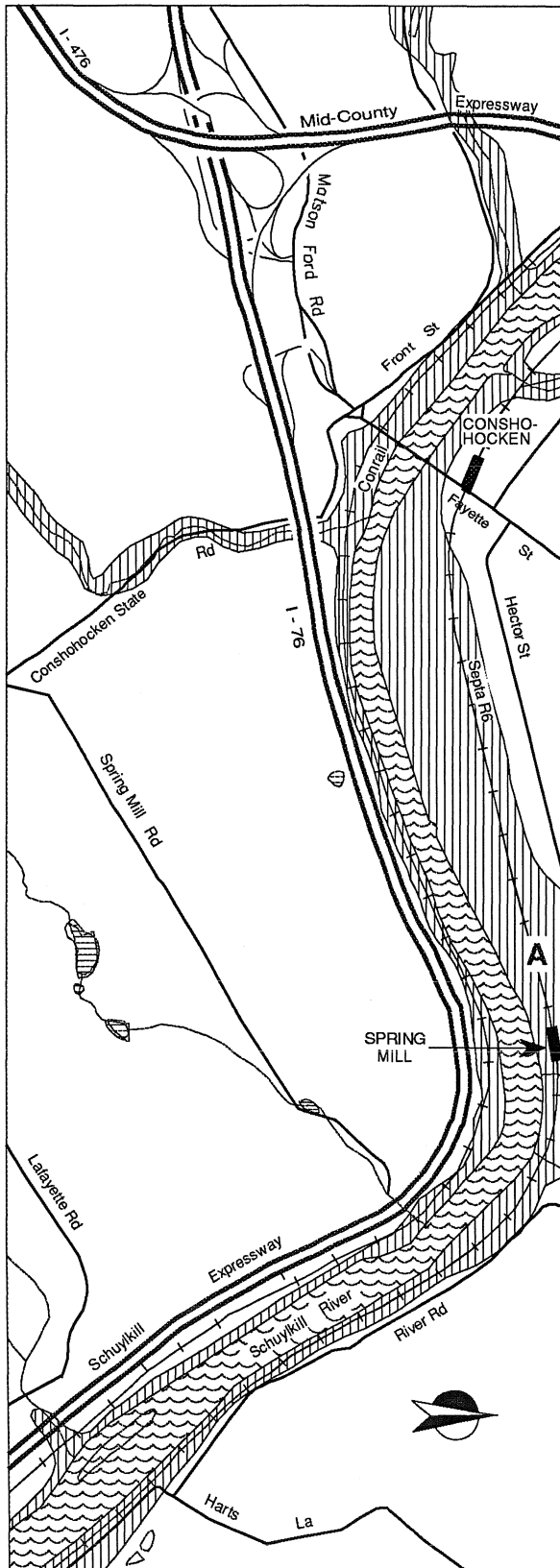
Vertical pattern: 100 - year flood plain

- A Henderson House**
Shoemaker Road north of Gulph.
Built 1792; enlarged mid-1800's.
Private.
- B Gulph Mills Village**
Built circa 1800.
Originally named "Bird-in-Hand".
Private.
- C Hughes Family House**
Built circa 1740; enlarged 1803.
Private.
- D Poplar Lane**
Holstein Road. Built circa 1750;
remodeled 1820. Private.
- E Christ Church (Old Swedes)**
Swedeland Road. Built 1725.
Oldest Swedish Lutheran church
in the United States. Church-owned.
- F Gulph Christian Church**
Gulph Mills and Matsons Ford Roads.
Built 1835; enlarged 1890.
Church-owned.
- G Old Gulph School House**
Matsons Ford Road, Gulph Mills.
Built 1696. Church-owned.
- H Gulph Mills Encampment**
Site of December, 1777 encampment
by the Continental Army (before moving
on to Valley Forge). County.





THE SCHUYLKILL EXPRESSWAY



Map 3

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

A Peter Legeaux Mansion

Spring Mill on the Schuylkill River.

Built circa 1735. Private.





THE SCHUYLKILL EXPRESSWAY

Map 4

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

A Mill Worker House

1426-1428 Rose Glen Road.

Built circa 1845, to house mill workers.
Private.

B Rose Glen Store and Post Office

Rose Glen Road at Mill Creek.

Built circa 1835.

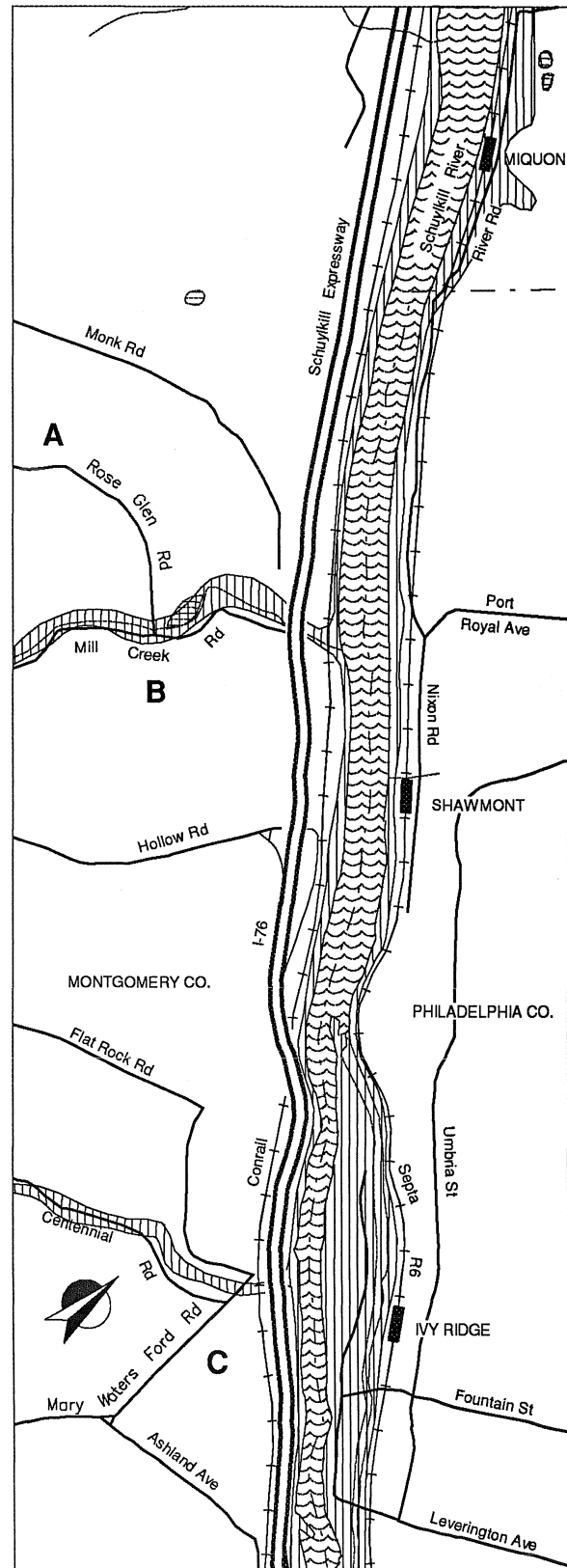
Private.

C Leedom House and Barn

501 Centennial Road, at Mary Waters Ford Road.

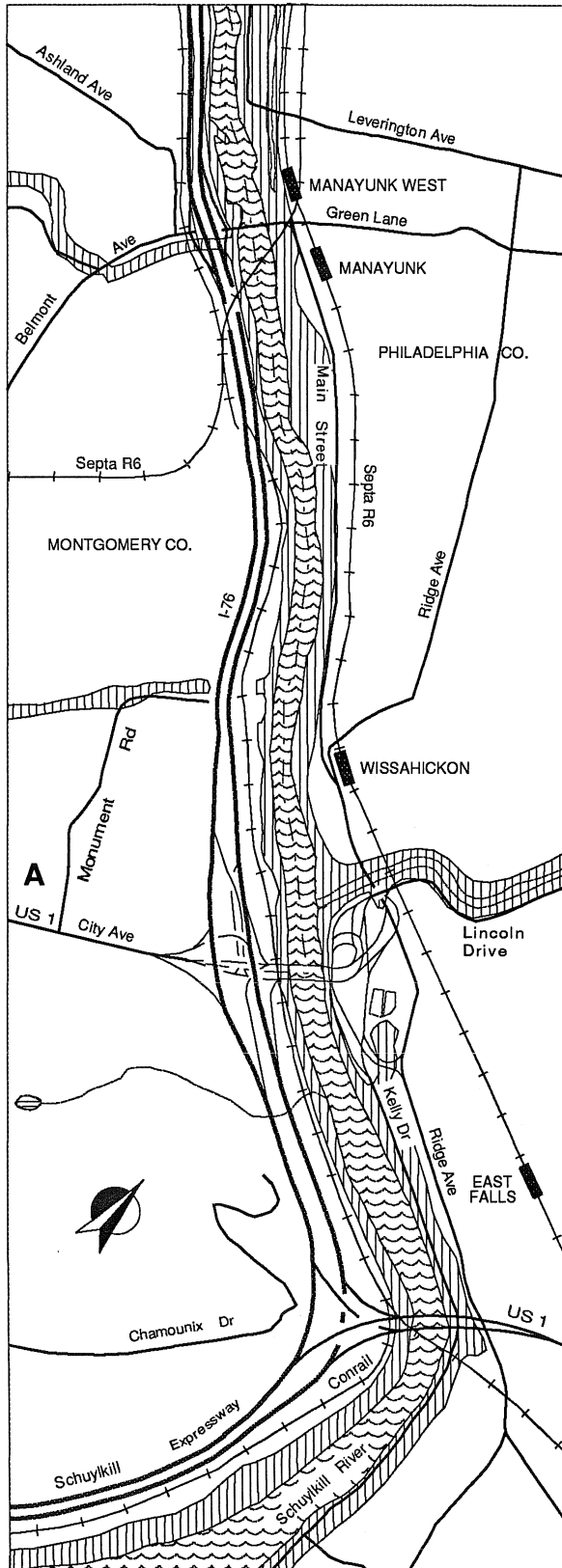
Built circa 1787; 19th century additions.

Private.





THE SCHUYLKILL EXPRESSWAY



Map 5

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

A Pencoyd

335 East City Line Avenue.
Built circa 1690. Private.





THE SCHUYLKILL EXPRESSWAY

Map 6

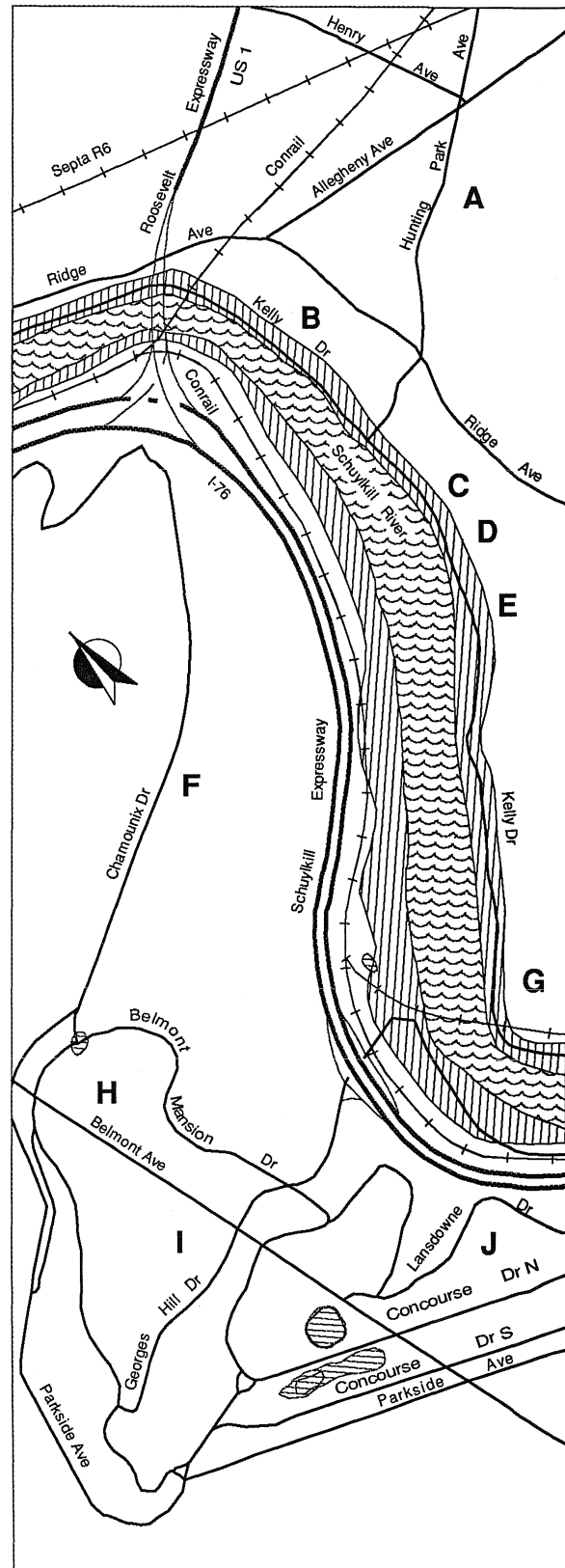
Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

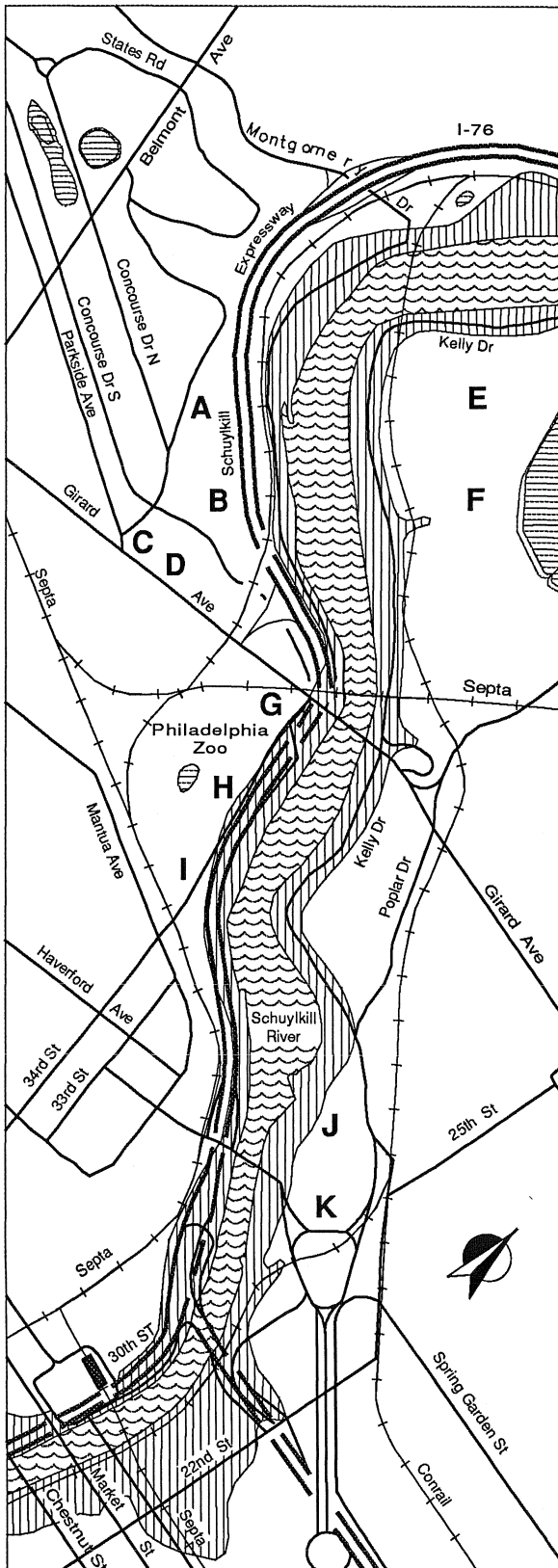
Vertical pattern: 100 - year flood plain

- A St. James the Less Church**
Hunting Park Avenue and 33rd Street.
Built circa 1846. Church-owned.
- B Laurel Hill Cemetery**
Lehigh and Ridge Avenue. Entrance built
1836; monuments dating to the 1840's. Private.
- C Strawberry Mansion**
Rebuilt 1790's. Fairmount Park Commission.
- D Woodford Mansion**
Dauphin Street and Ridge Avenue. Enlarged
circa 1756. Listed on the National Register.
Fairmount Park Commission.
- E Laurel Hill (Randolph House)**
Built 1748. Fairmount Park Commission.
- F Chamounix Mansion (Plumstead Mansion)**
Built 1802. Fairmount Park Commission.
- G Rockland Mansion**
Built 1810. Fairmount Park Commission.
- H Belmont Mansion**
Built circa 1730; enlarged circa 1755.
Fairmount Park Commission.
- I Ohio House**
Built for the 1876 Centennial Exposition.
Fairmount Park Commission.
- J Memorial Hall**
Built for the 1876 Centennial Exposition.
Fairmount Park Commission.





THE SCHUYLKILL EXPRESSWAY



Map 7

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

A Cedar Grove Mansion

Lansdowne at 41st Street. Built circa 1721; enlarged 1748 and 1799. Moved to Fairmount Park 1926. Fairmount Park Commission.

B The Cliffs

East Park near 33rd. Built 1757. Fairmount Park Commission.

C Sweetbriar

Lansdowne north of Girard. Built 1797. Fairmount Park Commission.

D Letitia Street House

Lansdowne near 38th Street. Built 1713; moved to Fairmount Park 1883. Fairmount Park Commission.

E Ormiston Mansion

Built 1798. Fairmount Park Commission.

F Mount Pleasant

Fountain Green Drive. Built circa 1760. Listed on the National Register. Fairmount Park Commission.

G Hatfield House

33rd Street and Girard Avenue. Moved to Fairmount Park and restored 1930. Fairmount Park Commission.

H Zoo Entrance Pavilion

Built 1873-1875. Philadelphia Zoo.

I Solitude

In Zoological Gardens. Built 1785 by John Penn. Fairmount Park Commission.

J Fairmount Water Works

Built 1819-1822. Fairmount Park Commission.

K Philadelphia Museum of Art

Opened 1928. Museum.





THE SCHUYLKILL EXPRESSWAY

Map 8

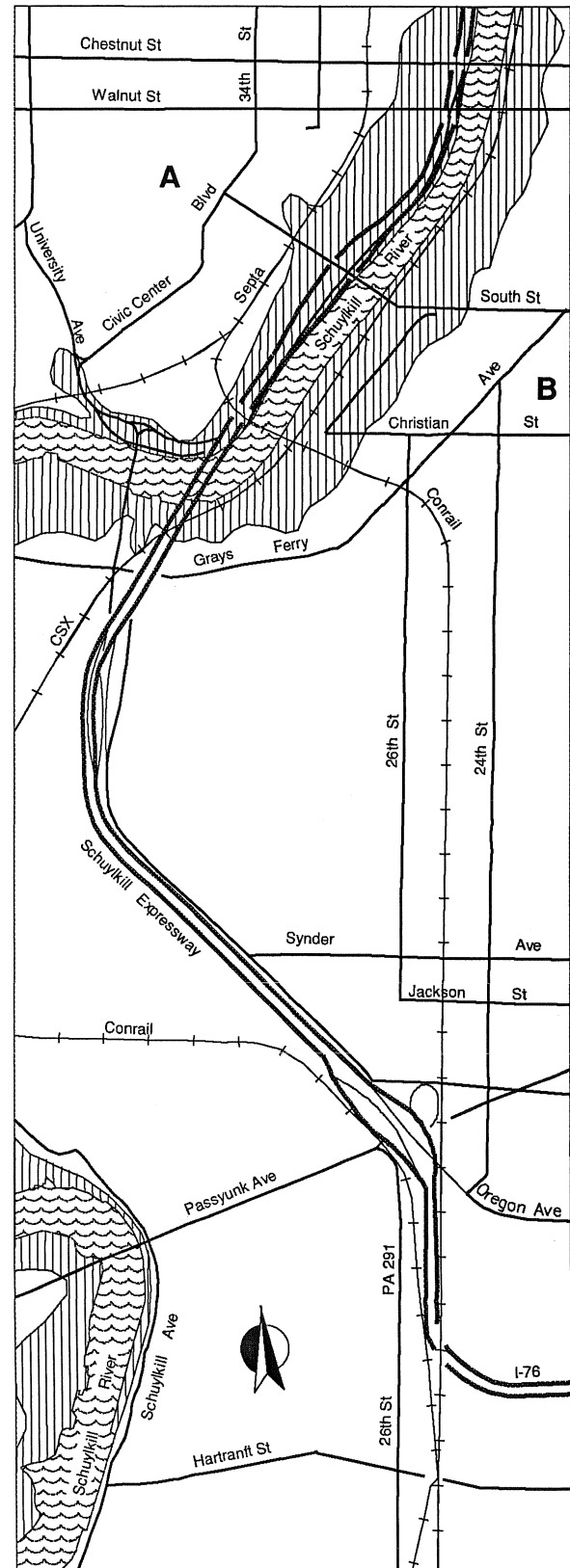
Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

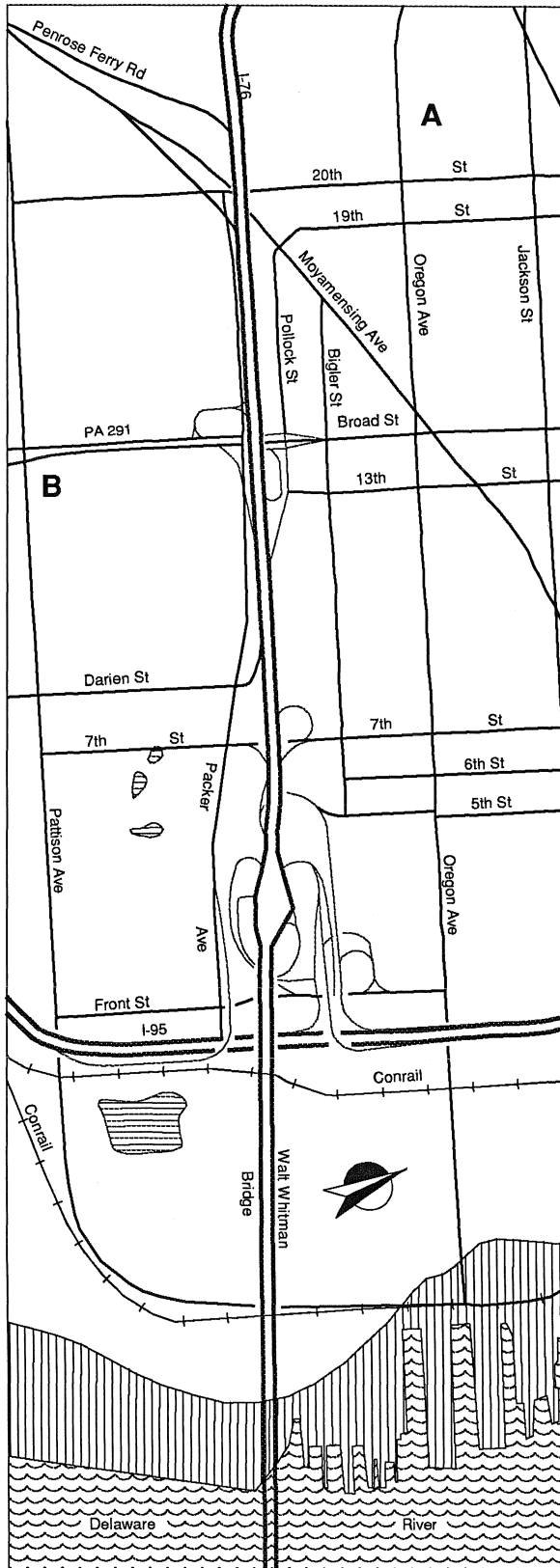
Vertical pattern: 100 - year flood plain

- A University of Pennsylvania**
Founded 1740. Significant buildings include Fine Arts Library; University Museum; College Hall. Private.
- B United States Naval Home**
Grays Ferry and Bainbridge. Built 1827-1833. Federal.





THE SCHUYLKILL EXPRESSWAY



Map 9

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

- A** **Stephen Girard Country House**
("Gentilhommiere") and Utility Building
Northwest corner of Shunk and 21st Streets.
Built circa 1750; enlarged 1798.
City of Philadelphia.
- B** **Belair**
Broad and Pattison Avenues. Built 18th
century. Fairmount Park Commission.



Print

on

Ivory

TRANSIT AND TRAFFIC DATA

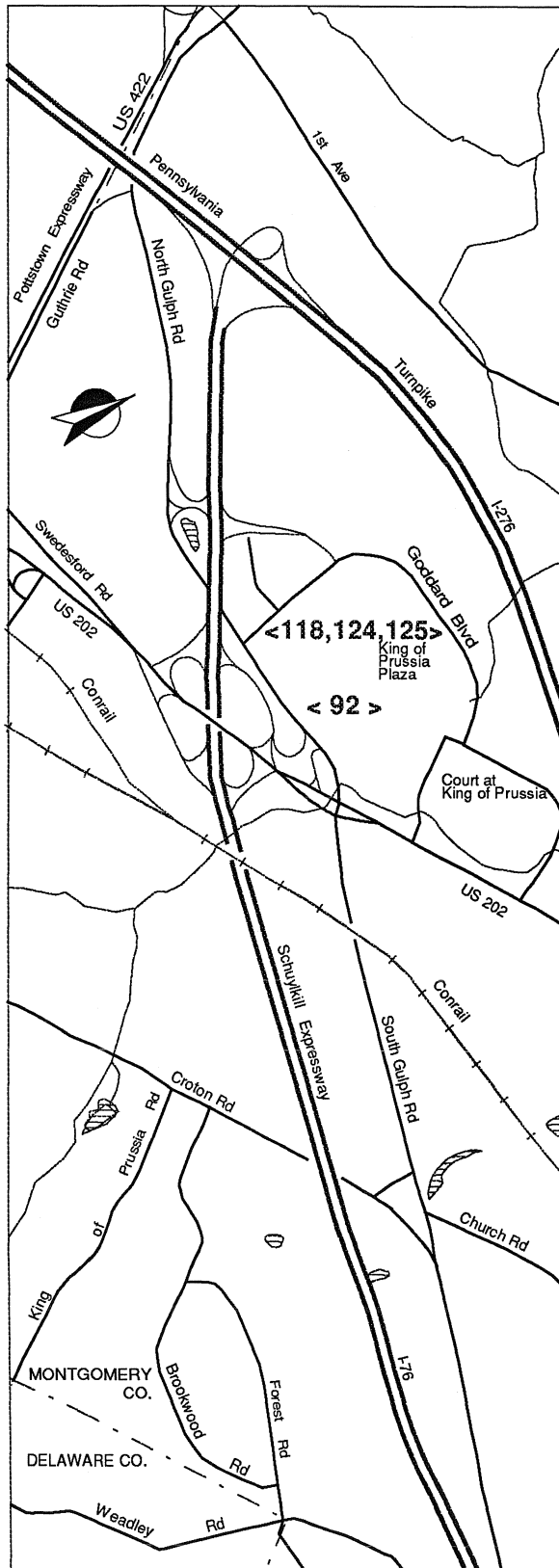
These maps illustrate those transit routes which either cross over or under the Schuylkill Expressway. Reference is made in the accompanying text to the route number, the origin and destination points, the road each route travels at the point of intersection, and whether the crossing is over or under I-76. All points of intersection are shown. There are six bus routes currently operating along the Schuylkill Expressway.

Average Annual Daily Traffic (AADT) counts are shown on the individual maps.

Information for this section was provided by Southeastern Pennsylvania Transportation Authority.



THE SCHUYLKILL EXPRESSWAY



Map 1

Transit and Traffic Data

Scale: 1" = 2000'

- 92** King of Prussia to West Chester
crosses over I-76 at US 202
- 118** Chester to King of Prussia
crosses under I-76 on North Gulph Road
- 124** Center City to King of Prussia and Chester-
brook crosses under I-76 at North Gulph Road
- 125** Center City to King of Prussia and Valley
Forge Park crosses under I-76 at North Gulph
Road





THE SCHUYLKILL EXPRESSWAY

Map 2

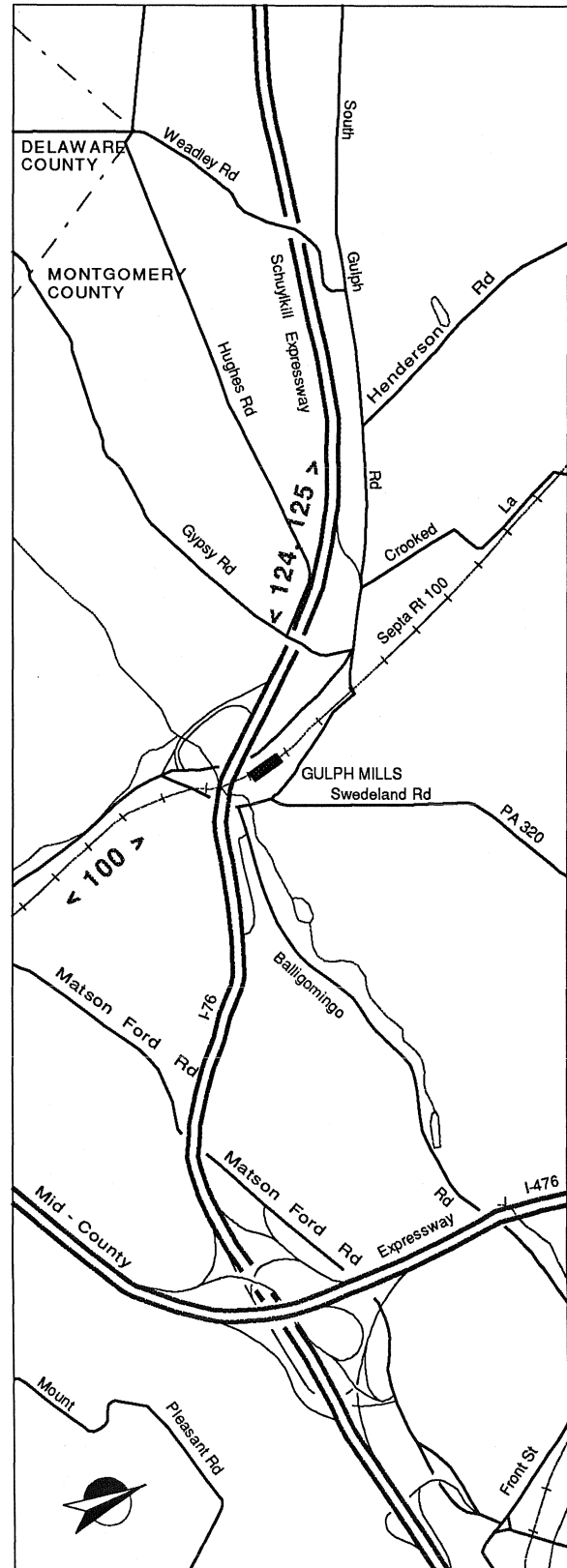
Transit and Traffic Data

Scale: 1" = 2000'

124 King of Prussia to Center City via Expressway

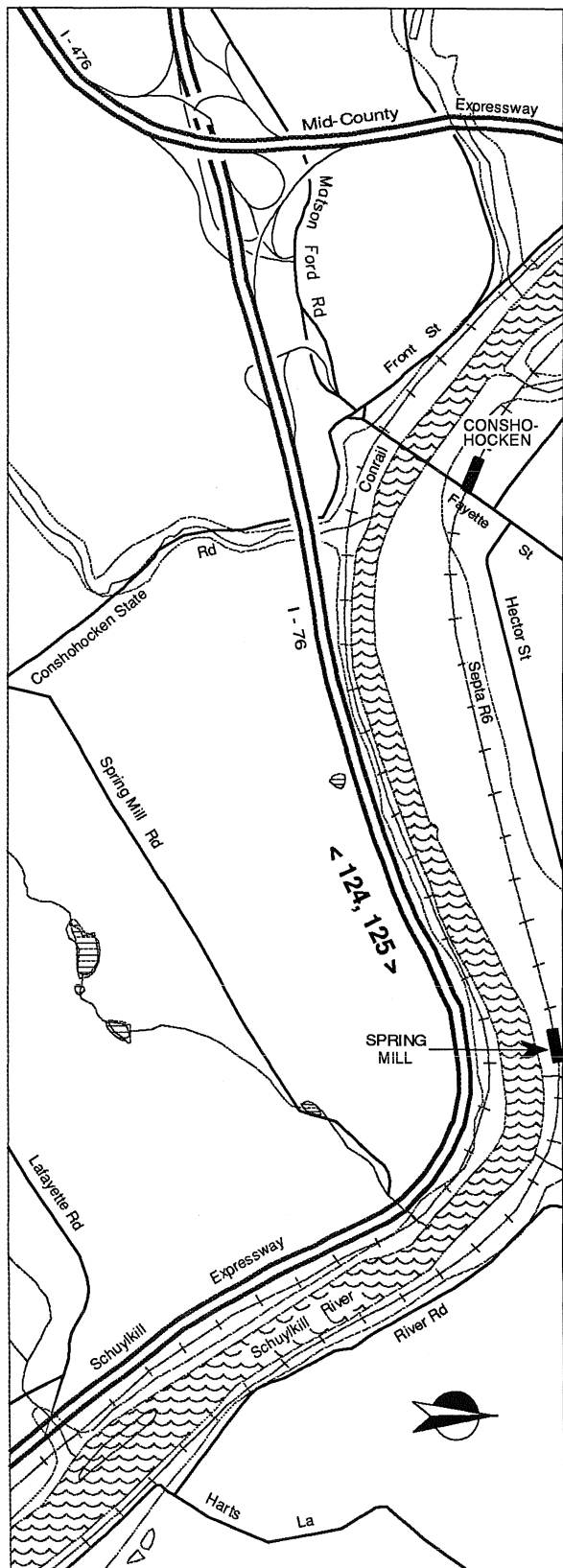
125 King of Prussia to Center City via Expressway

100 69th Street to Norristown Light Rail
crosses under I-76 north of PA 320





THE SCHUYLKILL EXPRESSWAY



Map 3

Transit and Traffic Data

Scale: 1" = 2000'

124 King of Prussia to Center City via Expressway

125 King of Prussia to Center City via Expressway





THE SCHUYLKILL EXPRESSWAY

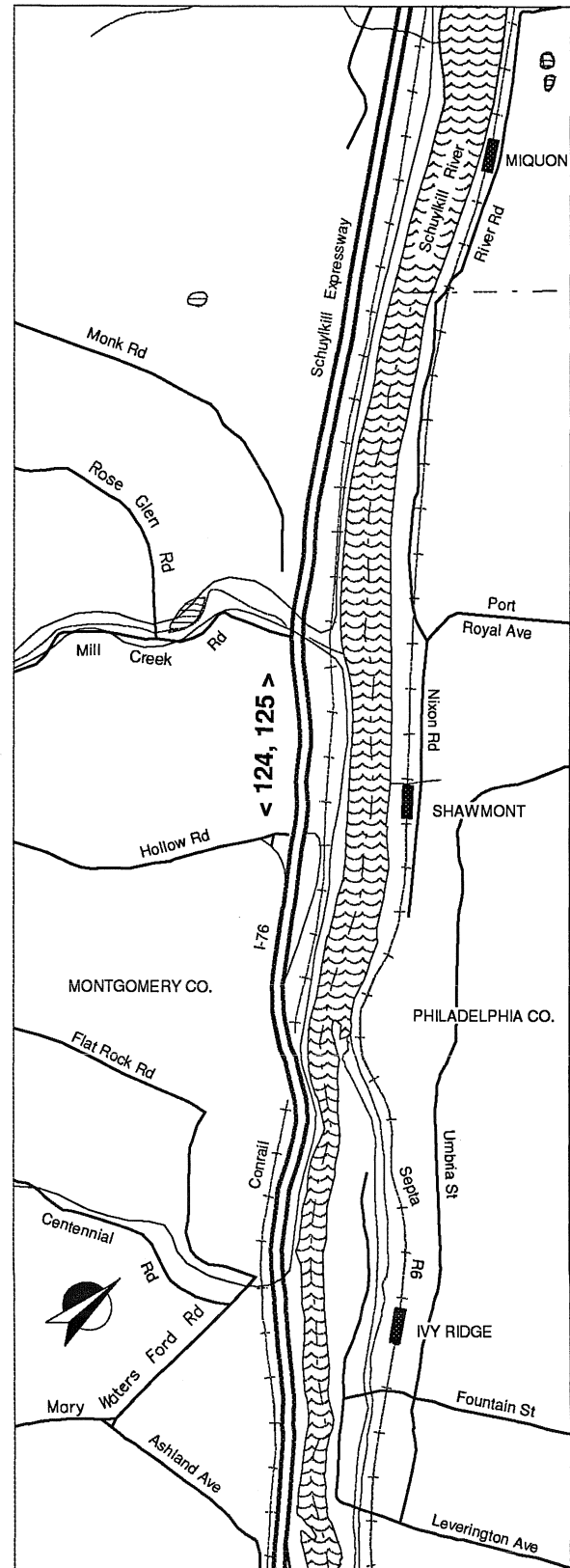
Map 4

Transit and Traffic Data

Scale: 1" = 2000'

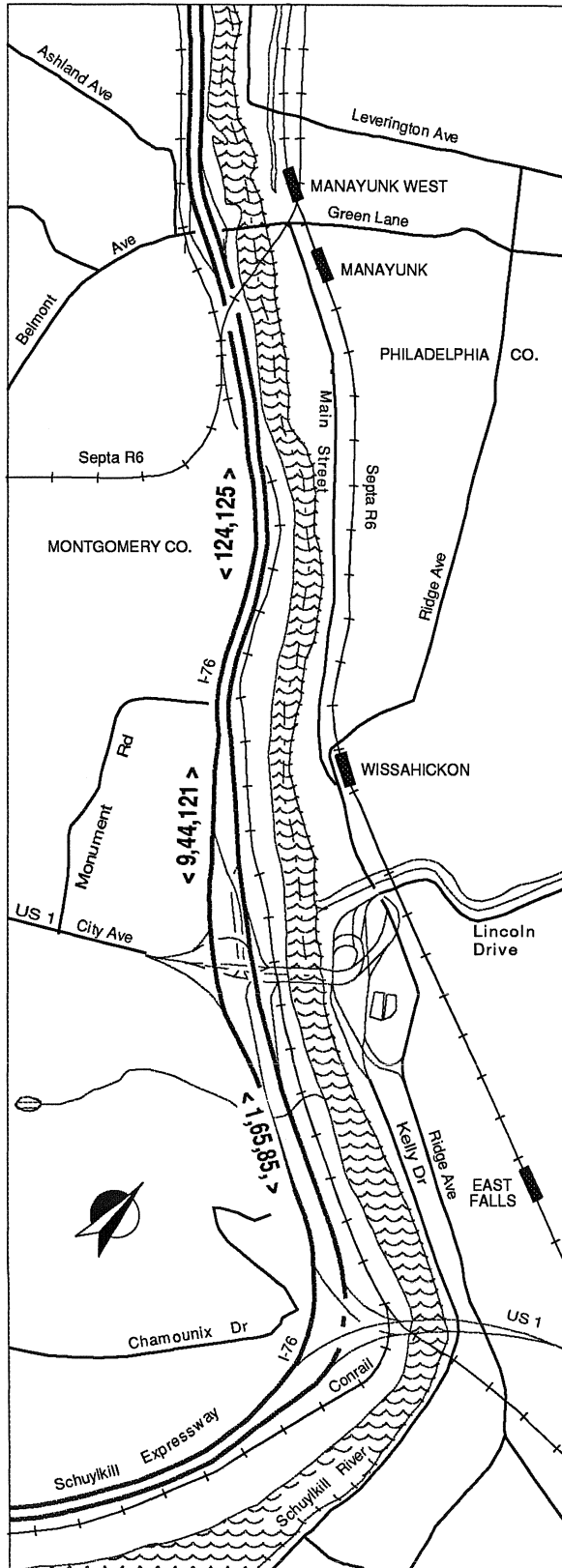
124 King of Prussia to Center City via Expressway

125 King of Prussia to Center City via Expressway





THE SCHUYLKILL EXPRESSWAY



Map 5

Transit and Traffic Data

Scale: 1" = 2000'

- 124** King of Prussia to Center City via Expressway
- 125** King of Prussia to Center City via Expressway
- 65** 69th Street to Germantown via City Avenue crosses under I-76 at US1
- 85** Parkside to Strawberry Mansion via Parkside crosses under I-76 at US1
- 9** Andorra to Center City via Expressway
- 44** Independence Mall to Ardmore via Expressway
- 121** Independence Mall to Gladwynne via Expressway
- 1** Northeast Philadelphia to 69th Street Terminal crosses under I-76 at US1





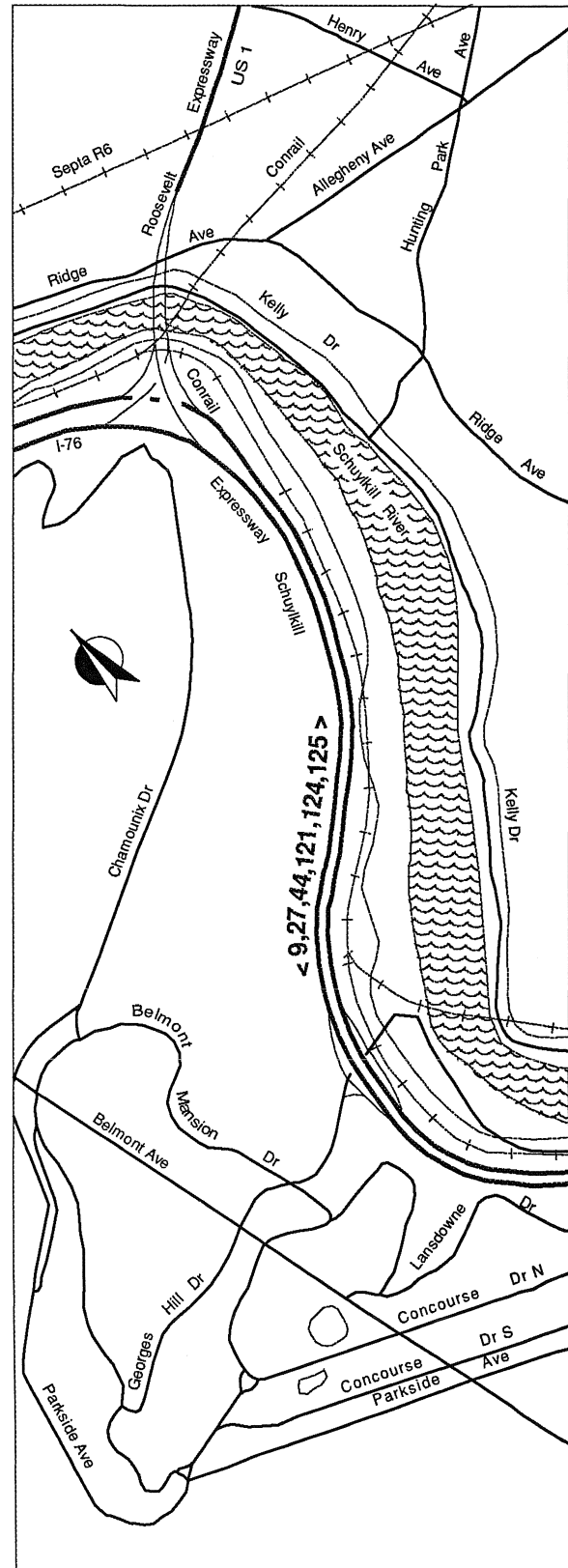
THE SCHUYLKILL EXPRESSWAY

Map 6

Transit and Traffic Data

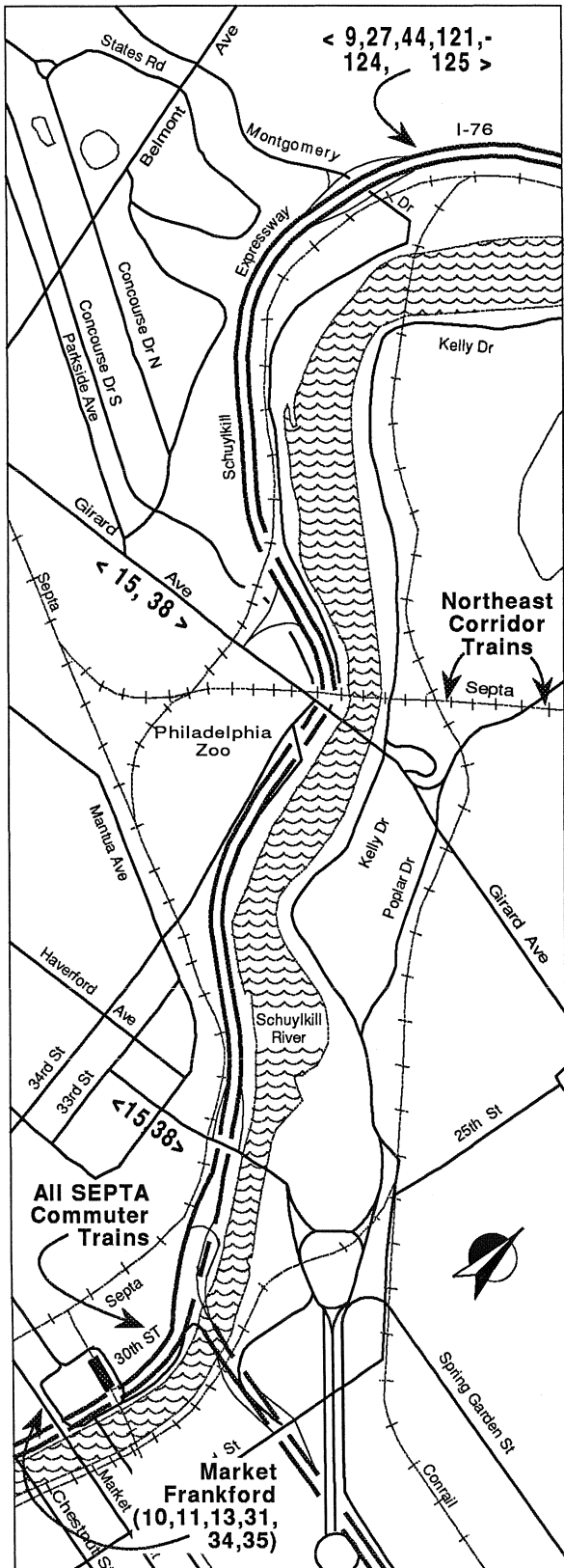
Scale: 1" = 2000'

- 9 Andorra to Center City via Expressway
- 27 Plymouth Meeting Mall to Center City via Expressway
- 44 Independence Mall to Ardmore via Expressway
- 121 Independence Mall to Gladwynne via Expressway
- 124 King of Prussia to Center City via Expressway
- 125 King of Prussia to Center City via Expressway





THE SCHUYLKILL EXPRESSWAY



Map 7

Transit and Traffic Data

Scale: 1" = 2000'

- 9 Andorra to Center City via Expressway
- 27 Plymouth Meeting Mall to Center City via Expressway
- 44 Independence Mall to Ardmore via Expressway
- 121 Independence Mall to Gladwynne via Expressway
- 124 King of Prussia to Center City via Expressway
- 125 King of Prussia to Center City via Expressway
- 15 Port Richmond to Haddington crosses over I-76 at Girard Avenue
- 38 East Falls and Bala Cynwyd to Independence Mall crosses over I-76 at both Girard Avenue and Spring Garden Street
- SEPTA and AMTRAK**
R-7, R-8, and Northeast Corridor trains crosses over I-76 west of Girard Avenue
- 43 Parkside to Northern Liberties crosses over I-76 at Spring Garden Street
- SEPTA**
All Regional Rail Lines crosses over I-76 North of JFK Boulevard
- Market Frankford Line**
crosses under I-76 at Market Street
- 10 Overbrook to Center City Subway-Surface crosses under I-76 at Market Street
- 11 Darby to Center City Subway-Surface crosses under I-76 at Market Street
- 13 Darby and Yeadon to Center City Subway Surface crosses under I-76 at Market Street
- 34 Angora to Center City Subway-Surface crosses under I-76 at Market Street
- 35 Eastwick to Center City Subway-Surface crosses under I-76 at Market Street
- 31 Overbrook Park to Center City crosses under I-76 at Market Street





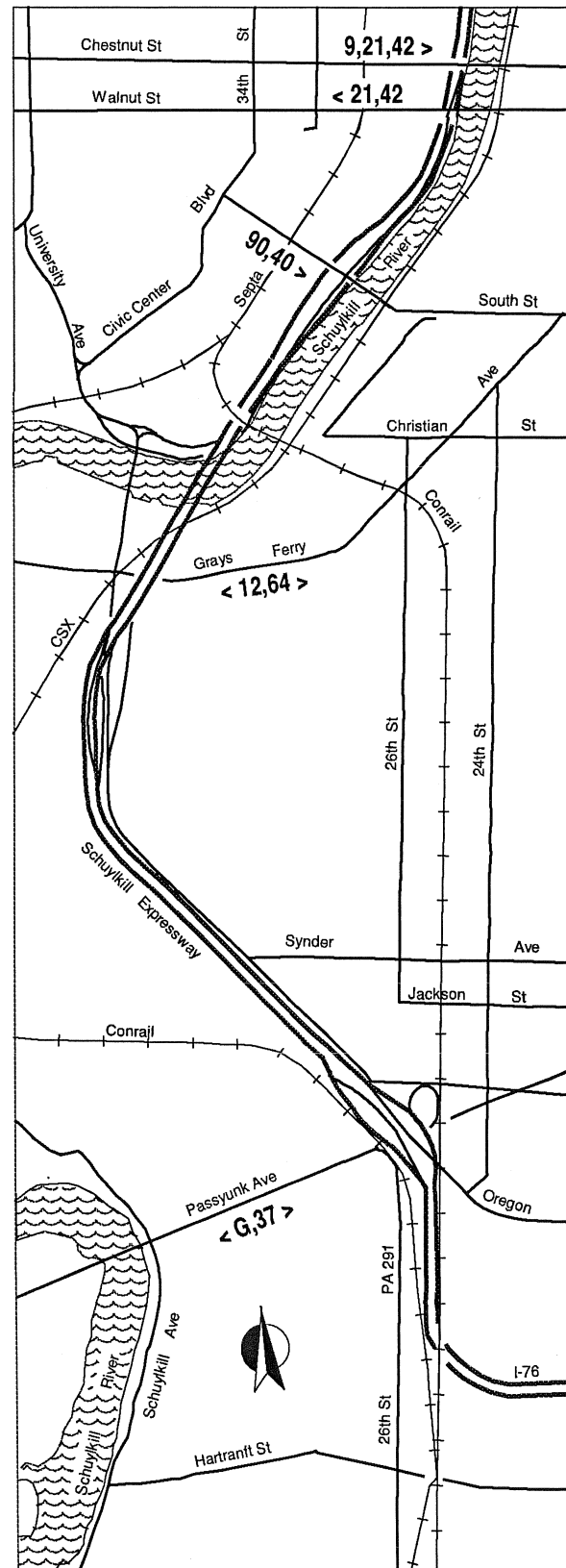
THE SCHUYLKILL EXPRESSWAY

Map 8

Transit and Traffic Data

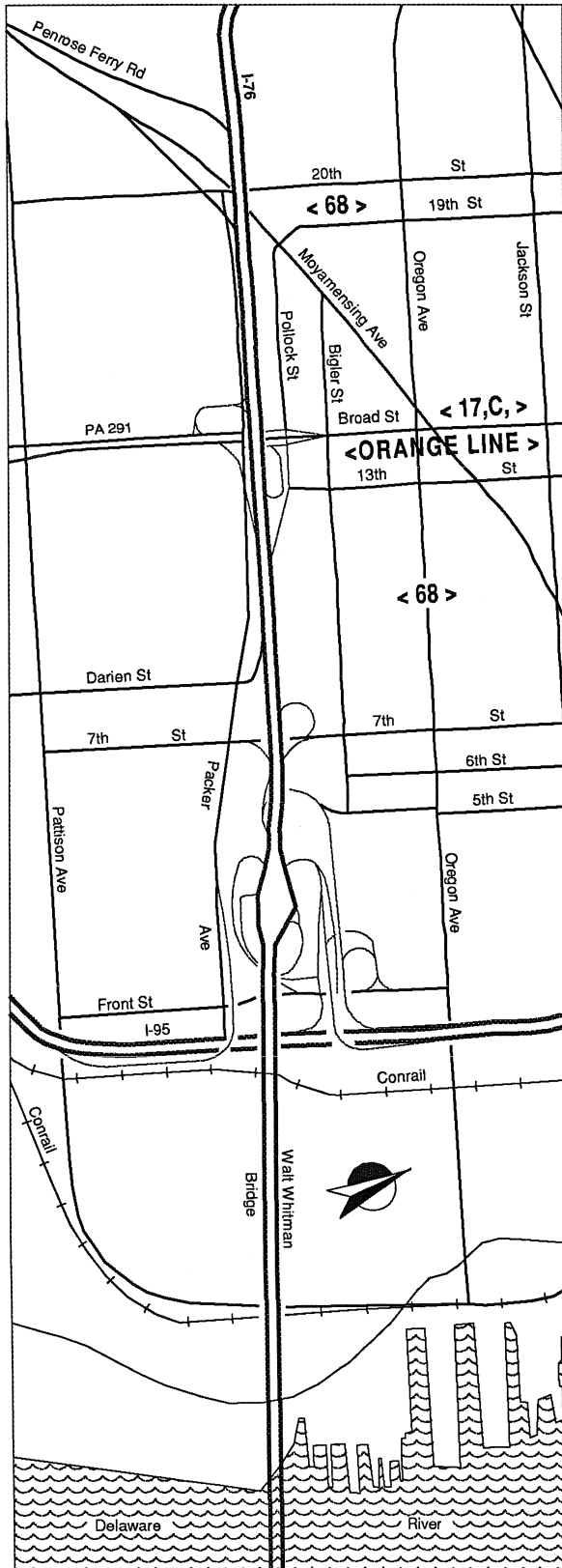
Scale: 1" = 2000'

- 9** Andorra to Center City via Expressway crosses over I-76 at Chestnut Street
- 21** 69th Street Terminal and Wycombe to Penn' Landing crosses over I-76 at Chestnut and Walnut Streets
- 42** West Philadelphia to Society Hill crosses over I-76 at Chestnut and Walnut Streets
- 40** Parkside to Society Hill crosses over I-76 at South Street
- 90** University City to Society Hill crosses over I-76 at South Street
- 12** Woodland to Independence Mall crosses under I-76 at Grays Ferry Avenue
- 64** Wyalusing to Queen Village crosses under I-76 at Grays Ferry Avenue
- G** South Philadelphia to Overbrook crosses under I-76 at Passyunk Avenue
- 37** South Philadelphia to Chester crosses under I-76 at Passyunk Avenue





THE SCHUYLKILL EXPRESSWAY



Map 9

Transit and Traffic Data

Scale: 1" = 2000'

17 South Philadelphia to Penn's Landing
crosses under I-76 at Broad Street

68 South Philadelphia to Int'l Airport
crosses under I-76 at 10th Street
and Moyamensing

C West Oak Lane to South Philadelphia
crosses under I-76 at Broad Street

Orange Line Broad Street Line
crosses under I-76 at Broad Street•



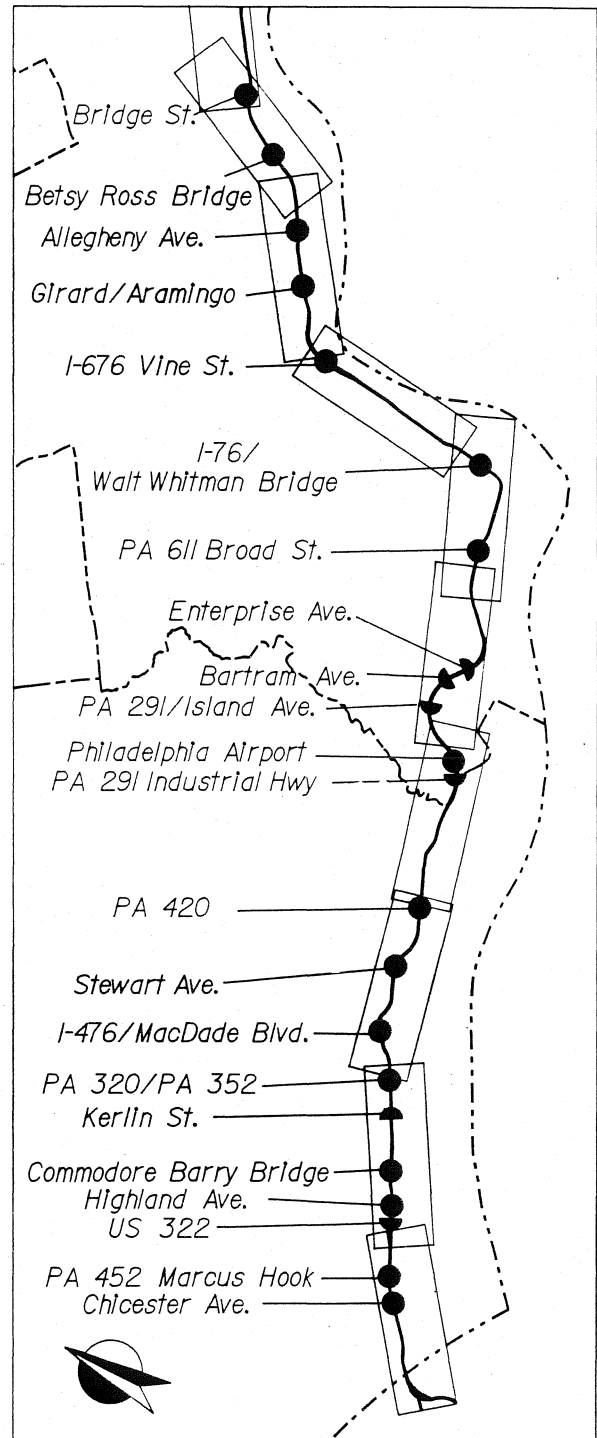
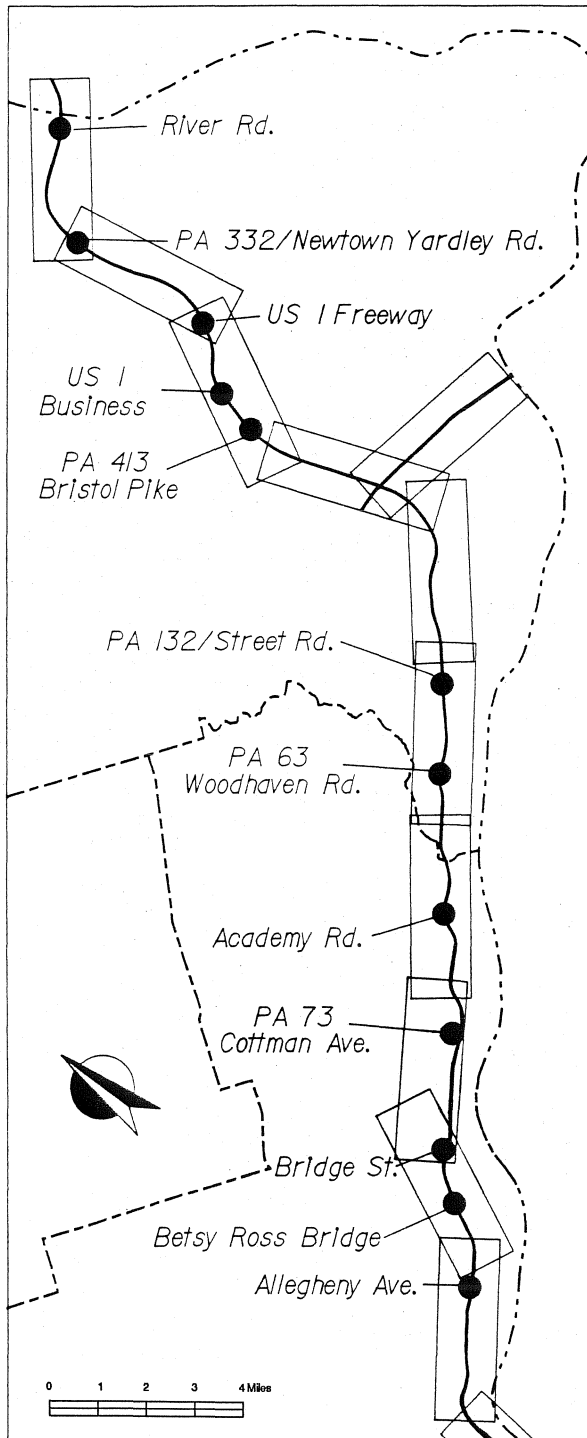
SECTION TWO



INTERSTATE ROUTE 95



INTERSTATE 95



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DESIGN FEATURES

These of maps inventory the current physical attributes, including physical characteristics and structural information, of Interstate Route 95. The roadway itself is three or four travel lanes by direction south of PA Route 413 (New Rodgers Road), and two lanes by direction north of PA Route 413. Travel lanes are 12' in width, with median strip and wide shoulders the entire length of the road. Because of its age, pavement quality is deteriorating in many places along its length.

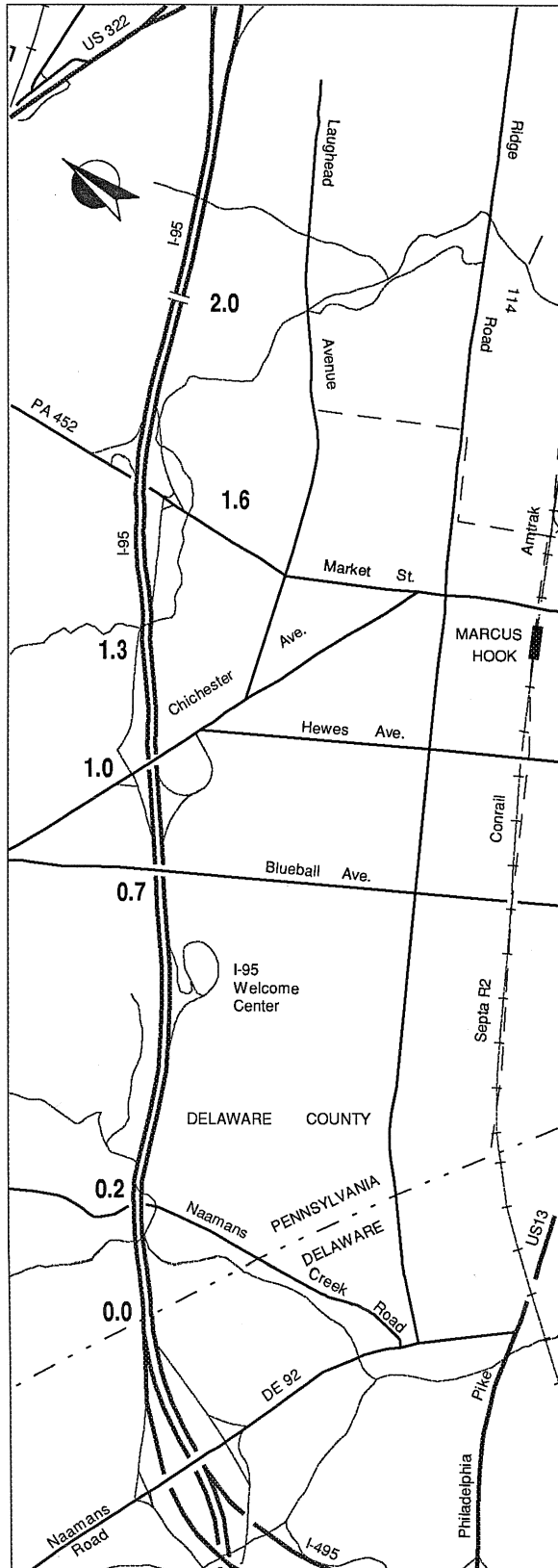
Segment/offset markers on this highway run south to north, and are shown on these maps accordingly. General information about the section of road shown in each map is located underneath the title block. Segment/offset numbers posted on the maps correspond to locations of structures which are described beside each map.

Specific information regarding structures is provided as necessary. Citations are made for either bridges or overpasses. For bridges, structure number, span length, and deck width are given, as well as information regarding the underneath facility. For structures overpassing Interstate Route 95, the distance between piers is given in exact measurements (where available) or is listed according the location of the piers. Structure number and description of facility carried are also given. Due to the interstate status of this highway, there are no sub-standard weight or clearance structures.

Information on these maps was gleaned from a combination of construction drawings, PennDOT Bridge Unit's structure lists, DVRPC reports, and field observations.



INTERSTATE 95



Map 1

Design Features

Scale: 1" = 2000'

Note: Three travel lanes by direction. Median 45' entire length. ROW varies between 260' and 330'.

0.0 State Line

0.2 Dual Bridge

over Naamans Creek Road SR 0491 LR 135
Span Length: 135' each
Deck Width: 50' each

0.7 Overpass

Blueball Avenue SR 3011 LR 23063
Distance Between Piers: 30', 65', 65', 30'

1.0 Overpass

Chichester Road SR 3009 LR A-1416
Distance Between Piers: 65', 65'

1.3 Dual Bridge

over Marcus Hook Creek
Span Length: 50' each
Deck Width: 55'

1.6 Dual Bridge

over Market Street PA 452 SR 0452 LR 418
Span Length: 80' each
Deck Width: 55' each

2.0 Overpass

Sun Oil Company private roadway
Distance Between Piers: 20', 70', 70', 20'





INTERSTATE 95

Map 2

Design Features

Scale: 1" = 2000'

Note: Three travel lanes by direction. Median varies between 16' and 60'. ROW varies between 105' and 340'.

2.5 Bridge

over US 322

Highland Avenue Off-Ramp

Span Length: SB- 230' NB- 30'

Deck Width: SB- 40' NB-45'

2.8 Overpass

Highland Avenue

Distance Between Piers:

60', 80'

3.3 Overpass

Engle Street SR 3007

LR 23011

Distance Between Piers:

25', 50', 50', 25'

3.7 Dual Bridge

US 322 Commodore Barry

Bridge Approaches SR8017

Span Length: SB- 800'

NB- 700'

Deck Width: 45' each

4.0 Dual Bridge

over Railroad

Span Length: 200' each

Deck Width: SB- 45' NB-70'

4.6 Bridge

over Kerlin Street

Span Length: 60'

Deck Width: 100'

4.7 Bridge

over Chester Creek

Span Length: 350'

Deck Width: 90'

5.2 Overpass

Crosby Street Pedestrian
Bridge

Distance Between Piers: 90'

5.4 Three Overpasses

Potter St.; Upland St.;

and Edgemont Avenue

PA 352 SR 0352 LR 132

Distance Between Piers:

90' each

5.5 Overpass

Walnut Street Pedestrian
Bridge

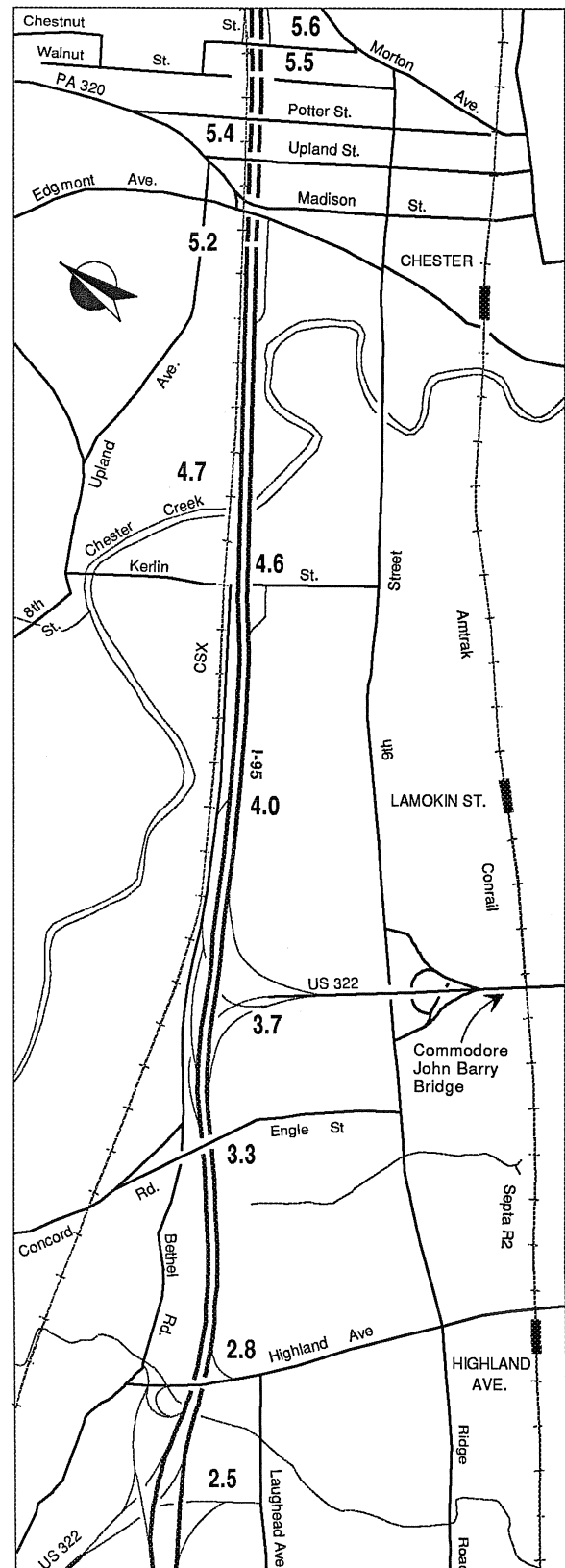
Distance Between Piers:

90'

5.6 Overpass

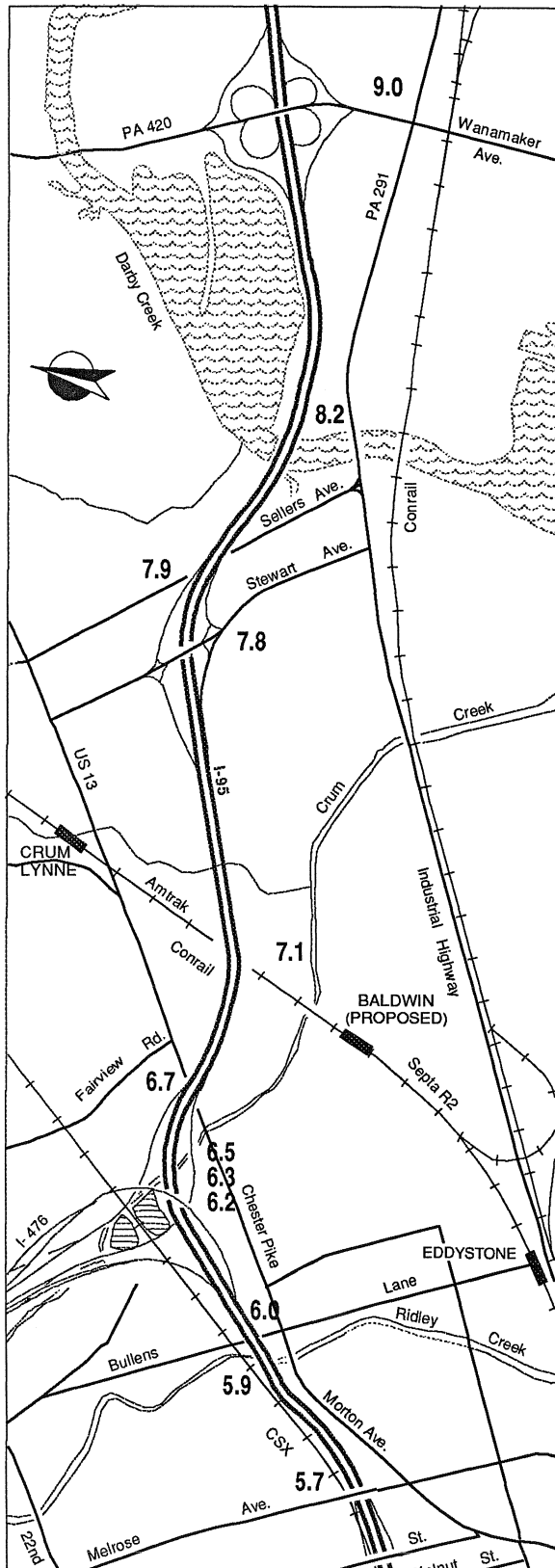
Chestnut Street

Distance Between Piers: 90'





INTERSTATE 95



Map 3

Design Features

Scale: 1" = 2000'

Note: Four travel lanes by direction north of 6.7. Two travel lanes by direction at I-476 interchange. Three travel lanes south of 5.9. Median varies between 5' to 25'. ROW varies between 100' and 300'.

5.7 Overpass

Melrose Avenue
Distance Between Piers:
180'

5.9 Bridge

over Ridley Creek
Span Length: 220'
Deck Width: 90'

6.0 Bridge

over Bullens Lane
Span Length: 100' approx.
Deck Width:
100'

6.2 Dual Bridge

over I-476 to I-95 NB Ramp
Span Length: 180' approx.
Deck Width: 40' each

6.3 Overpass

I-95 Northbound to I-476
Ramp
Distance Between Piers:
70', 45'

6.5 Dual Bridge

over Crum Creek
Span Length: 220' approx.
Deck Width: 40' each

6.7 Dual Bridge

over Chester Pike
Span Length: 170' approx.
Deck Width: 65' each

7.1 Dual Bridge

over AMTRAK
Span Length: 300' approx.
Deck Width: 60' each

7.8 Overpass

Stewart Avenue SR 2033
LR 180-SP
Distance Between Piers:
80', 80'

7.9 Dual Bridge

over Sellers Avenue
Span Length: 300' each
Deck Width: 80' each

8.2 Bridge

over Darby Creek
Span Length: 340' Deck
Width: 140'

9.0 Overpass

Wanamaker Avenue PA
420 SR 0420 LR 30-SP-A-
Distance Between Piers:
50', 50', 50', 50'





INTERSTATE 95

Map 4

Design Features

Scale: 1" = 2000'

Note: Three or Four travel lanes by direction. Median varies between 25' and 48'. ROW varies between 260' and 850'.

10.7 - 11.2 Dual Elevated Section

Span Length: 3000'

Deck Width: 85' each roadway

11.4 Overpass

SEPTA R1 Airport Line

Distance Between Piers: 95', 95'

11.6 Overpass

Philadelphia International Airport "Departures" Ramp

Distance Between Piers: 90', 90'

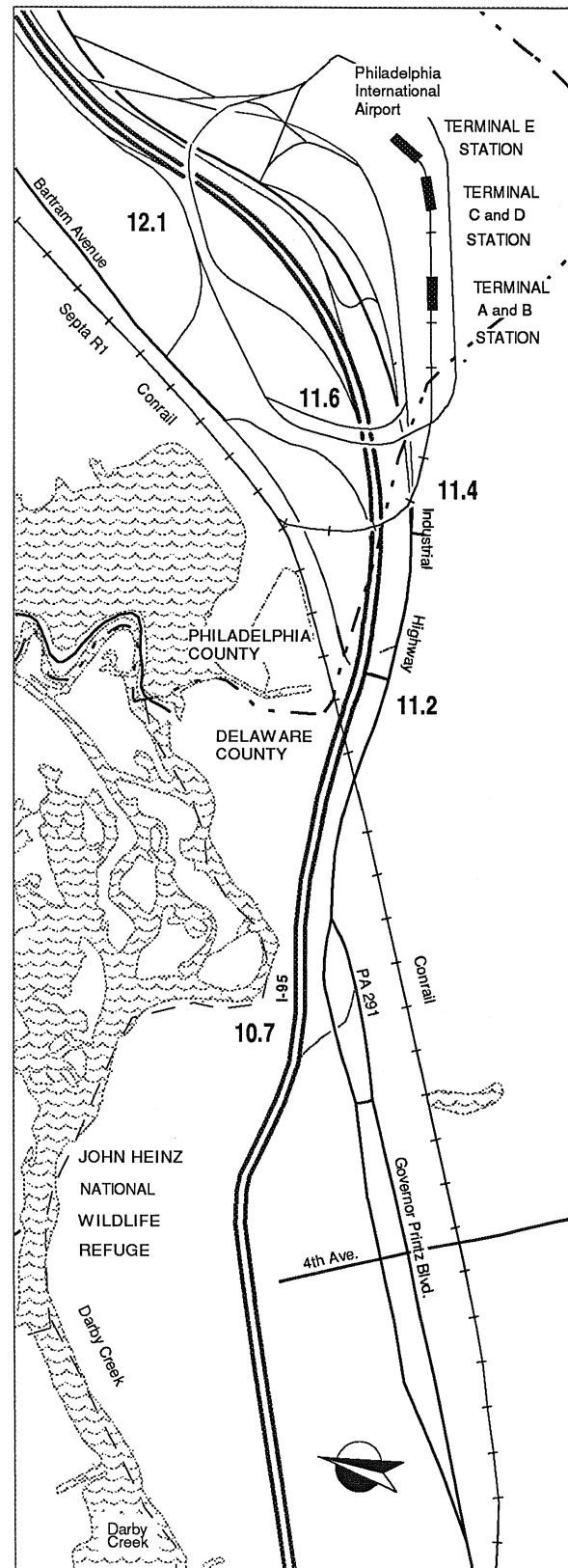
11.6 Overpass

Philadelphia International Airport "Arrivals" Ramp Distance Between Piers: 95', 95'

12.1 Overpass

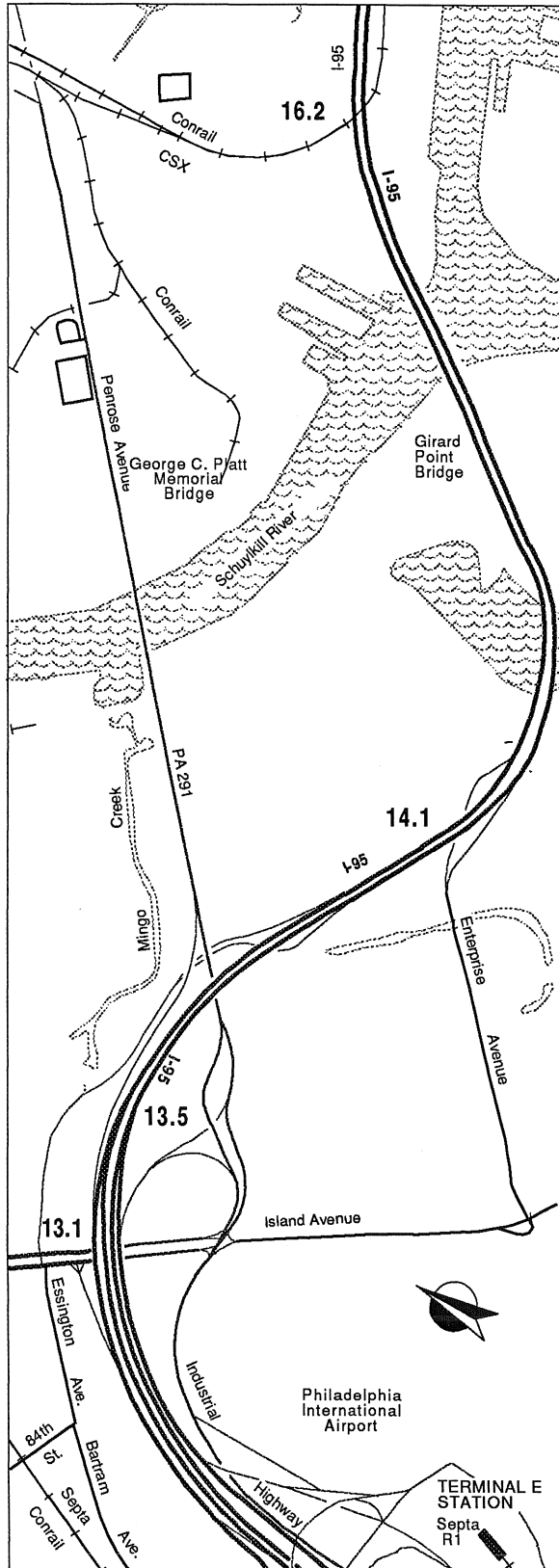
Airport Exit Ramp to I-95 South

Distance Between Piers: 85', 65'





INTERSTATE 95



Map 5

Design Features

Scale: 1" = 2000'

Note: Four travel lanes by direction. ROW varies between 210' and 250'.

13.1 Dual Bridge

over Island Avenue

Span Length: N/A

Deck Width: 150' each

13.5 - 14.1 Dual Elevated Section

Span Length: 3200' each

Deck Width: 70'

14.1 - 16.2 Girard Point Bridge (single span/dual span)

Over Schuylkill River

Span Length: 11,000'

Deck Width: 70' each roadway





INTERSTATE 95

Map 6

Design Features

Scale: 1" = 2000'

Note: Three travel lanes by direction. Median varies between 13' and 40'. ROW varies between 150' and 327'.

16.2 - 18.1 Elevated Section

Span Length: 10,000'

Deck Width: 110'

18.5 Overpass

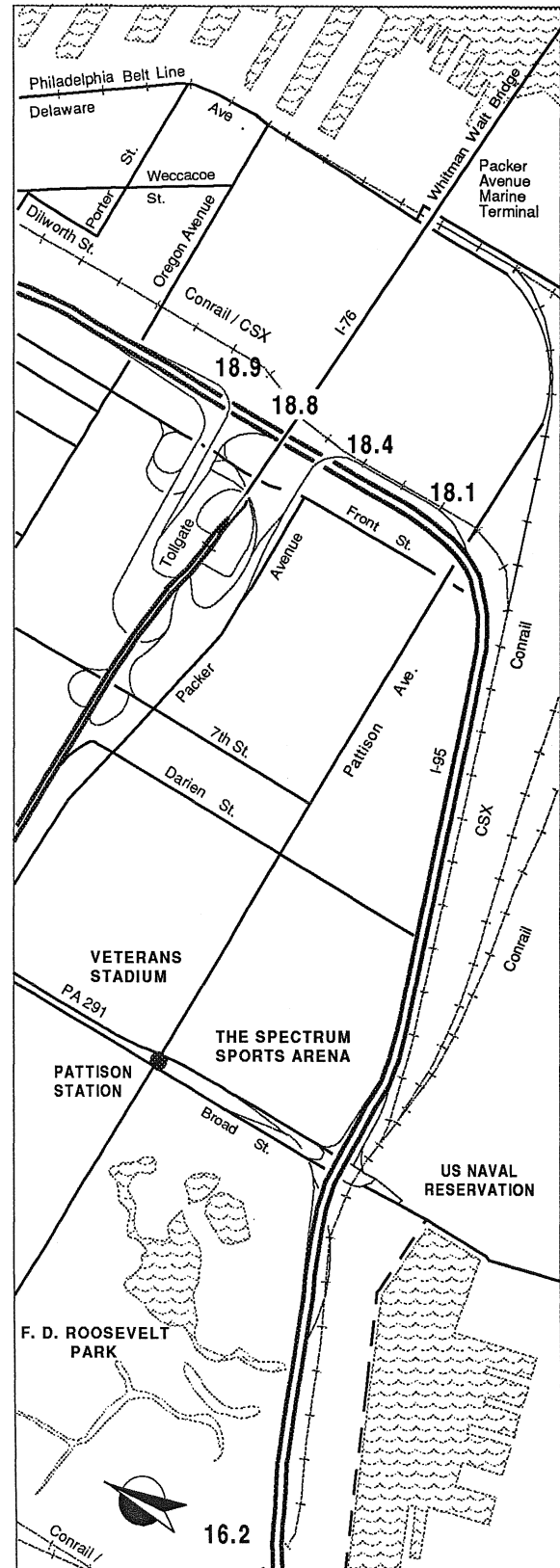
Walt Whitman Bridge

Distance Between Piers: 70', 70'

18.9 - 20.4 Elevated Section

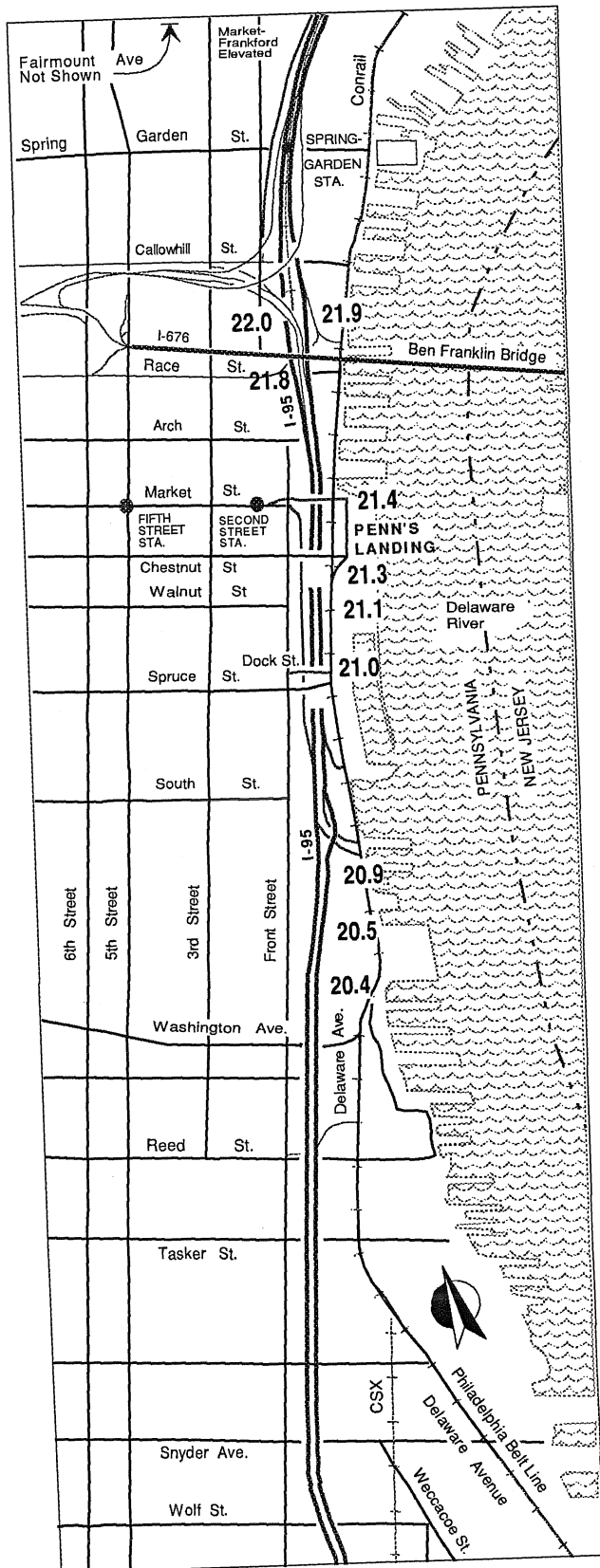
I-76 Westbound to I-95 Northbound ramp

Distance Between Piers: 80', 65'





INTERSTATE 95



Map 7 Design Features

Scale: 1" = 2000'

Note: Three travel lanes by direction. Median 25' entire length. ROW varies between 210' and 260'.

18.9 - 20.4 Elevated Section
I-76 Westbound to I-95
Northbound ramp Distance
Between Piers: 80', 65'

20.5 Bridge
over Queen Street
Span Length: 50'
Deck Width: 145'

20.9 - 21.3 Depressed Section
Length: 2000' approx.

21.0 Overpass
Dock Street, Tunnel
Distance Between Piers: 25',
70', 70', 25'

21.1 Overpass
Walnut Street SR 3006
Distance Between Piers: 25',
70', 100'

21.3 Overpass
Chestnut Street SR 3008,
Tunnel
Distance Between Piers: 30',
70', 70', 30'

21.4 Overpass
Market Street SR 2004
Distance Between Piers:
110', 110'

21.8 Overpass
Ben Franklin Bridge US 30
Distance Between Piers: 140',
155'

21.9 Overpass (SB)
I-95 Northbound to I-676
Distance Between Piers: 50'

22.0 Overpass
I-676 to I-95 Northbound
Distance Between Piers: 50'

22.1 Dual Bridge
over Spring Garden Street
SR 2006 LR 67030
Span Length: 125' each
Deck Width: SB- 60' NB- 80'

22.1 Dual Bridge
over Fairmount Avenue
Span Length: 55' each
Deck Width: 70' each

22.2 Dual Bridge
over Brown Street
Span Length: 55' each
Deck Width: SB- 65' NB- 70'

22.3 Dual Bridge
over Poplar Street
Span Length: 35' each
Deck Width: SB- 90' NB- 70'





INTERSTATE 95

Map 8

Design Features

Scale: 1" = 2000'

Note: Three travel lanes by direction. Median varies between 4' and 20'. ROW varies between 140' and 300'.

22.4 - 22.7 Elevated Section

Dual Bridge

Span Length: 1620' each

Deck Width: 70' each

22.7 Bridge

over Shackamaxon Street

Span Length: 60'

Deck Width: 135'

22.8 Bridge

over Marlborough Street

Span Length: 45'

Deck Width: 135'

22.9 Bridge

over Columbia Avenue SR

2010 LR 67037

Span Length: 50'

Deck Width: 125'

22.9 - 24.4 Elevated Section

Span Length: 7200'

Deck Width: 115'

24.8 Bridge

over Allegheny Avenue SR

2016 LR 67288

Span Length: 111'

Deck Width: 115'

24.9 Bridge

over Westmoreland Street

Span Length: 100'

Deck Width: 150'

25.0 Bridge

over Wensley Street

Span Length: 50'

Deck Width: 125'

25.1 Bridge

over Ontario Street

Span Length: 60'

Deck Width: 130'

25.2 Bridge

over Schiller Street

Span Length: 50'

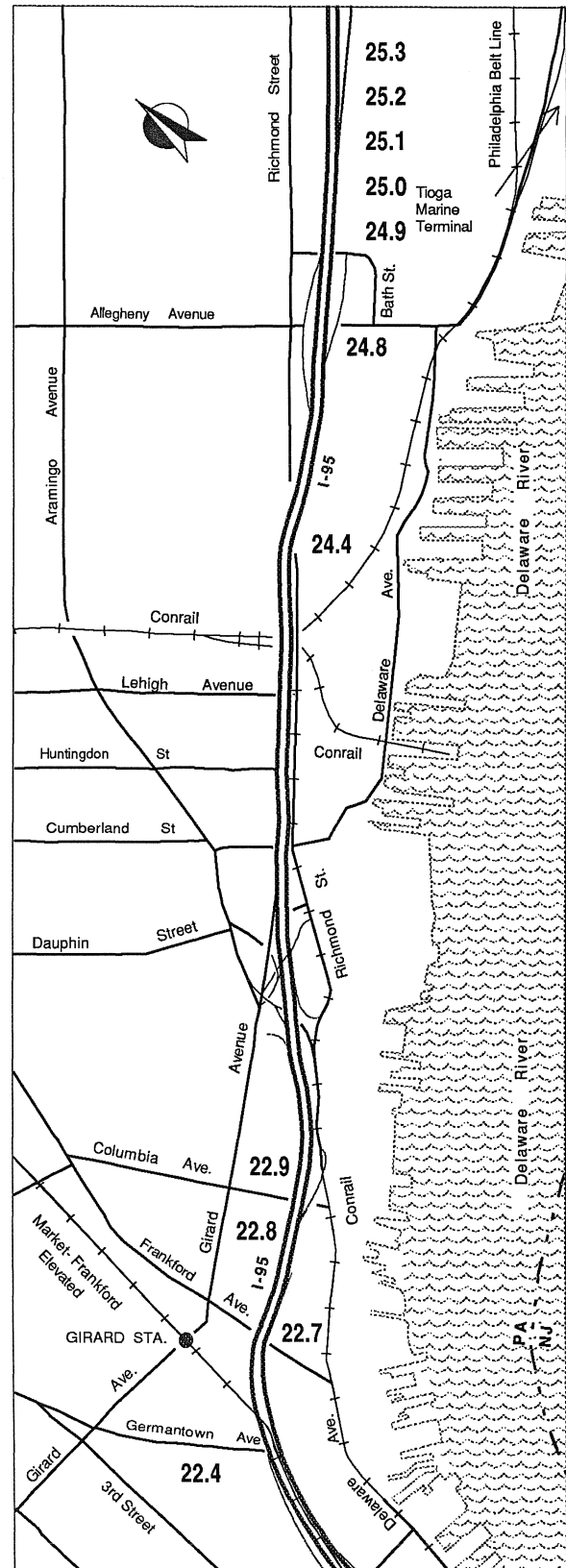
Deck Width: 135'

25.3 Bridge

over Tioga Street

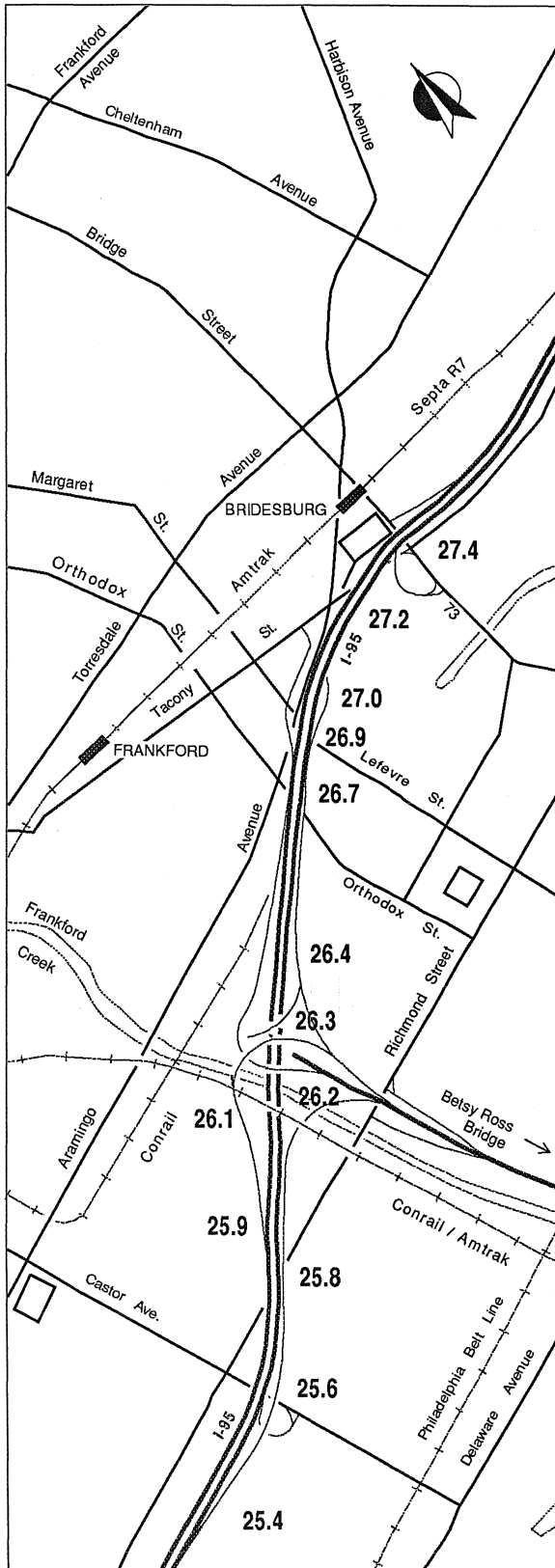
Span Length: 60'

Deck Width: 140'





INTERSTATE 95



Map 9

Design Features

Scale: 1" = 2000'

Note: Three travel lanes by direction. Median varies between 8' and 4'. ROW varies between 150' and 300'.

25.4 Bridge
over Venango Street
Span Length: 60'
Deck Width: 125'

25.6 Bridge
over Castor Avenue
SR 1005 LR 67288
Span Length: 80'
Deck Width: 150'

25.8 Bridge
over Richmond Avenue SR
2001 LR 67348
Span Length: 135'
Deck Width: 135'

25.9 Bridge
over Wheatsheaf Lane
Span Length: 95'
Deck Width: 140'

26.1 Overpass
Amtrak Delair Branch
Distance Between Piers: 70',
60'

26.2 Bridge
over Frankford Creek
Span Length: 200'
Deck Width: 120'

26.3 Overpass
Betsey Ross Bridge ramp to
Southbound I-95
Distance Between Piers: 75',
95'

26.3 Overpass
Ramp Southbound I-95 to
Betsey Ross Bridge
Distance Between Piers: 75',
75'

26.4 - 26.7 Elevated
Section Span Length: 1600'
approx.
Deck Width: 145'
Distance Between Piers: 95',
95'

26.9 Bridge
over Margaret Street
and Bermuda Street
Span Length: 200'
Deck Width: 110'

27.0 Bridge
over Exit 18 Ramp
Span Length: 125'
Deck Width: 100'

27.2 - 27.4 Elevated
Section Span Length: 1000'
approx.
Deck Width: 95'





INTERSTATE 95

Map 10

Design Features

Scale: 1" = 2000'

Note: Four travel lanes taper to three travel lanes by direction. Median varies between 10' and 20'. ROW varies between 165' and 220'.

27.6 Bridge

over Fraley Street
Span Length: 55'
Deck Width: 140'

27.7 Bridge

over Service Road
Span Length: 110'
Deck Width: 135'

28.0 Bridge

over Vankirk Street
Span Length: 110'
Deck Width: 135'

28.1 Bridge

over Comly Street
Span Length: 110'
Deck Width: 135'

28.7 Bridge

over Levick Street
Span Length: 135'
Deck Width: 135'

28.9 Bridge

over Magee Avenue
Span Length: 115'
Deck Width: 135'

29.0 Bridge

over Unruh Avenue
Span Length: 130'
Deck Width: 135'

29.2 Bridge

over Longshore Avenue
Span Length: 120'
Deck Width: 135'

29.3 Bridge

over New State Road
SR 0073 LR 67350 and Conrail
Span Length: 370'
Deck Width: 135'

29.5 Bridge

over Princeton Avenue
SR 8023
Span Length: 160'
Deck Width: 125'

29.7 Bridge

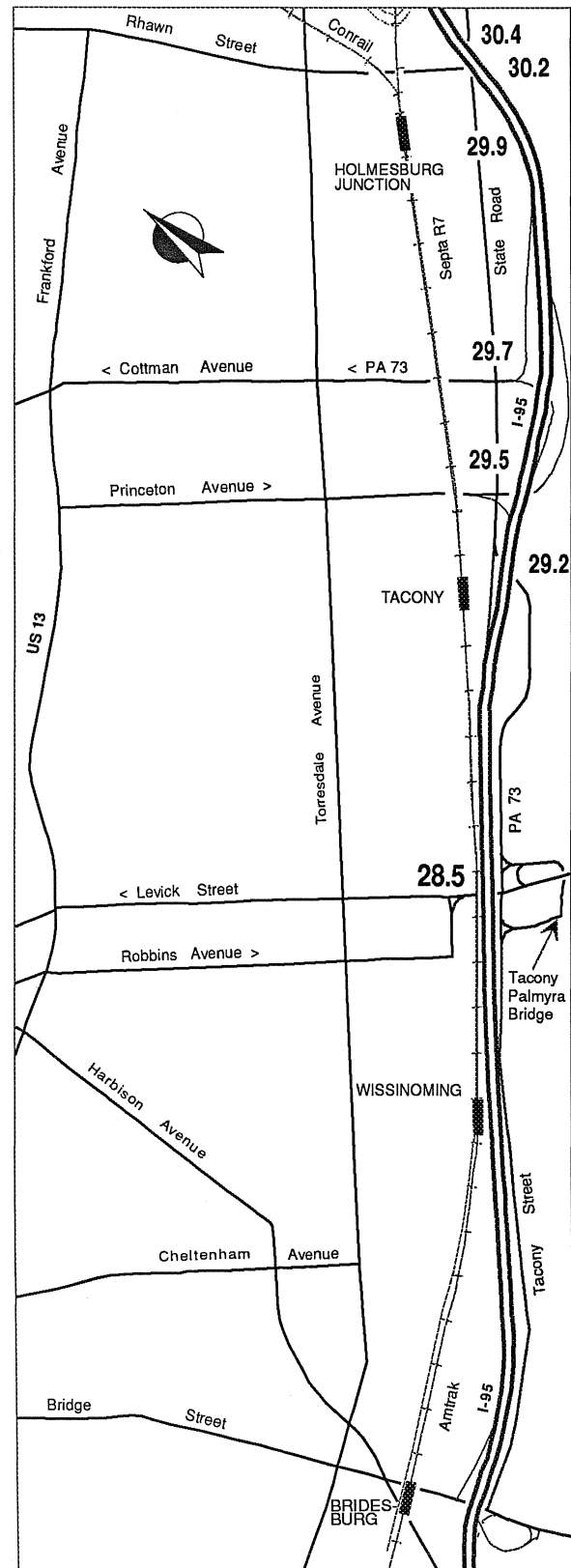
over Cottman Avenue PA 73
SR 0073 LR 67293
Span Length: 140'
Deck Width: 115'

29.8 Bridge

over Bleigh Avenue
Span Length: 195'
Deck Width: 125'

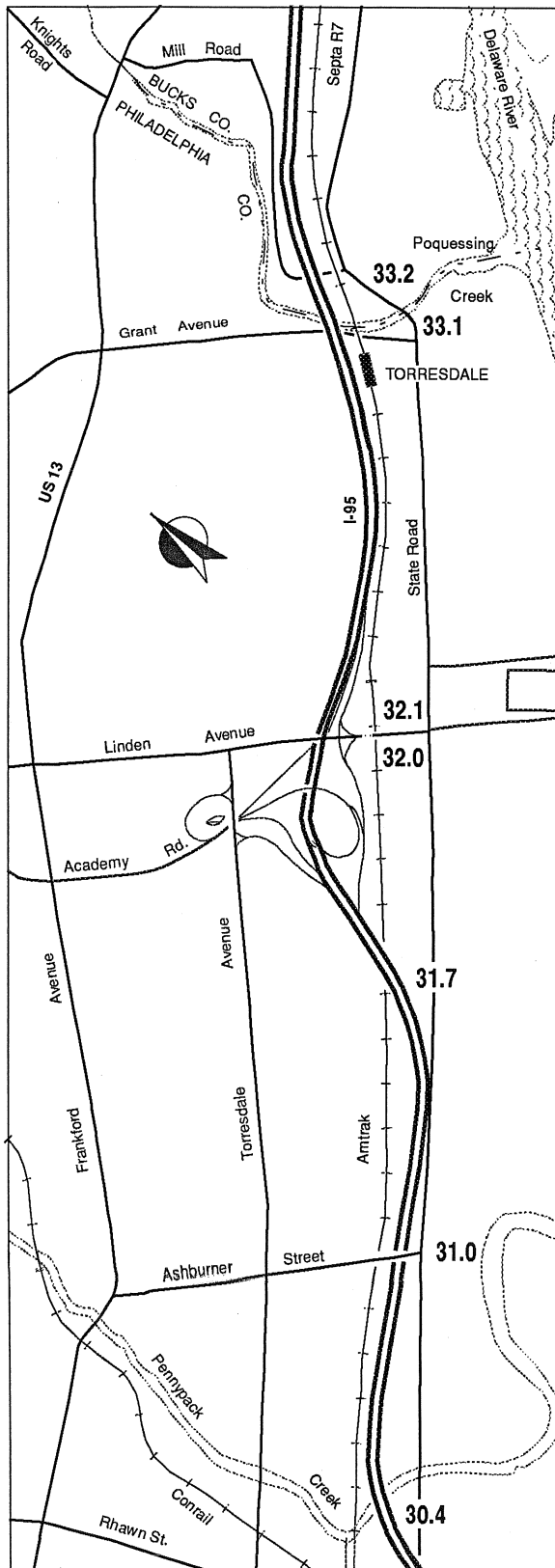
29.8 - 30.2 Elevated Section

Span Length: 2075'
Deck Width: 120'





INTERSTATE 95



Map 11

Design Features

Scale: 1" = 2000'

Note: Three travel lanes widen to four travel lanes by direction south of Academy Road. Median varies between 10' and 60'. ROW varies between 200' and 260'.

30.4 Dual Bridge

over Pennypack Creek
Span Length: 430' each
Deck Width: 56' each

31.0 Overpass

Ashburner Avenue
Distance Between Piers: 70', 70'

31.7 Dual Bridge

over AMTRAK
Span Length: SB- 450' NB- 380'
Deck Width: 55' each

32.0 Overpass

Academy Road SR 8027 LR 1032
Distance Between Piers: 75', 60'

32.1 Overpass

Linden Avenue SR 1016 LR 67295
Distance Between Piers: 95', 75'

33.1 Dual Bridge

over Poquessing Creek
and Grant Avenue SR 1018 LR 67357
Span Length: SB- 375' NB- 310'
Deck Width: 45' each

33.2 Dual Bridge

over Mill Road 2001 LR 09009
Span Length: 90' each
Deck Width: 50' each





INTERSTATE 95

Map 12

Design Features

Scale: 1" = 2000'

Note: Three travel lanes by direction. 60' Median. ROW varies between 300' and 250' south.

34.1 Overpass

Tennis Avenue

Distance Between Piers: 25', 70', 70', 40'

34.8 Overpass

Woodhaven Road PA 63 SR 0063 LR 1029

Distance Between Piers: 25', 75', 75', 25'

35.3 Dual Bridge

over Station Avenue SR 2005 LR 09002

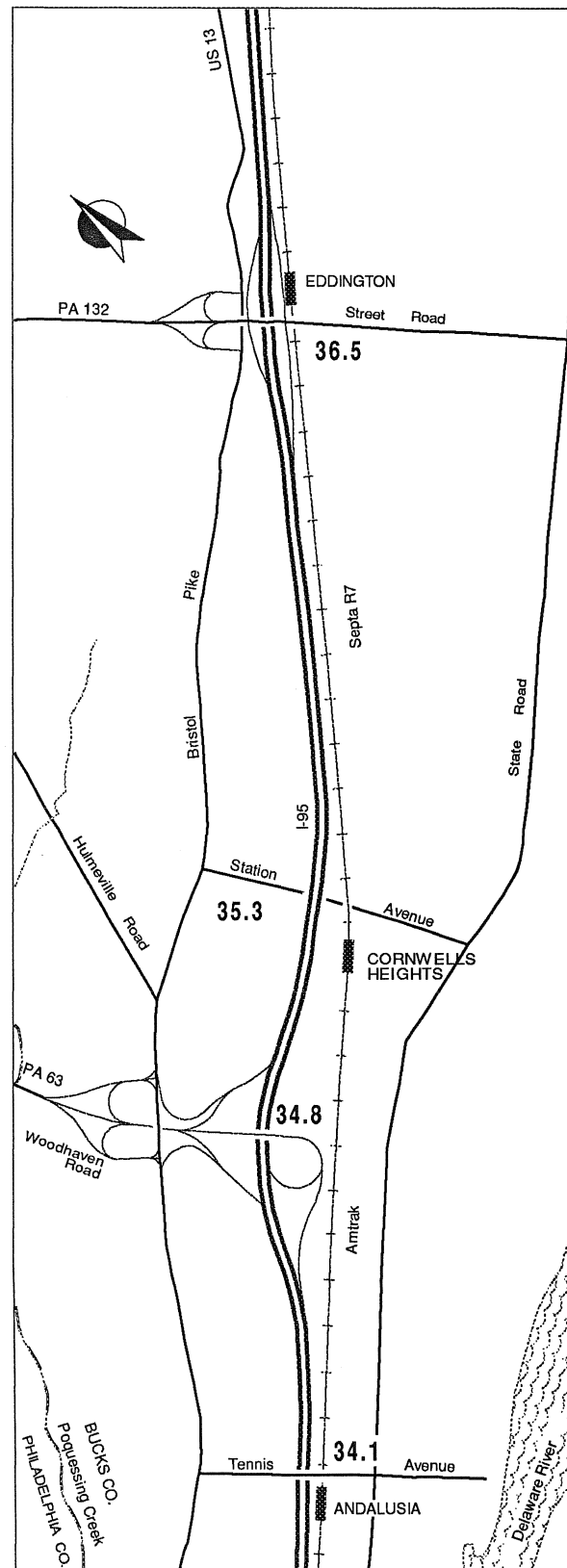
Span Length: 50' each

Deck Width: 50' each

36.5 Overpass

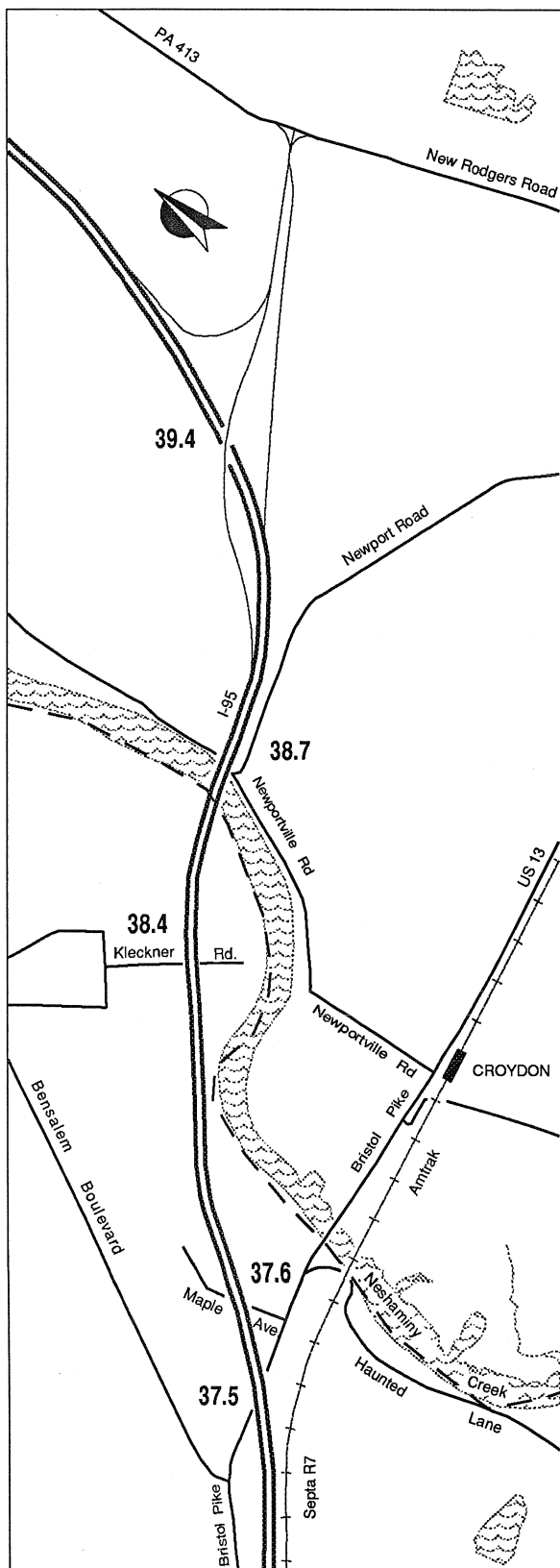
Street Road PA 132 SR 0132 LR 252

Distance Between Piers: 25', 65', 65', 25'





INTERSTATE 95



Map 13

Design Features

Scale: 1" = 2000'

Note: Two travel lane widens to three travel lanes by direction south of 413. Median 60' entire length. ROW varies between 200' and 250'.

37.5 Dual Bridge

over Bristol Pike U.S. 13 SR0113 LR150
Span Length: SB- 290' NB- 280'
Deck Width: 45' each

37.6 Dual Bridge

over Maple Avenue
Span Length: 150' each
Deck Width: 50' each

38.4 Dual Bridge

over Kleckner Road
Span Length: 40' each
Deck Width: 50' each

38.7 Dual Bridge

over Neshaminy Creek
and Newportville Road SR2027 LRA276
Span Length: SB- 650' NB- 655'
Deck Width: 45' each

39.4 Overpass

Southbound On-Ramp
Distance Between Piers: SB- 25', 75'
NB- 75', 25'





INTERSTATE 95

Map 14

Design Features

Scale: 1" = 2000'

Note: Two travel lanes by direction. Median 60' entire length. ROW varies between 260' and 290'.

40.3 Overpass

Ford Road

Distance Between Piers: 90'

40.7 Dual Bridge

over PA Turnpike I-276

Span Length: 230' each

Deck Width: 40' each

40.8 Dual Bridge

over New Falls Road SR2006 LR09012

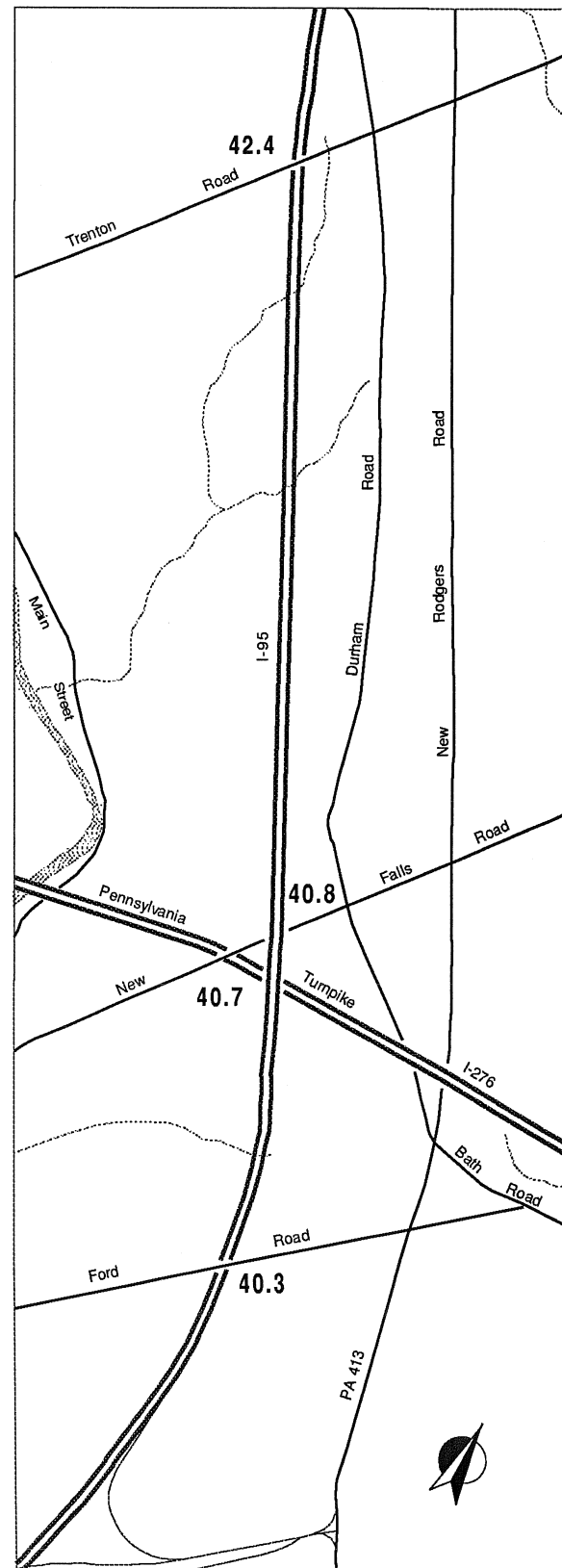
Span Length: SB- 155' NB- 165'

Deck Width: 40' each

42.4 Overpass

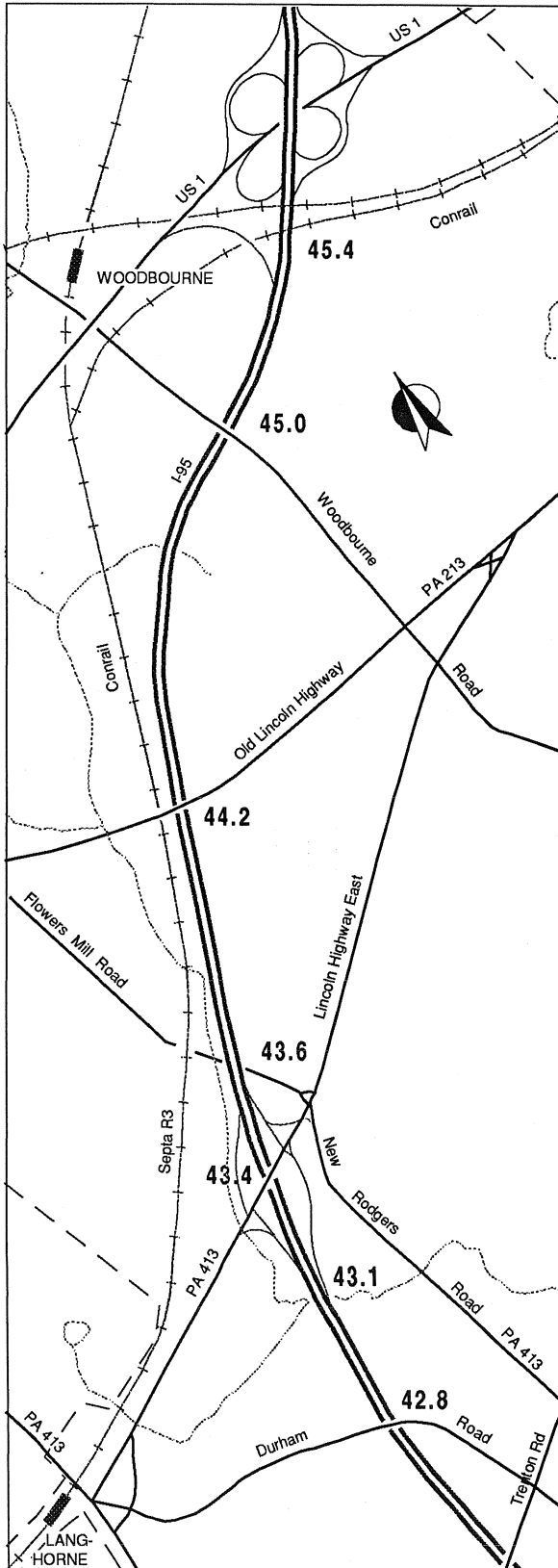
Trenton Road SR2018 LRA48

Distance Between Piers: 80'





INTERSTATE 95



Map 15

Design Features

Scale: 1" = 2000'

Note: Two travel lanes by direction. Median 60' entire length. ROW varies between 320' and 180' at railroad crossing.

42.8 Overpass

Durham Road SR 2049 LR 9172
Distance Between Piers: 90'

43.1 Dual Bridge

over Mill Creek
Span Length: SB- 60'
Deck Width: SB- 55'

43.4 Overpass

Lincoln Highway U.S. 1 SR 0001 LR 281
Distance Between Piers: 90'

43.6 Dual Bridge

over Flowers Mill Road
Span Length: 130' each
Deck Width: 50' each

44.2 Overpass

Old Lincoln Highway PA 213 SR 0213 LR 281-SP
Distance Between Piers: SB- 70' NB-75'

45.0 Overpass

Woodbourne Road SR 2033 LR A-210
Distance Between Piers: 90'

45.4 Dual Bridge

Two Conrail Lines
Span Length: SB- 405' NB- 390'
Deck Width: SB- 40' NB- 50'





INTERSTATE 95

Map 16

Design Features

Scale: 1" = 2000'

Note: Two travel lanes by direction. Median 60' entire length.
ROW varies between 260' and 310'

45.6 Dual Bridge

over U.S. 1 SR 2068 LR 281-PAR

Span Length: 225' each

Deck Width: 45' each

46.1 Overpass

Big Oak Road

Distance Between Piers: 80'

46.5 Dual Bridge

over Conrail-SEPTA

Span Length: 210' each

Deck Width: 40' each

47.6 Dual Bridge

over Langhorne Yardley Road

SR 2049 LR 252 Spur

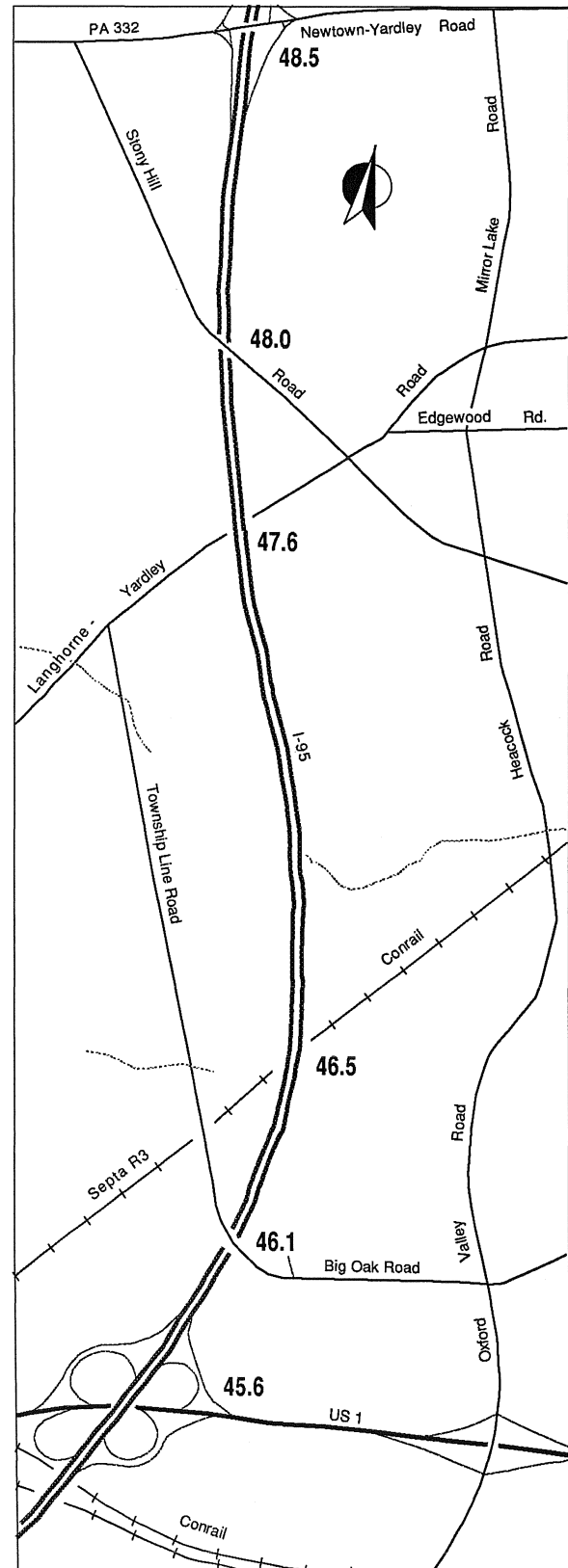
Span Length: 120' each

Deck Width: 40' each

48.0 Overpass

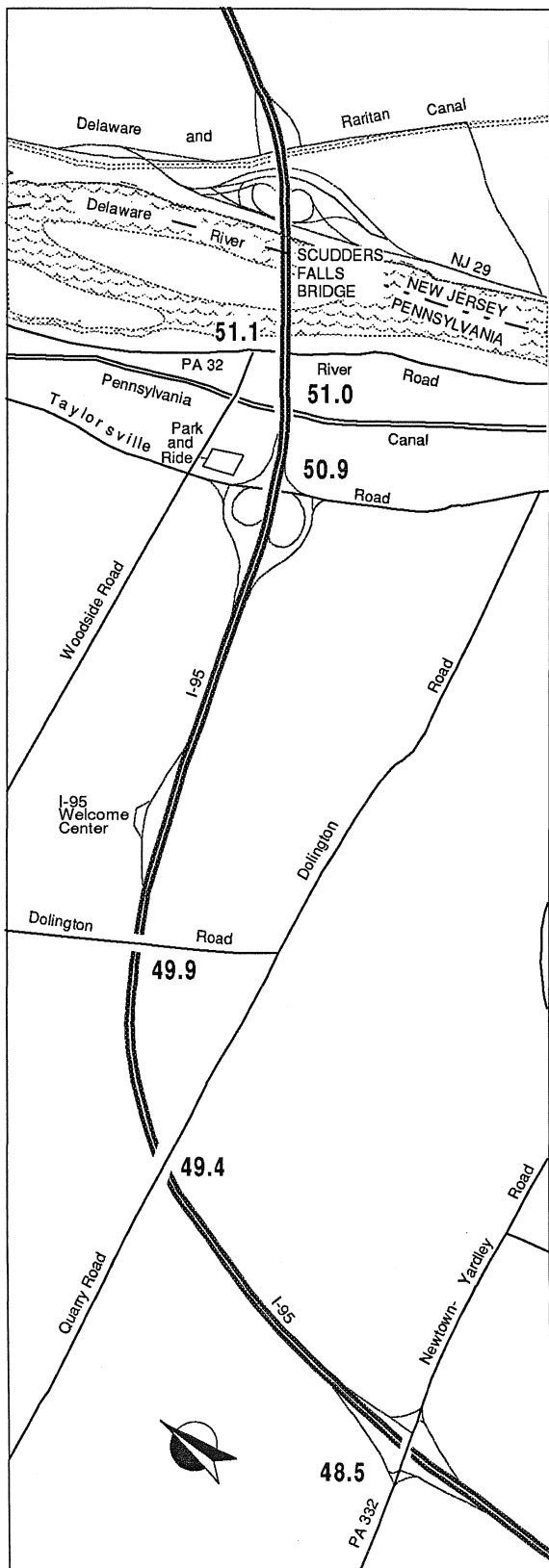
Stony Hill Road SR 2069 LR 09021

Distance Between Piers: 80'





INTERSTATE 95



Map 17

Design Features

Scale: 1" = 2000'

Note: Two travel lanes by direction. 4' Median on Scudder Falls Bridge and widens to 60' south of 49.9. ROW varies between 260' and 310'.

48.5 Overpass

Newtown-Yardley Road PA 332 SR 0332 LR 252
Distance Between Piers: 90'

49.4 Overpass

Quarry Road
Distance Between Piers: 80'

49.9 Overpass

Dolington Road SR 2075 LR 09025
Structure Length: 80'
Expressway Width: 80'

50.9 Bridge

over Taylorsville Road SR 2071 LR 09151
Span Length: 130' Long
Deck Width: 80' Wide

51.0 Bridge

over Pennsylvania Canal
Span Length: 70' Long
Deck Width: 70' Wide

51.3 Scudder Falls Bridge (State Line)

Span Length: 1700' Long
Deck Width: 65' Wide



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ADJACENT LAND USE

The adjacent land use maps identify the land use on parcels immediately adjacent to Interstate Route 95's right of way. The purpose of this inventory is to highlight land use considerations which may impact, positively or negatively, upon large-scale roadway modifications.

Nine different land use categories are used to designate adjacent land uses. These categories are general in nature so as to facilitate their use, but sufficiently specific to allow for a meaningful designation of land use. The nine categories are:

- Agricultural
- Cemetery
- Commercial/Industrial
- Institutional
- Parkland
- Residential
- Transportation
- Vacant (parcels over 10 acres identified as such)
- Water

Adjacent land uses are identified on the map along both sides of the highway. The land use noted at the top of each map extends toward the bottom of the page until a different land use is cited under a horizontal line.

Special conditions and anecdotal information that may provide additional insight into the nature of adjacent parcels is provided where appropriate. The presence of special structures, including the proper names of identifiable places located adjacent to the roadway is also noted.

Information presented on this map was assembled from DVRPC in-house data, including aerial photographs, and field views.



INTERSTATE 95

Commercial/Industrial

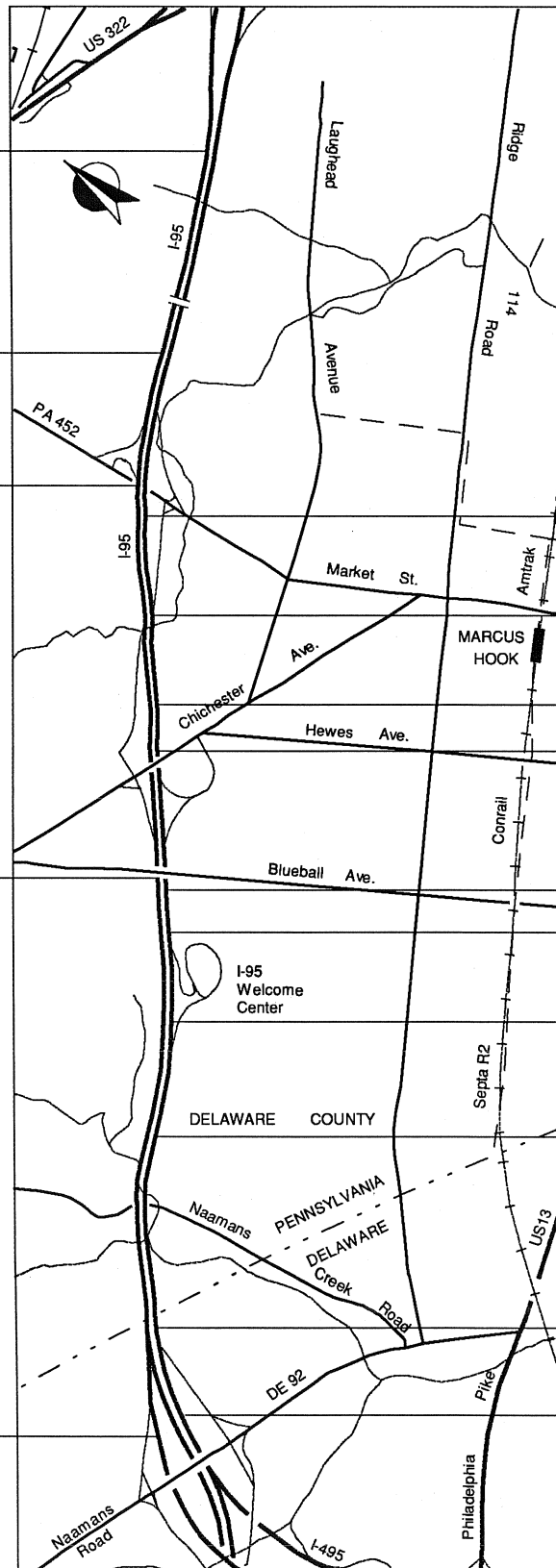
Vacant

Commercial/Industrial

Residential

Vacant
(10 or more contiguous acres)

Commercial/Industrial



Map 1

Adjacent Land Use

Scale: 1" = 2000'

Vacant
(10 or more contiguous acres)

Commercial/Industrial

Vacant

Residential

Vacant
(10 or more contiguous acres)

Commercial/Industrial

Vacant

Cemetery
(Lawn Croft)

Vacant
(10 or more contiguous acres)

Commercial/Industrial

Vacant
(10 or more contiguous acres)





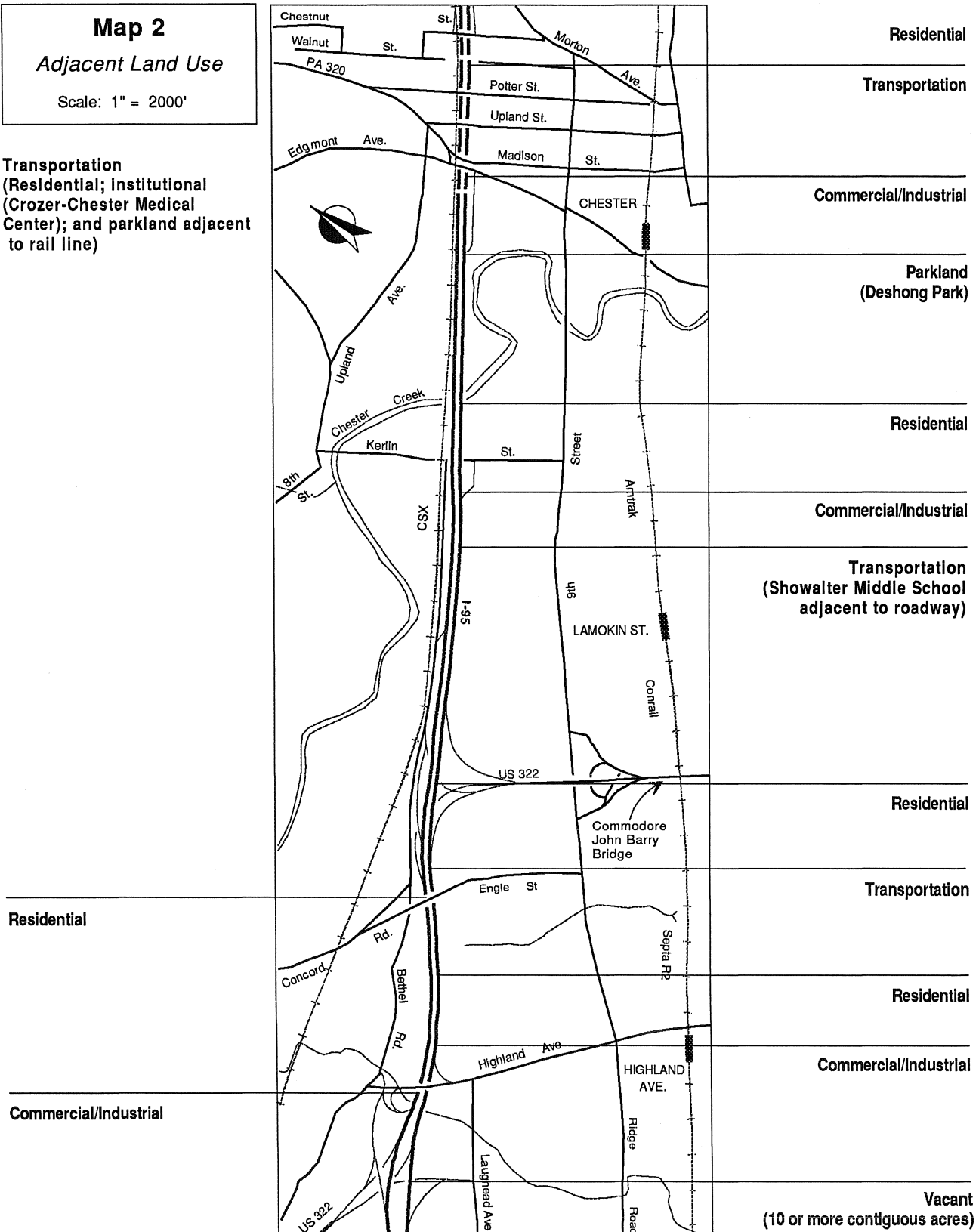
INTERSTATE 95

Map 2

Adjacent Land Use

Scale: 1" = 2000'

Transportation
(Residential; Institutional
(Crozer-Chester Medical
Center); and parkland adjacent
to rail line)





INTERSTATE 95

Parkland

Residential

Commercial/Industrial

Parkland

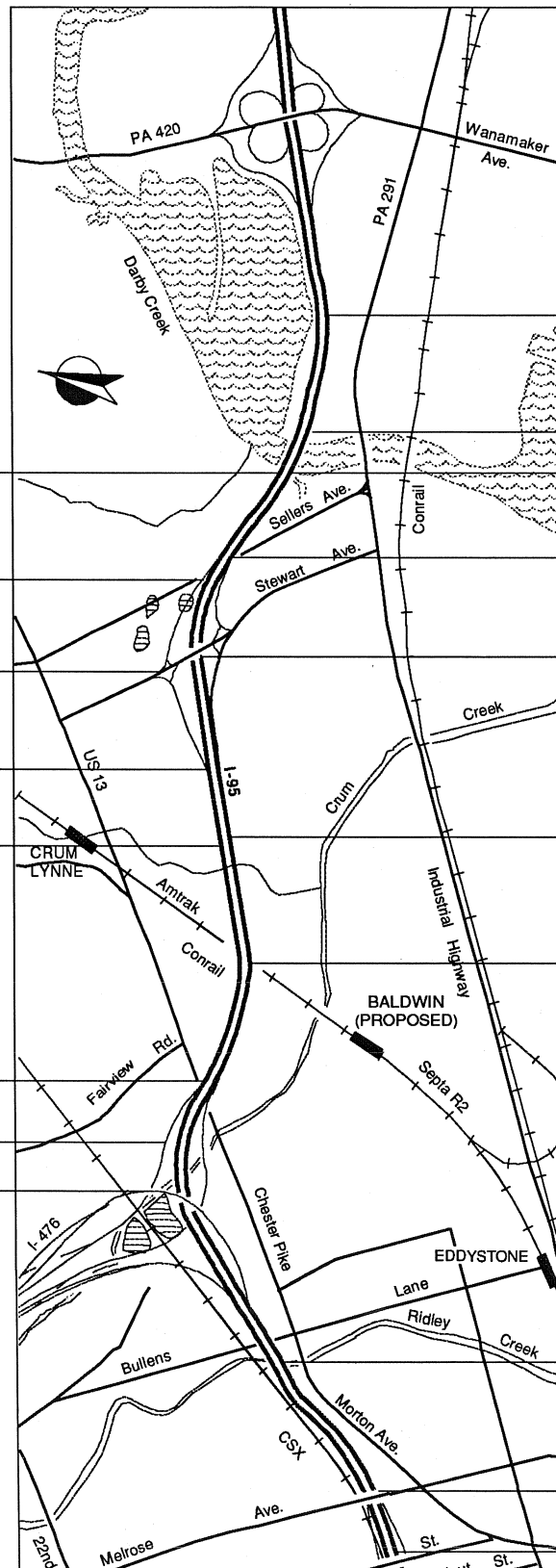
Vacant
(owned by Boeing Helicopter)

Commercial/Industrial

Residential

Commercial/Industrial

Transportation
(Widener University adjacent
to rail line)



Map 3
Adjacent Land Use

Scale: 1" = 2000'

Commercial/Industrial

Vacant
(10 or more contiguous acres)

Water

Commercial/Industrial

Vacant

Commercial/Industrial
(Boeing Helicopter)

Vacant
(10 or more contiguous acres)

Commercial/Industrial

Residential

Commercial/Industrial

Residential





INTERSTATE 95

Map 4

Adjacent Land Use

Scale: 1" = 2000'

Vacant
(10 or more contiguous acres)

Parkland

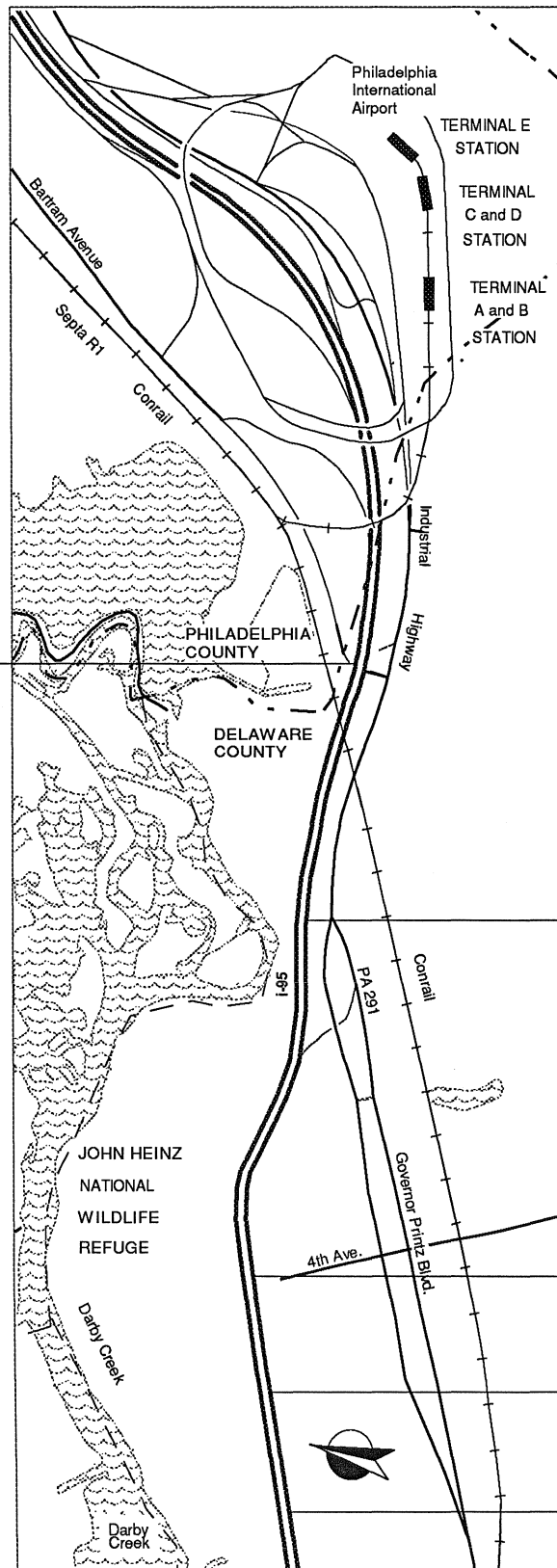
Transportation

Commercial/Industrial
(Airport Business Center)

Residential

Commercial/Industrial

Vacant
(10 or more contiguous acres)





INTERSTATE 95

Parkland
(FDR Park and golf course)

Commercial/Industrial

Water

Vacant
(10 or more contiguous acres)

Commercial/Industrial

Transportation

Vacant
(10 or more contiguous acres)

Map 5 Adjacent Land Use

Scale: 1" = 2000'

Commercial/Industrial

Water

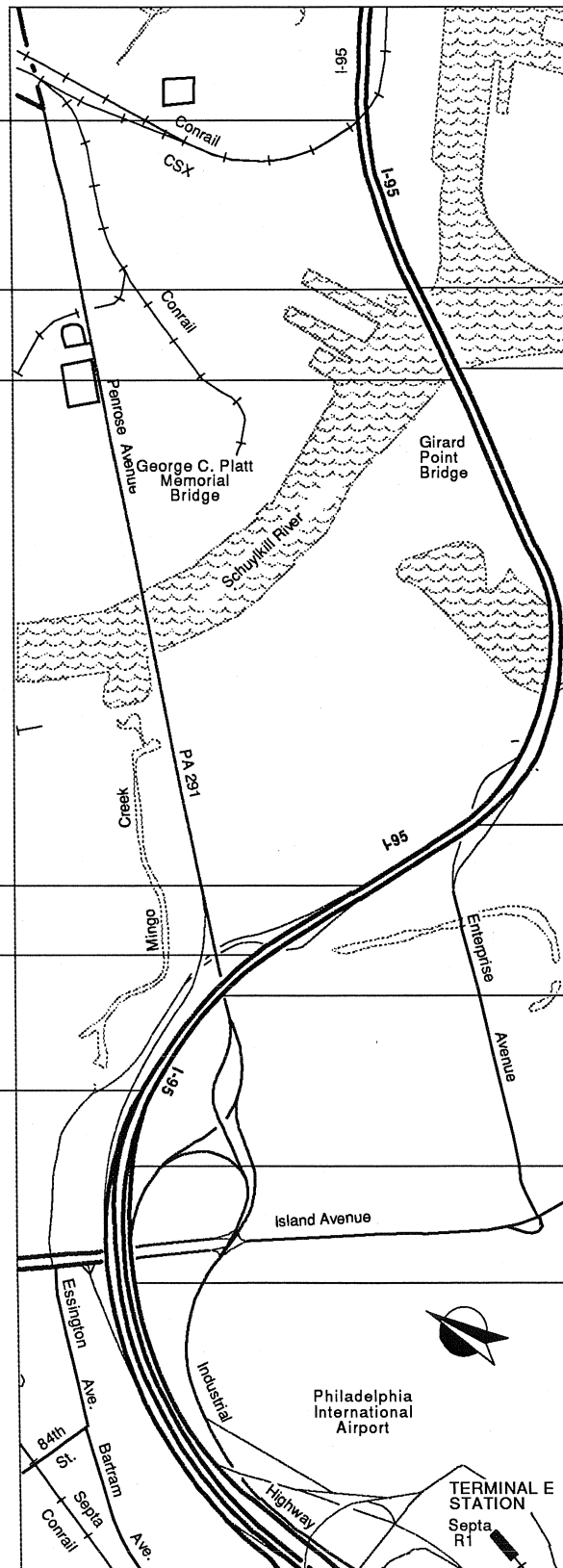
Vacant
(10 or more contiguous acres)

Commercial/Industrial
(Penrose Industrial Park)

Vacant

Commercial/Industrial

Transportation





INTERSTATE 95

Map 6

Adjacent Land Use

Scale: 1" = 2000'

Commercial/Industrial
(Food Distribution Center)

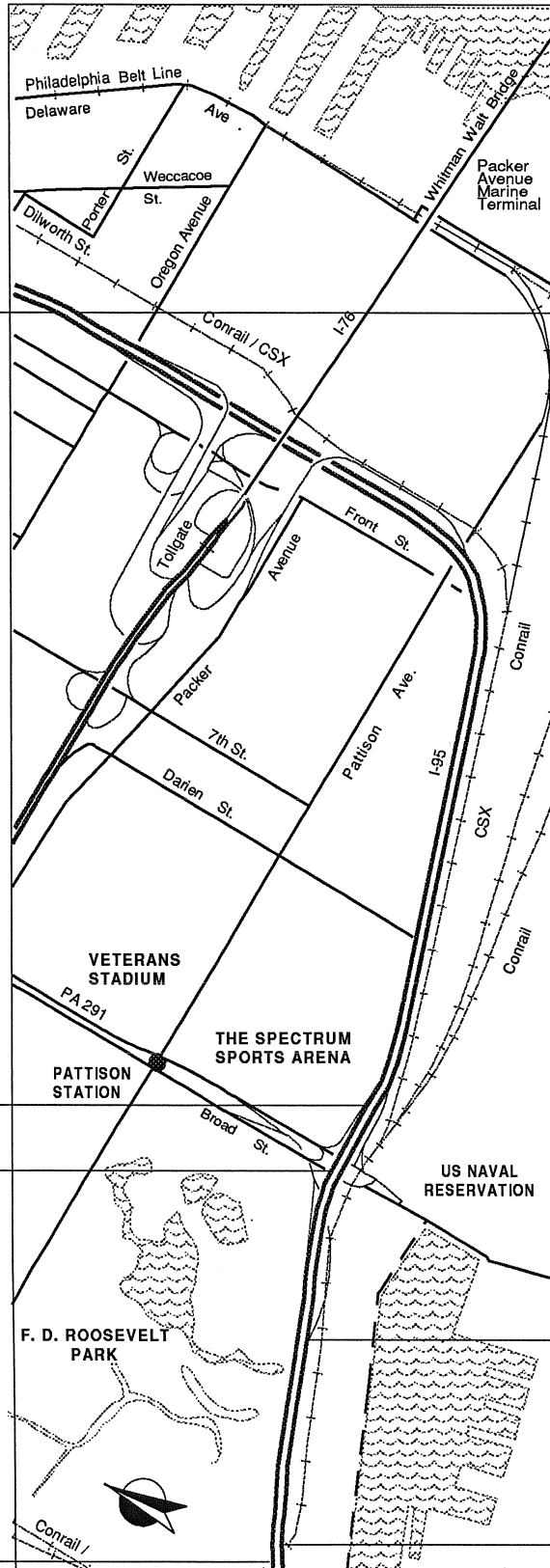
Transportation

Transportation
(JFK Stadium adjacent to road)

Parkland
(FDR Park and golf course)

Parkland

Commercial/Industrial





INTERSTATE 95

Commercial/Industrial

Vacant

Transportation

Vacant
(Queen Village west of Front)

Transportation

Residential

Map 7

Adjacent Land Use

Scale: 1" = 2000'

Commercial/Industrial

Transportation

Vacant

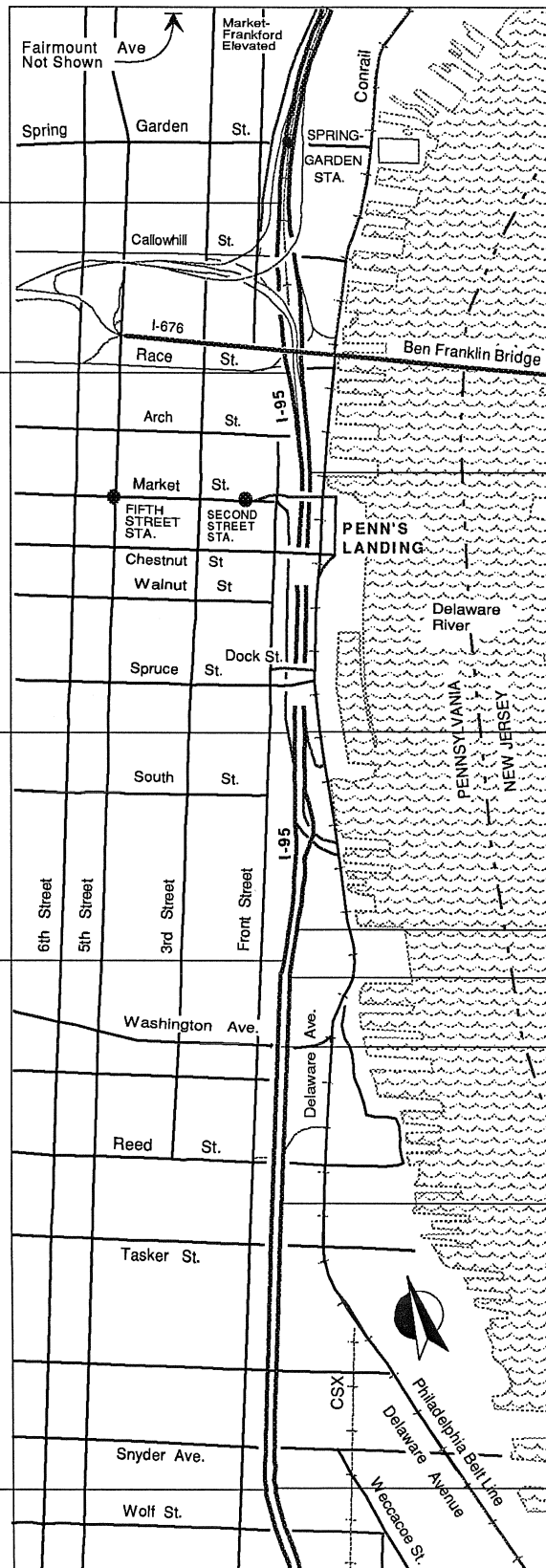
Commercial/Industrial

Transportation

Commercial/Industrial

Transportation

Commercial/Industrial





INTERSTATE 95

Map 8
Adjacent Land Use
Scale: 1" = 2000'

Commercial/Industrial

Parkland

Residential

Commercial/Industrial

Transportation

Commercial/Industrial

Transportation

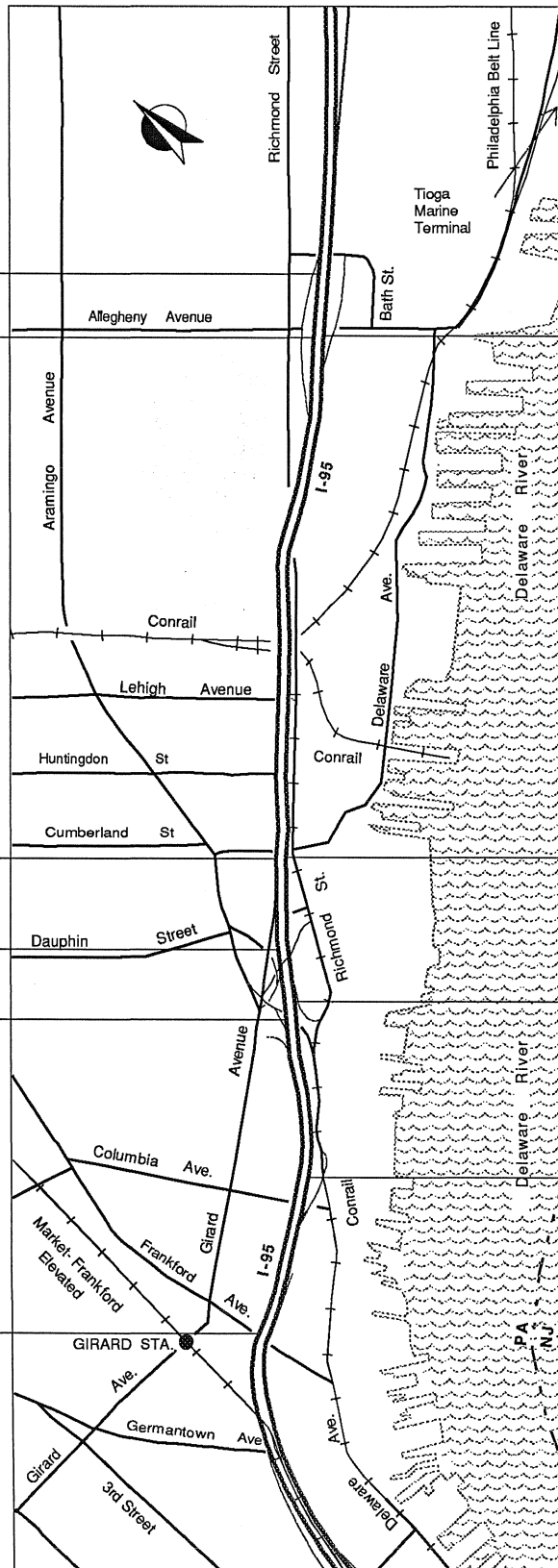
Residential

Commercial/Industrial

Transportation
(Riverside Industrial Park
adjacent to roads)

Commercial/Industrial

Commercial/Industrial





INTERSTATE 95

Residential

Transportation

Commercial/Industrial

Vacant
(10 or more contiguous acres)

Commercial/Industrial

Residential

Commercial/Industrial

Map 9

Adjacent Land Use

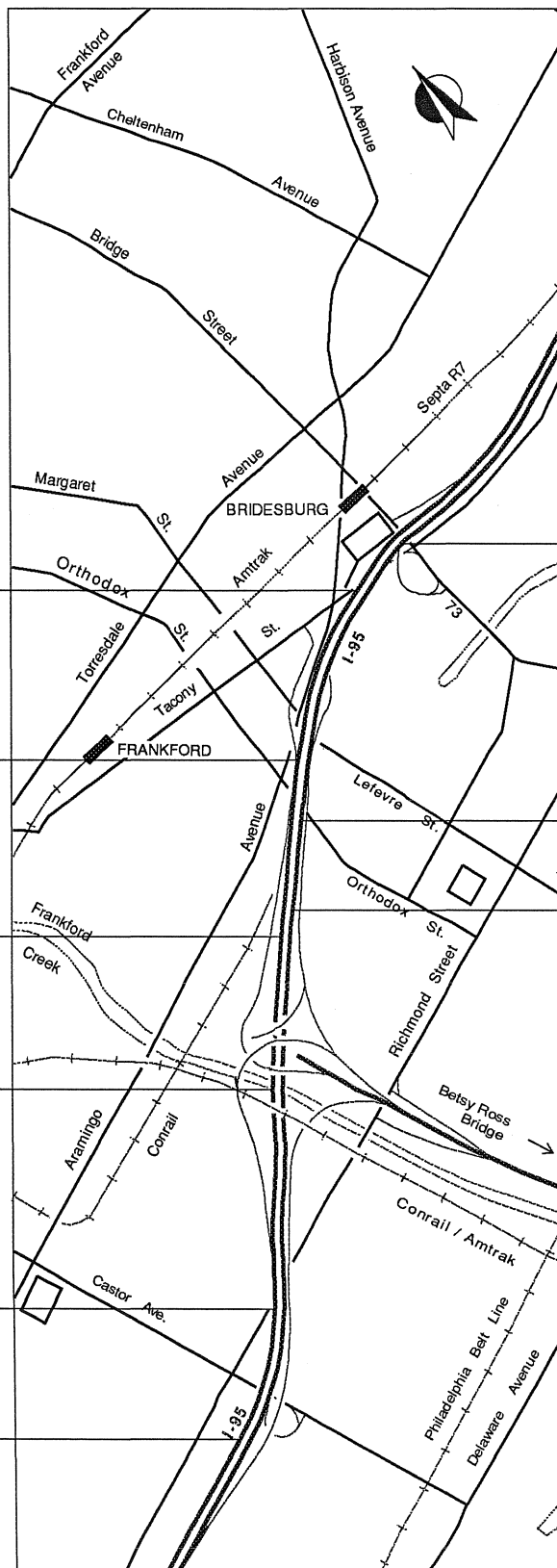
Scale: 1" = 2000'

Transportation
(Tacony Street)

Commercial/Industrial

Cemetery
(Most Holy Redeemer)

Commercial/Industrial





INTERSTATE 95

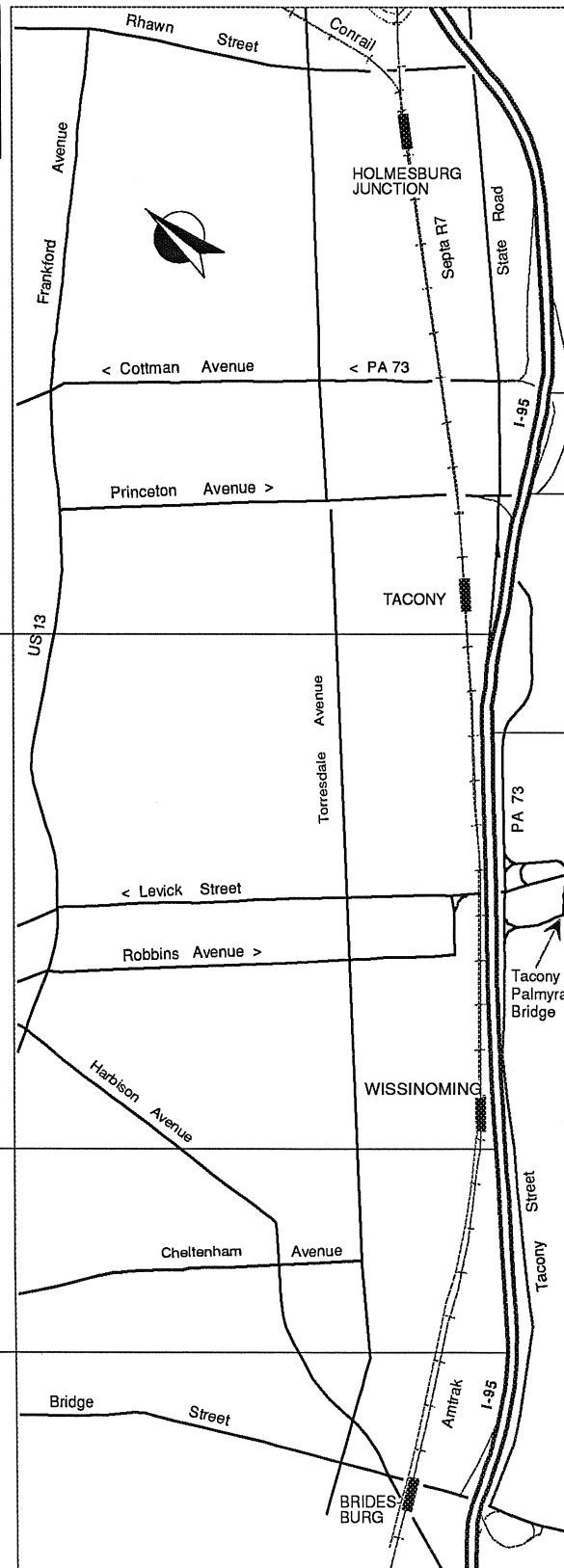
Map 10
Adjacent Land Use
Scale: 1" = 2000'

Commercial/Industrial

Transportation
(Disston Park
adjacent to rail line)

Commercial/Industrial

Residential



Commercial/Industrial

Vacant

Commercial/Industrial

Transportation
(Tacony Street)





INTERSTATE 95

Commercial/Industrial

Transportation

Vacant

Residential

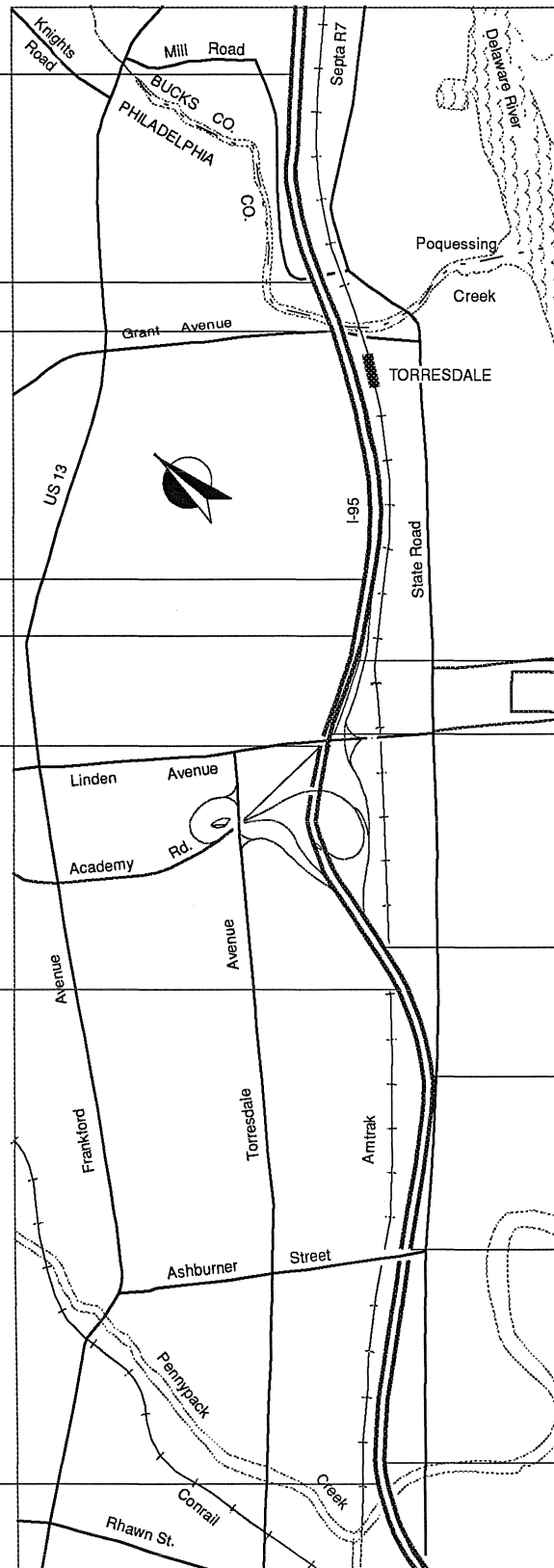
Vacant

Residential

Commercial/Industrial

Transportation
(Holmesburg Prison adjacent
to rail line)

Parkland
(Pennypack Park)



Map 11

Adjacent Land Use

Scale: 1" = 2000'

Transportation

Commercial/Industrial

Transportation

Commercial/Industrial

Transportation
(Philadelphia Police Academy)

Institutional
(Philadelphia County Prison)

Vacant





INTERSTATE 95

Map 12
Adjacent Land Use
Scale: 1" = 2000'

Residential

Commercial/Industrial

Transportation

Commercial/Industrial

Institutional
(Holy Ghost)

Residential

Institutional
(St. Elizabeth's Convent)

Commercial/Industrial

Transportation

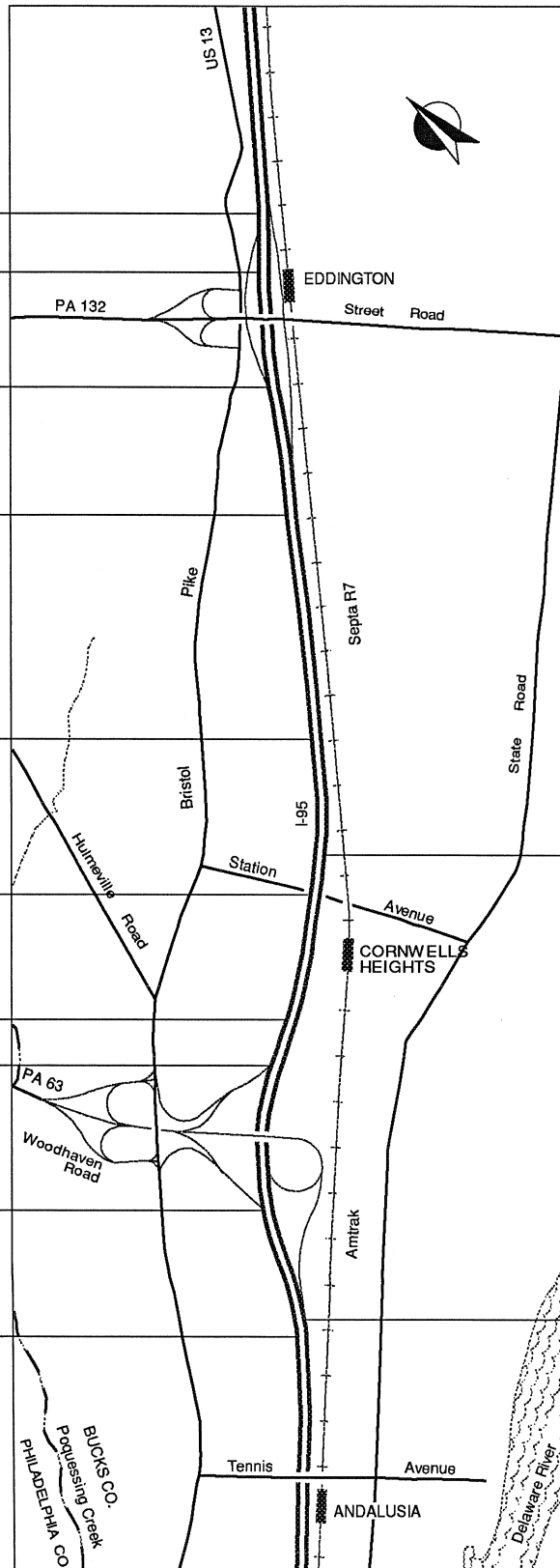
Commercial/Industrial

Residential

Transportation
(Expressway 95 Industrial
Park; Delaware Expressway
Industrial Park; Liberty
Bell Industrial Park and
Woodhaven Industrial Park
adjacent to rail line)

Vacant
(10 or more contiguous acres)

Transportation





INTERSTATE 95

Commercial/Industrial

Vacant
(10 or more contiguous acres)

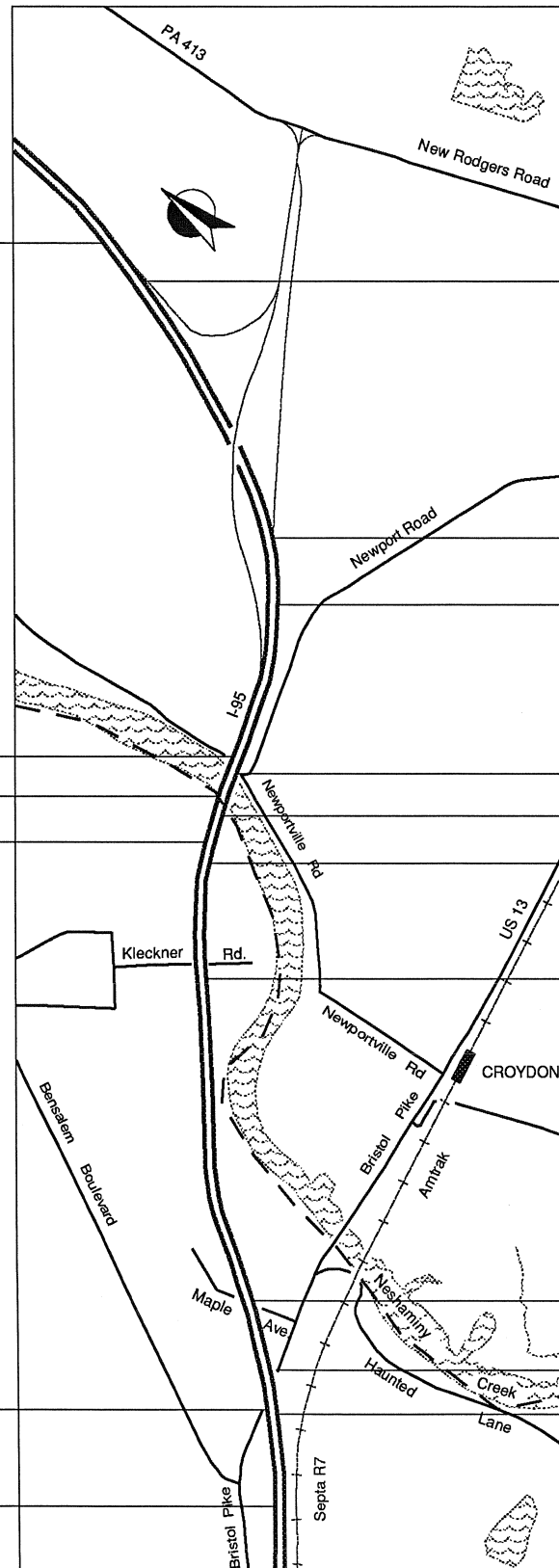
Water

Vacant

Residential

Commercial/Industrial

Residential



Map 13
Adjacent Land Use

Scale: 1" = 2000'

Commercial/Industrial

Transportation

Vacant

Transportation

Water

Vacant

Residential

Vacant

Residential

Commercial/Industrial

Transportation
(Expressway 95 Industrial Park;
Delaware Expressway Industrial
Park; Liberty Bell Industrial
Park; and Wood-
haven Industrial Park adja-
cent to rail line)





INTERSTATE 95

Map 14
Adjacent Land Use
Scale: 1" = 2000'

Residential

Vacant
(10 or more contiguous acres)

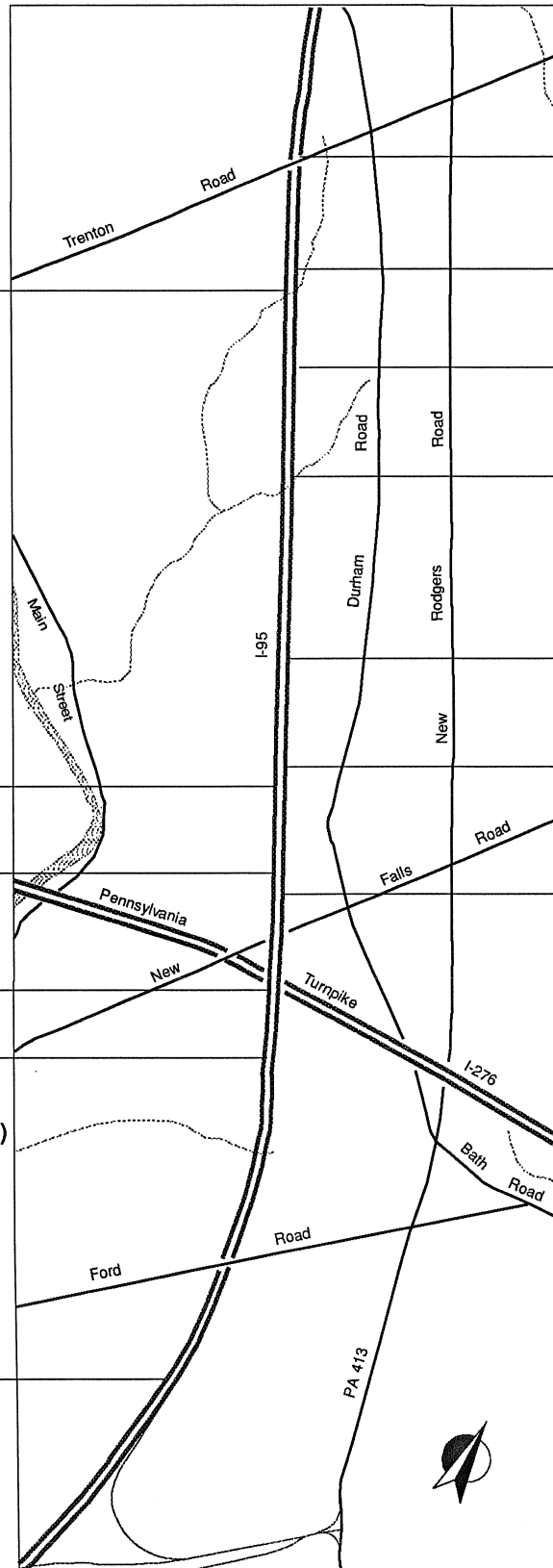
Residential

Transportation

Residential
(Newportville)

Commercial/Industrial
(Keystone Industrial Park II
and III; U.S. Army Reserve Center)

Vacant
(10 or more contiguous acres)



Residential

Vacant
(Hulm Creek; 10 or
more contiguous acres)

Residential
(Ramblewood, Country
Club Apartments)

Vacant
(10 or more contiguous acres)

Residential

Vacant
(10 or more contiguous acres)

Residential
(Parkview Apartments,
Tanglewood)

Commercial/Industrial
(Bristol Plaza Shopping
Center; Keystone
Industrial Park I)





INTERSTATE 95

Vacant
(10 or more contiguous acres)

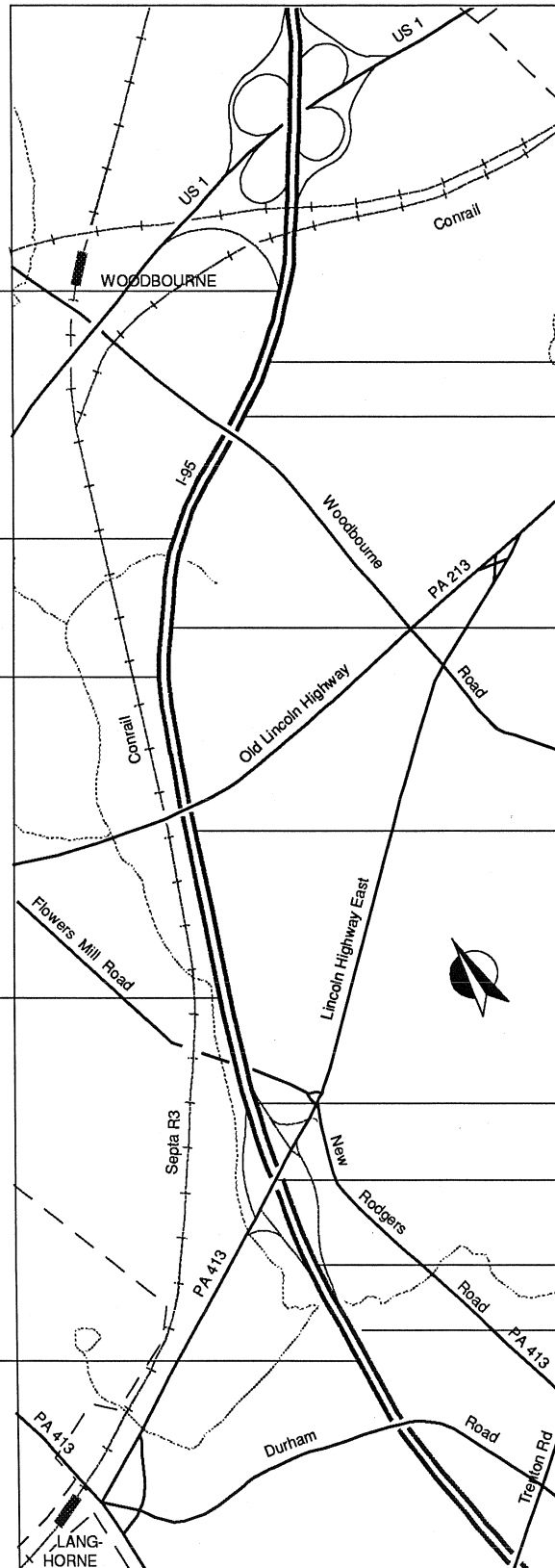
Agricultural

Vacant
(10 or more contiguous acres)

Transportation

Water
(Mill Creek)

Residential
(Middletown Trace Apartments)



Map 15
Adjacent Land Use

Scale: 1" = 2000'

Vacant
(10 or more contiguous acres)

Residential

Vacant
(10 or more contiguous acres)

Commercial/Industrial

Vacant
(10 or more contiguous acres)

Transportation

Commercial/Industrial

Vacant

Residential
(Villas of Middletown)





INTERSTATE 95

Map 16
Adjacent Land Use
Scale: 1" = 2000'

Agricultural

Agricultural

Residential

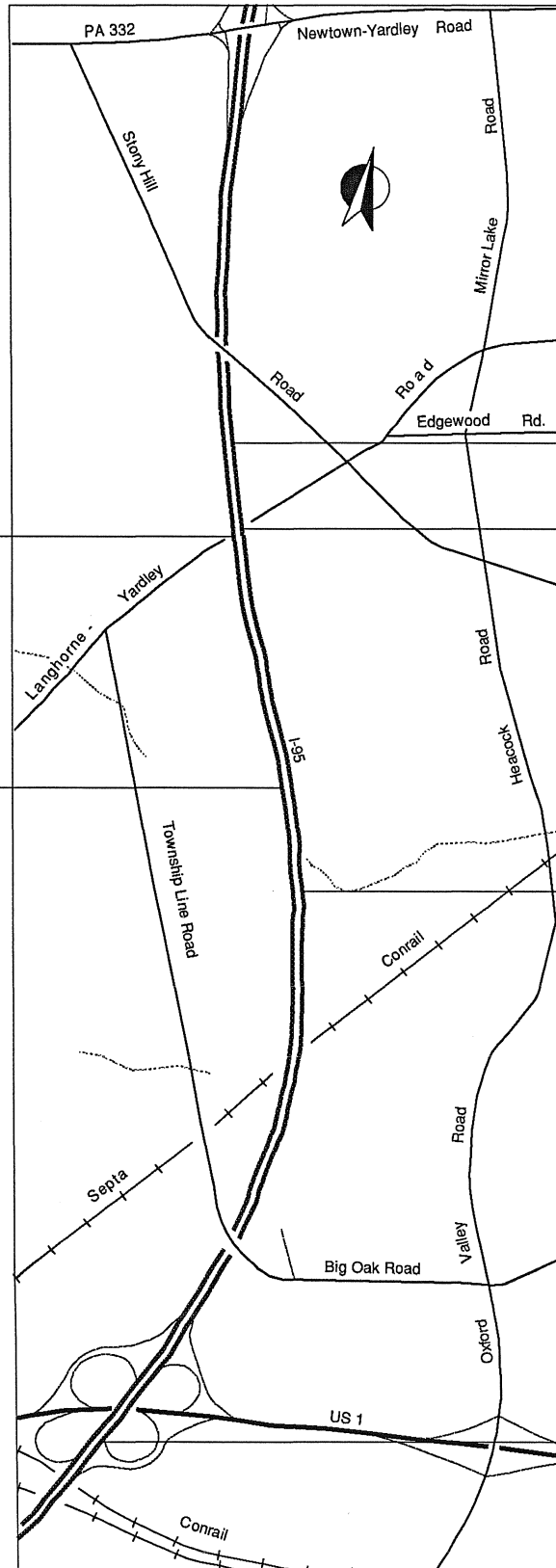
Commercial/Industrial

Residential
(Heacock Meadows)

Vacant
(10 or more contiguous acres)

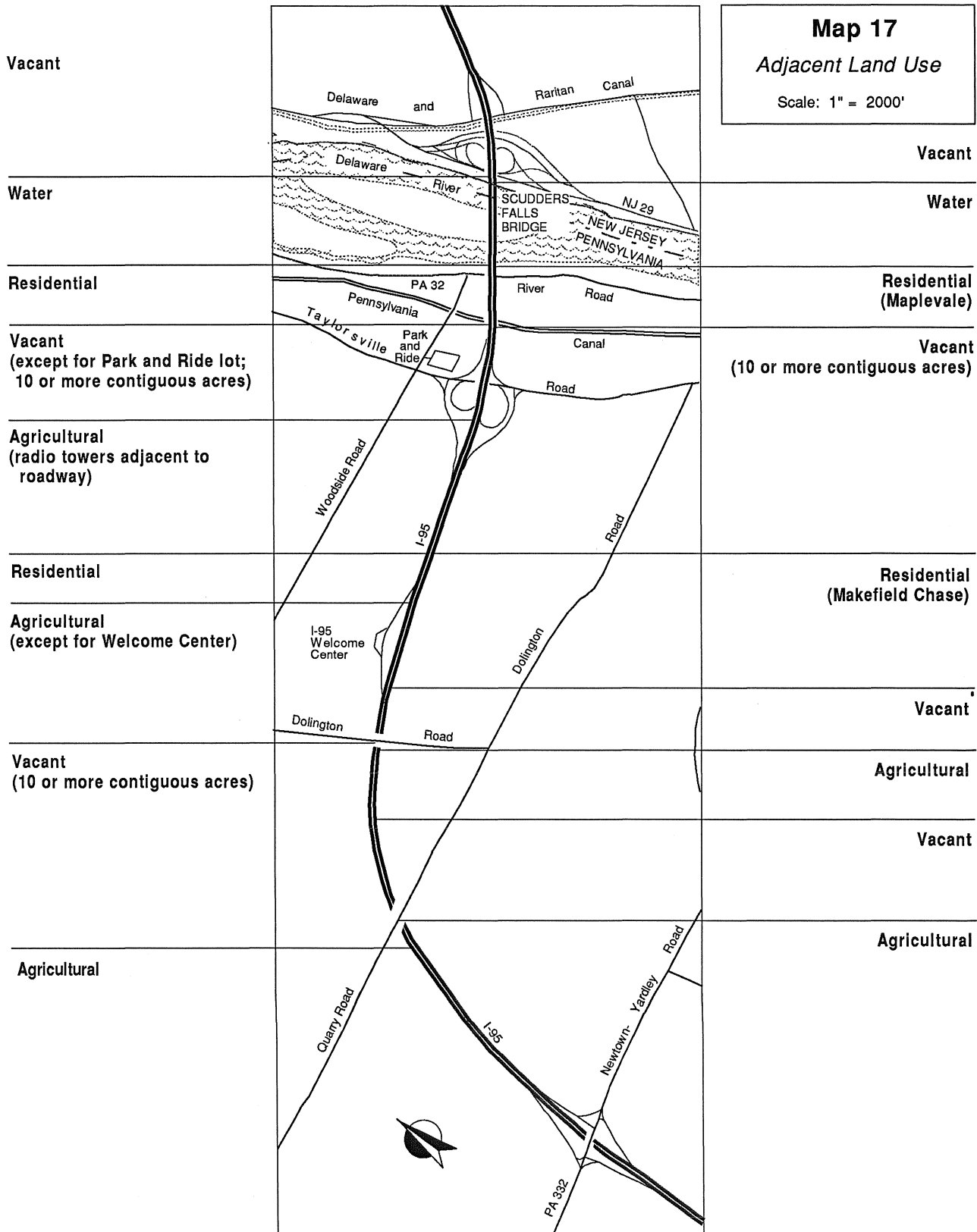
Vacant
(10 or more contiguous acres)

Commercial/Industrial
(Cabot, Cabot and Forbes
Industrial Park and
Oxford Valley Mall)





INTERSTATE 95



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PLANNED IMPROVEMENTS

This map series summarizes transportation improvements to the Interstate Route 95 corridor which are either underway or programmed. Improvements are also shown for parallel and intersecting roadways. Projects which are not yet funded are not included in this listing.

Each project can be located on the map by referencing its TIP number, which is placed as closely as possible to actual location. When a project entails a corridor, it is referenced at one end only.

Two official program numbers, the Transportation Improvement Program (TIP; source: DVRPC) number and the Program Management System (PMS; source: Penn DOT) number are provided for reference purposes. In the few cases where a TIP number is not specified, federal funding is not involved and the project is therefore not on the TIP. Cost and estimated let and completion dates are subject to change.

Each project is catalogued according to the following format:

- Project Description
- Project Location
- Limits of Project (if necessary)
- Special Comments (related projects, funding, etc.)
- Transportation Improvement Program and Program Management System Numbers
- Current Estimated Cost
- Actual or Estimated Let Date
- Estimated Completion Date

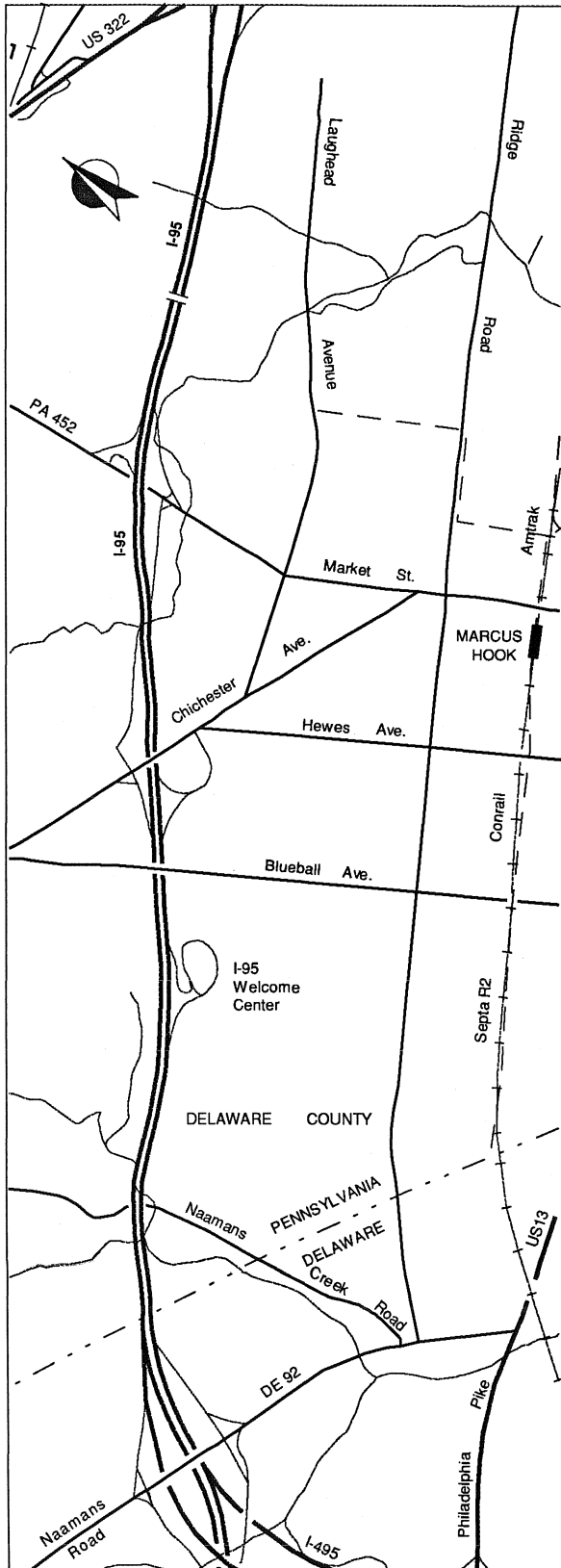
There are currently 14 projects scheduled for the I-95 corridor. In addition, there are currently plans to reconstruct whole sections of the roadway. The projects listed in this report can be found on maps 2-5, 7, 9, and 11-14.

Information was gathered for this section from DVRPC's Transportation Improvement Program, the City of Philadelphia Streets Department, and the PennDOT 12-year plan.





INTERSTATE 95



Map 1

Planned Improvements

Scale: 1" = 2000'

NO PROJECTS





INTERSTATE 95

Map 2
Planned Improvements
Scale: 1" = 2000'

Construct ramp

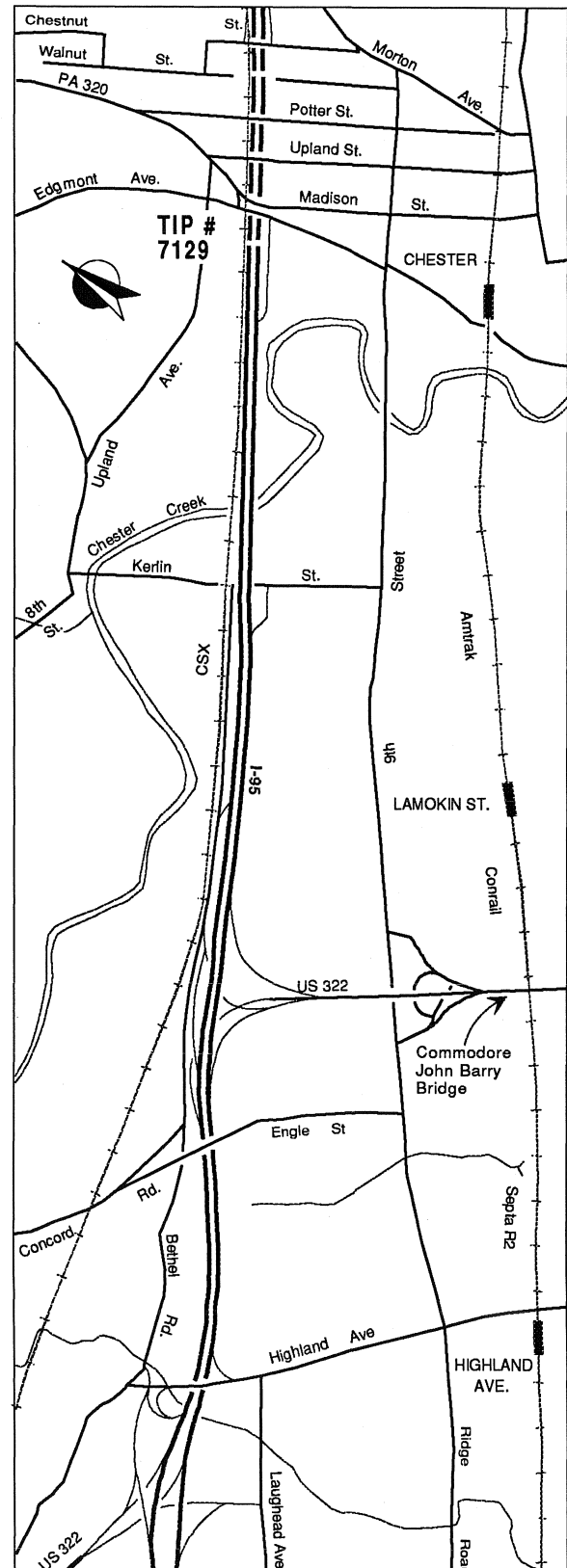
PA 352 (Edgmont Avenue) to Southbound I-95

TIP # 7129 PMS # 063C059

\$3.0 M

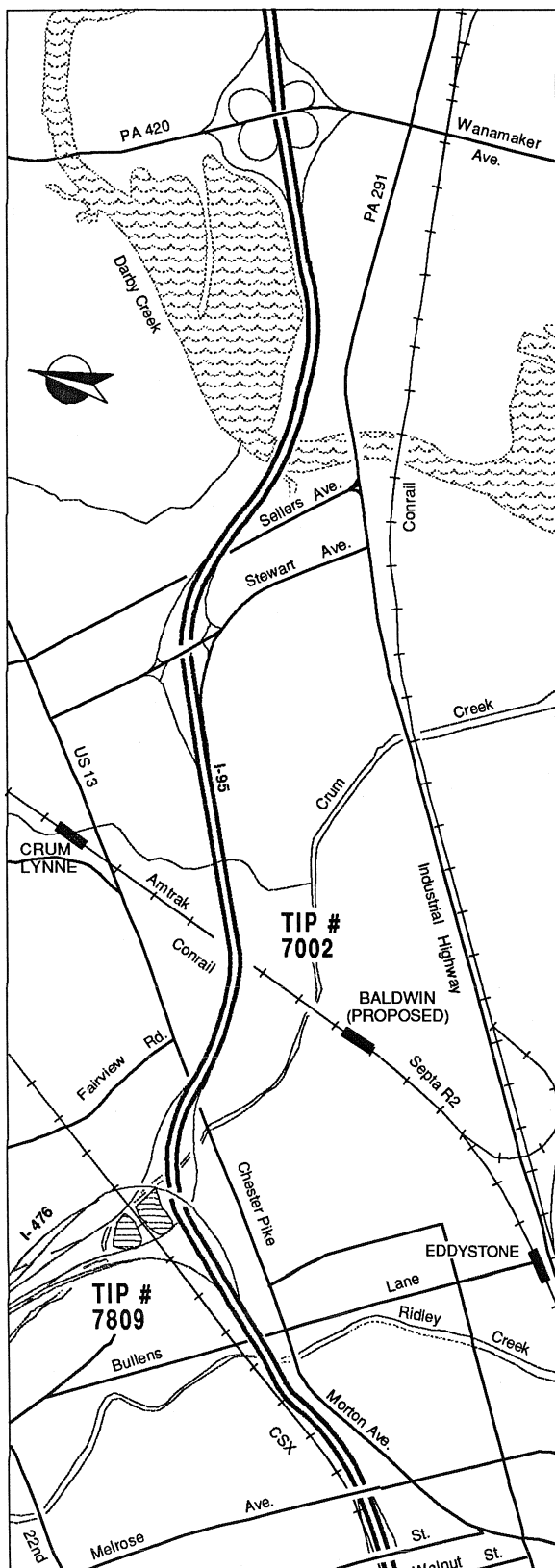
Let date: 1993 (estimated)

Estimated Completion: Late 1994





INTERSTATE 95



Map 3

Planned Improvements

Scale: 1" = 2000'

Install noise barriers

I-95 and I-476 (Mid-County Expressway) ramps

TIP # 7809 PMS # 063C500

\$2.8 M

Let date: Mid 1992 (estimated)

Estimated Completion: Mid 1993

Construct park-and-ride lot

I-95 and Amtrak North-east Corridor/SEPTA R-2 line

TIP # 7002 PMS # 063C032

\$8.6 M

Let date: Late 1994 (estimated)

Estimated Completion: Late 1995





INTERSTATE 95

Map 4

Planned Improvements

Scale: 1" = 2000'

Restore roadway

PA 291 (Governor Printz Boulevard)

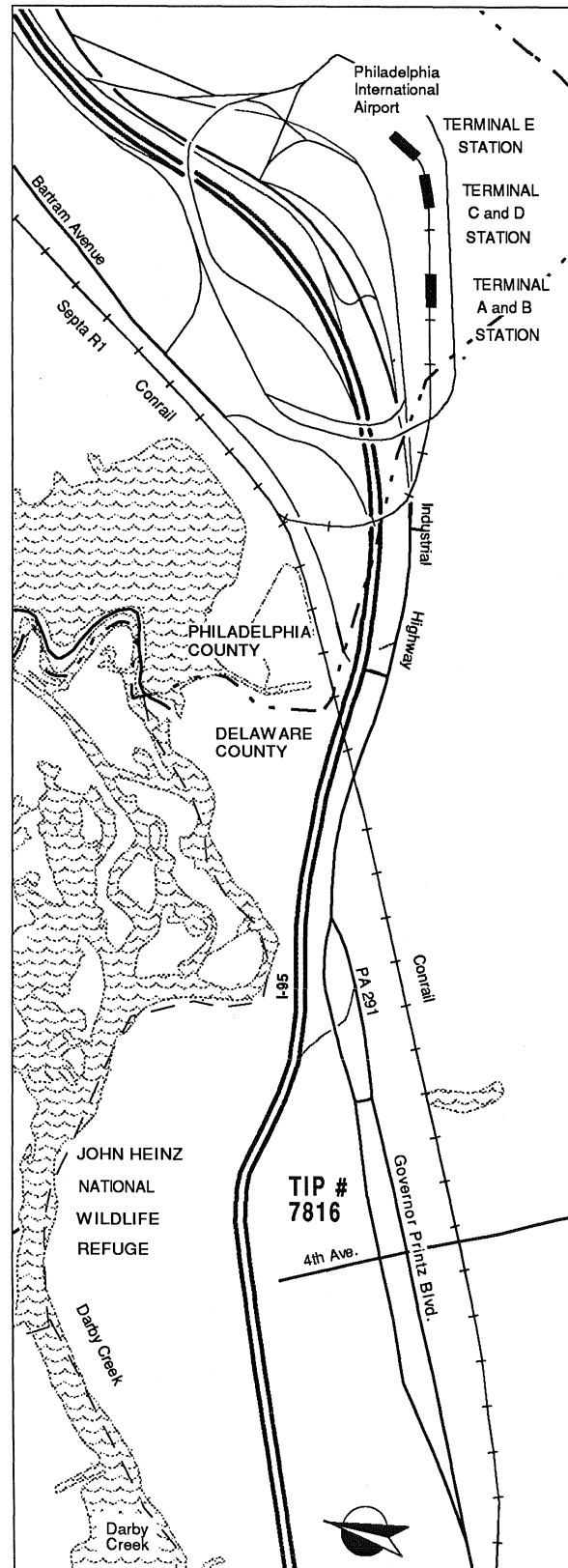
I-95 overpass to Jansen Avenue

TIP # 7816 PMS # 063M0048

\$2.1 M

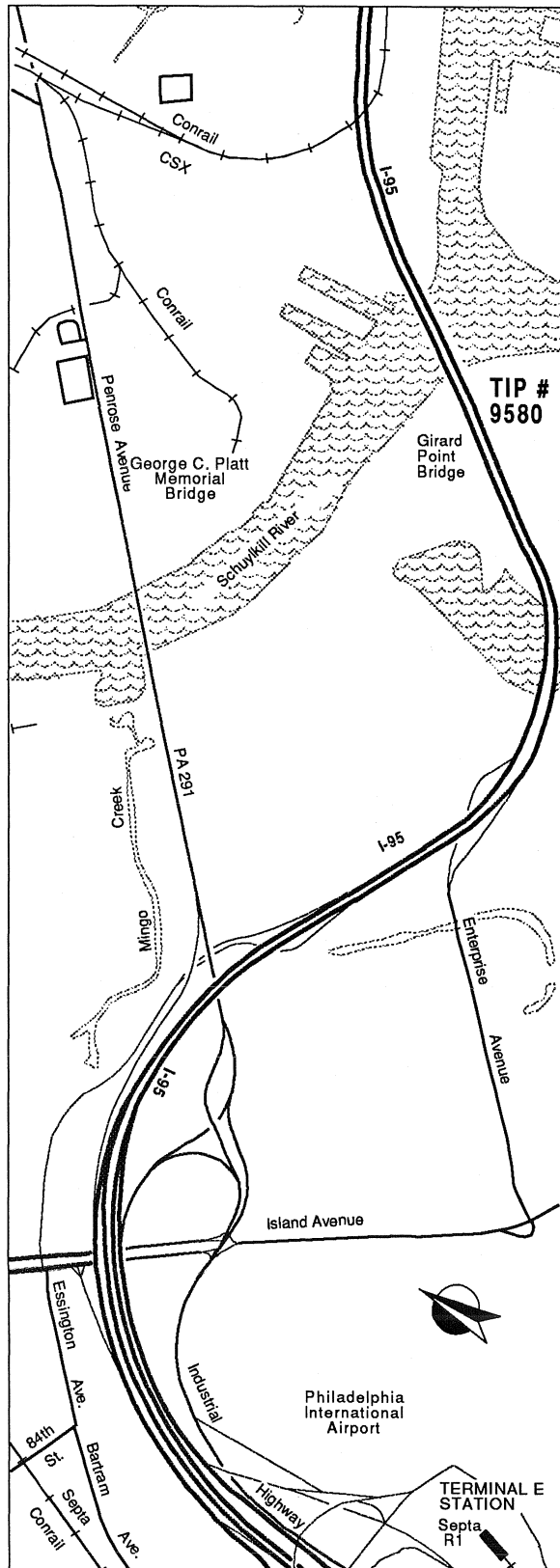
Let date: 5/91

Estimated Completion: Late 1992





INTERSTATE 95



Map 5

Planned Improvements

Scale: 1" = 2000'

Rehabilitate bridge, second support pin and hanger

I-95: Girard Point Bridge

TIP # 9580 PMS # 065C114P

\$2.5 M

Let date: 10/91

Estimated Completion: Late 1992

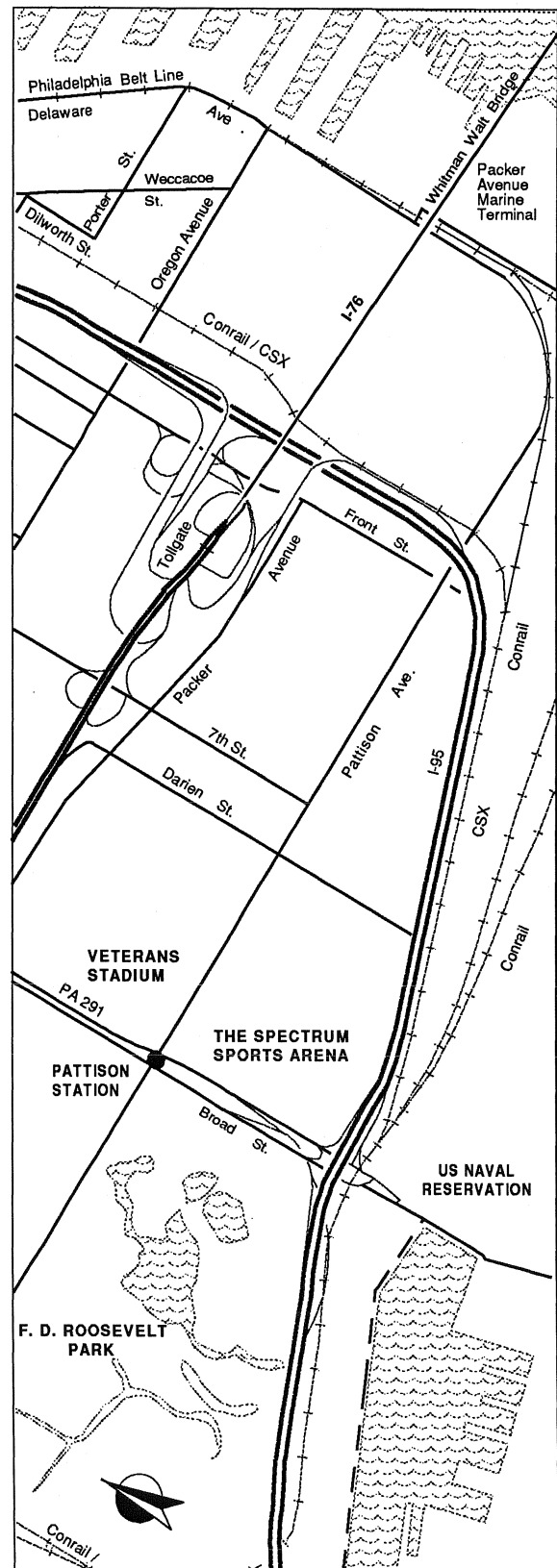




INTERSTATE 95

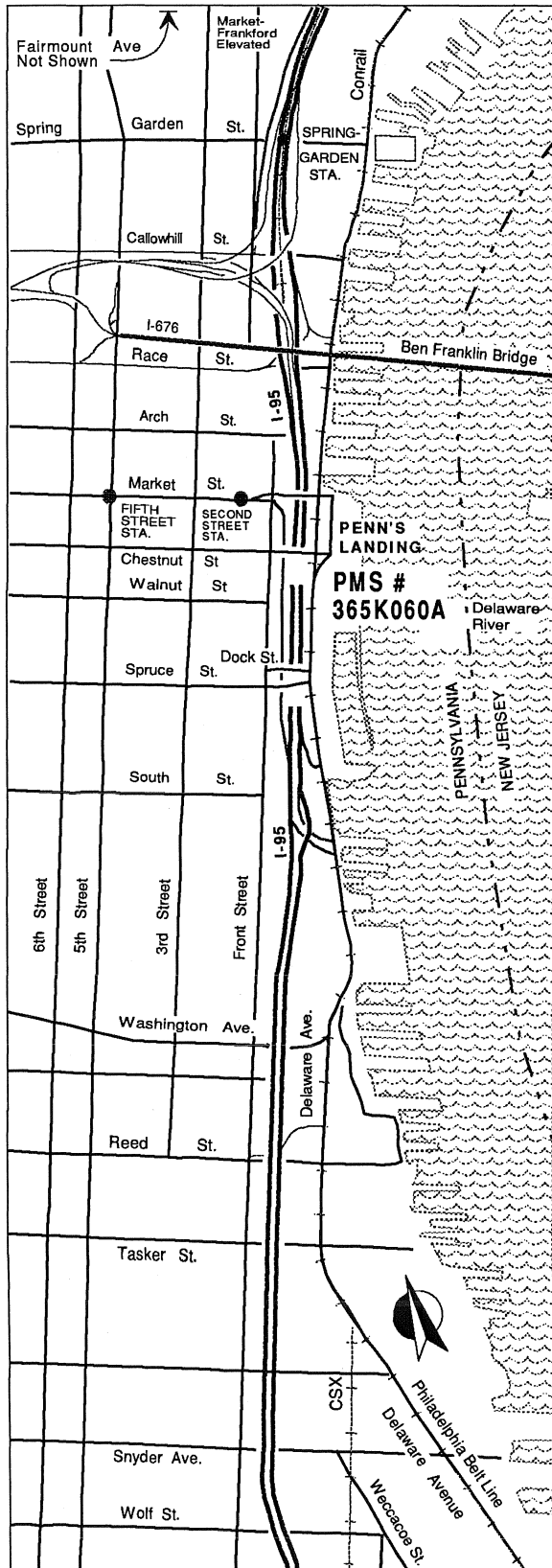
Map 6
Planned Improvements
Scale: 1" = 2000'

NO PROJECTS





INTERSTATE 95



Map 7

Planned Improvements

Scale: 1" = 2000'

Perform maintenance and inspection of tunnel

I-95 Tunnel

Chestnut Street

TIP # (none) PMS # 365K060A

\$440 K

Let date: 5/131/90

Estimated Completion: Mid 1993

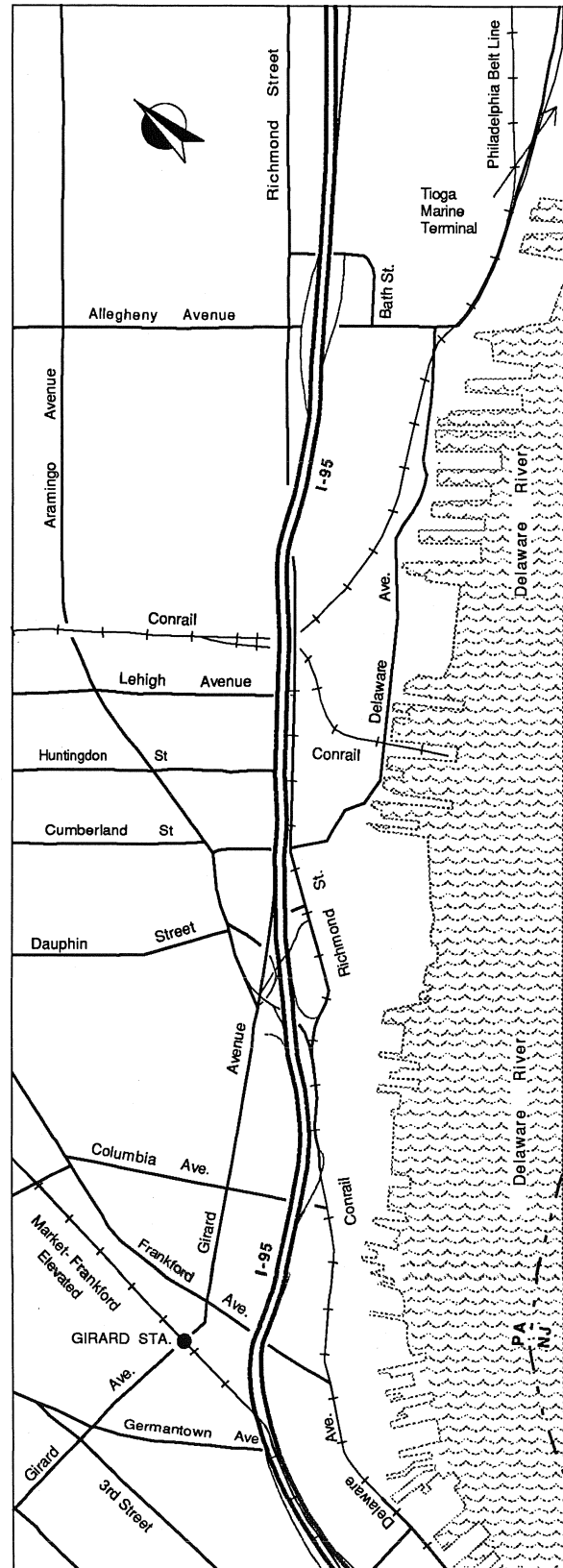




INTERSTATE 95

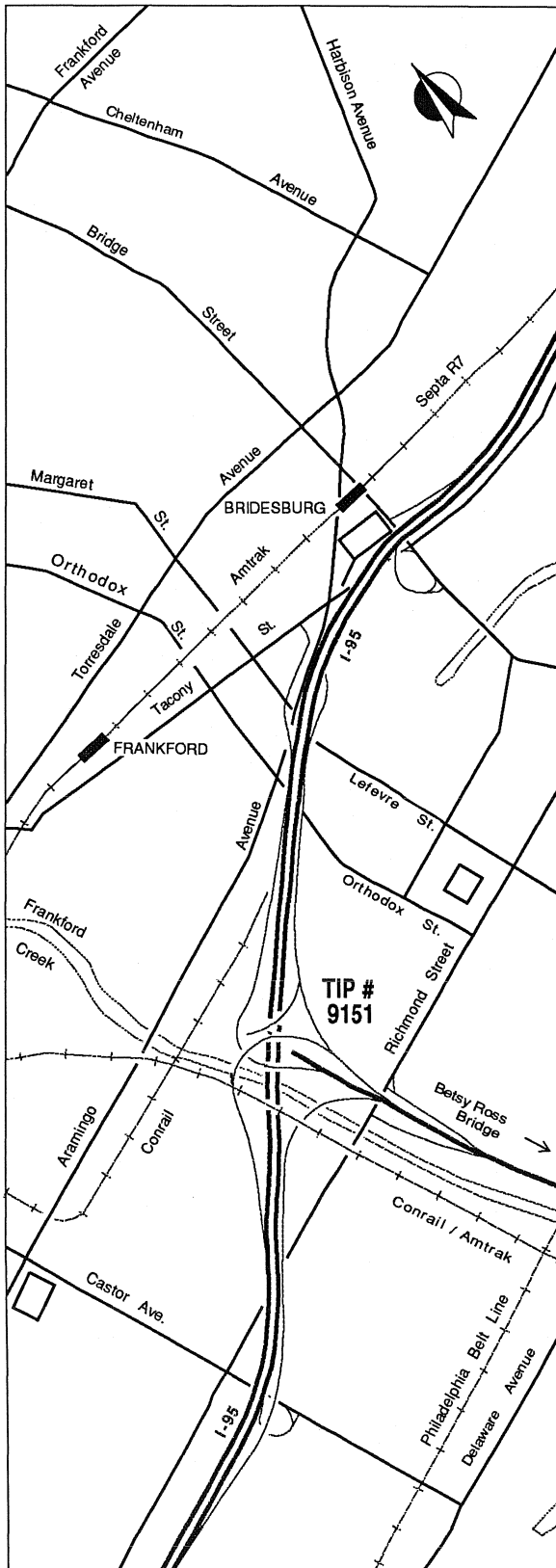
Map 8
Planned Improvements
Scale: 1" = 2000'

NO PROJECTS





INTERSTATE 95



Map 9

Planned Improvements

Scale: 1" = 2000'

Construct Interchange

I-95, Aramingo avenue, and Betsy Ross Bridge

TIP # 9151

PMS # 065C7013

\$48 M

Let date: Mid 1993 (estimated)

Estimated Completion: Late 1995

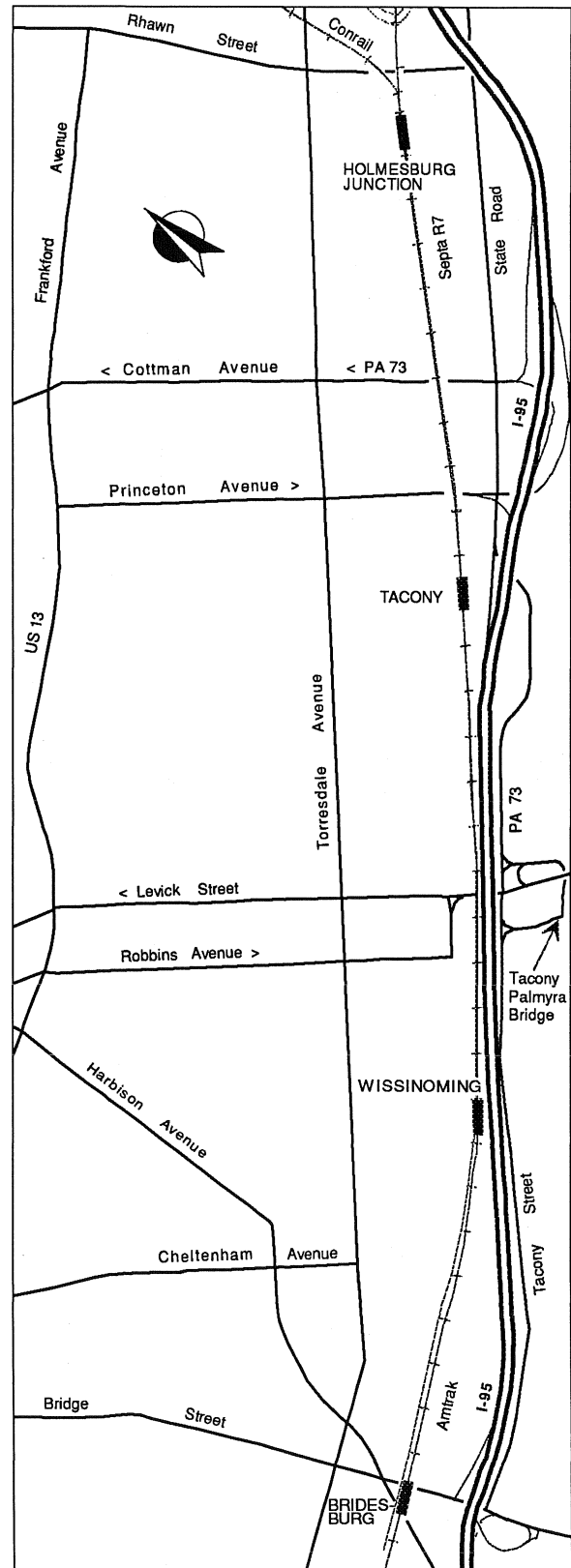




INTERSTATE 95

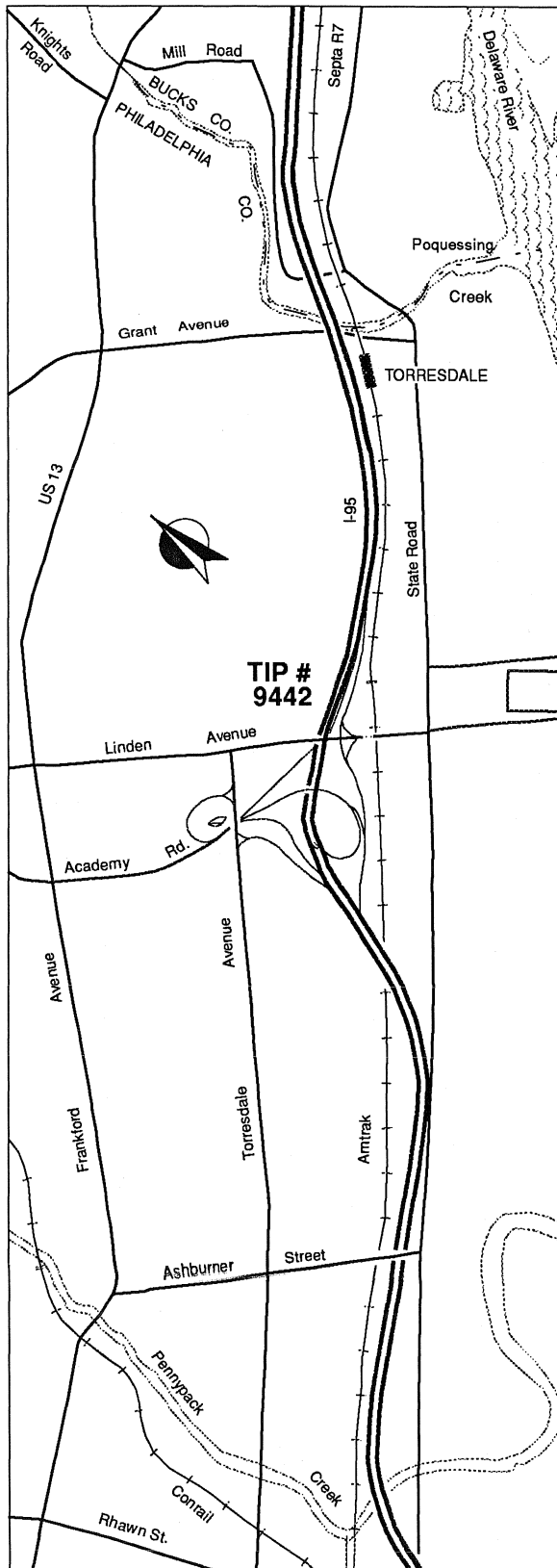
Map 10
Planned Improvements
Scale: 1" = 2000'

NO PROJECTS





INTERSTATE 95



Map 11

Planned Improvements

Scale: 1" = 2000'

Replace bridge

Linden Avenue bridge over Amtrak
I-95 to State Road

TIP # 9442 PMS # 0651143

\$3.6 M

Let date: early 1993 (estimated)

Estimated Completion: Late 1993





INTERSTATE 95

Map 12

Planned Improvements

Scale: 1" = 2000'

Widen and rehabilitate ramps

I-95 and PA 63 (Woodhaven Road) Interchange

Tennis Avenue to Station Avenue

TIP # 5579

PMS # 061M5402

\$6.7 M

Let date: Mid 1992 (estimated)

Estimated Completion: Late 1993

Construct regional fringe parking lot

I-95 and Cornwells Heights Train Station

TIP # 5002

PMS # 061C022

\$11 M

Let date: Late 1993 (estimated)

Estimated Completion: Late 1994

Perform Intersection Improvements

I-95 ramps and PA 132 (Street Road)

TIP # 5417 PMS # 061S503N

\$360 K

Let date: early 1993

Estimated Completion: Mid 1994

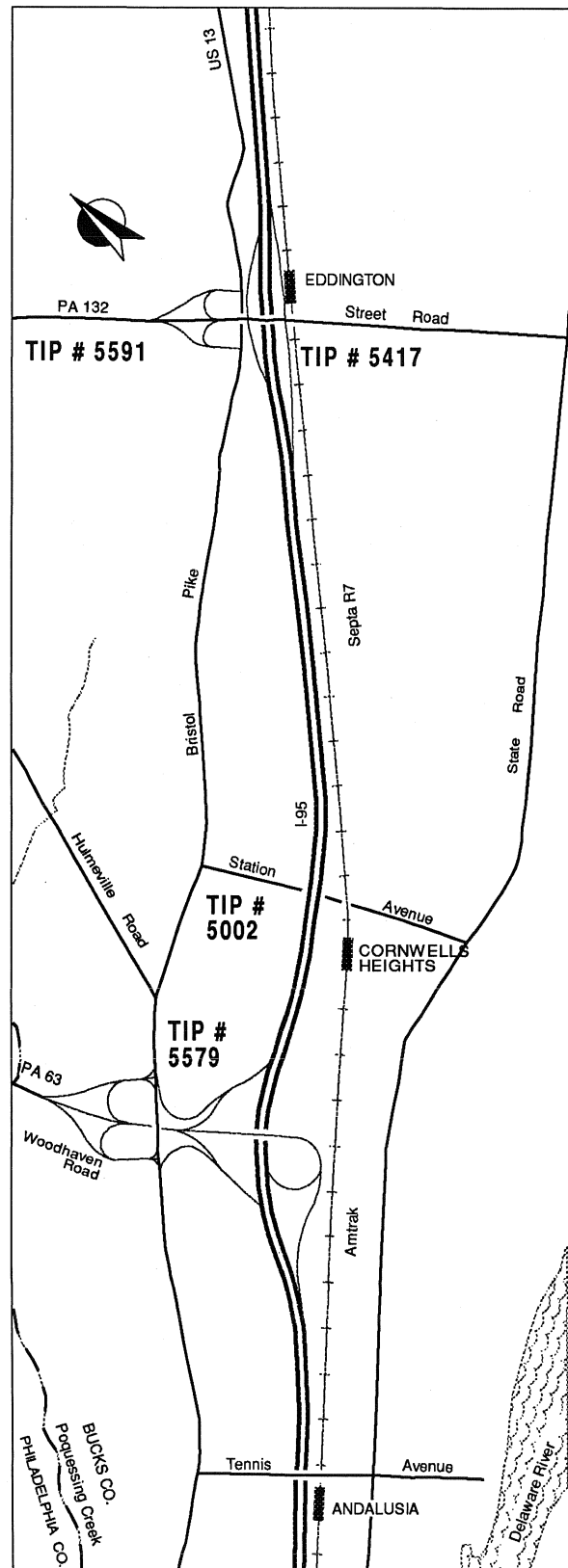
Restore Pavement

PA 132 (Street Road) US1 to State Road

TIP # 5591 PMS # 061M009A

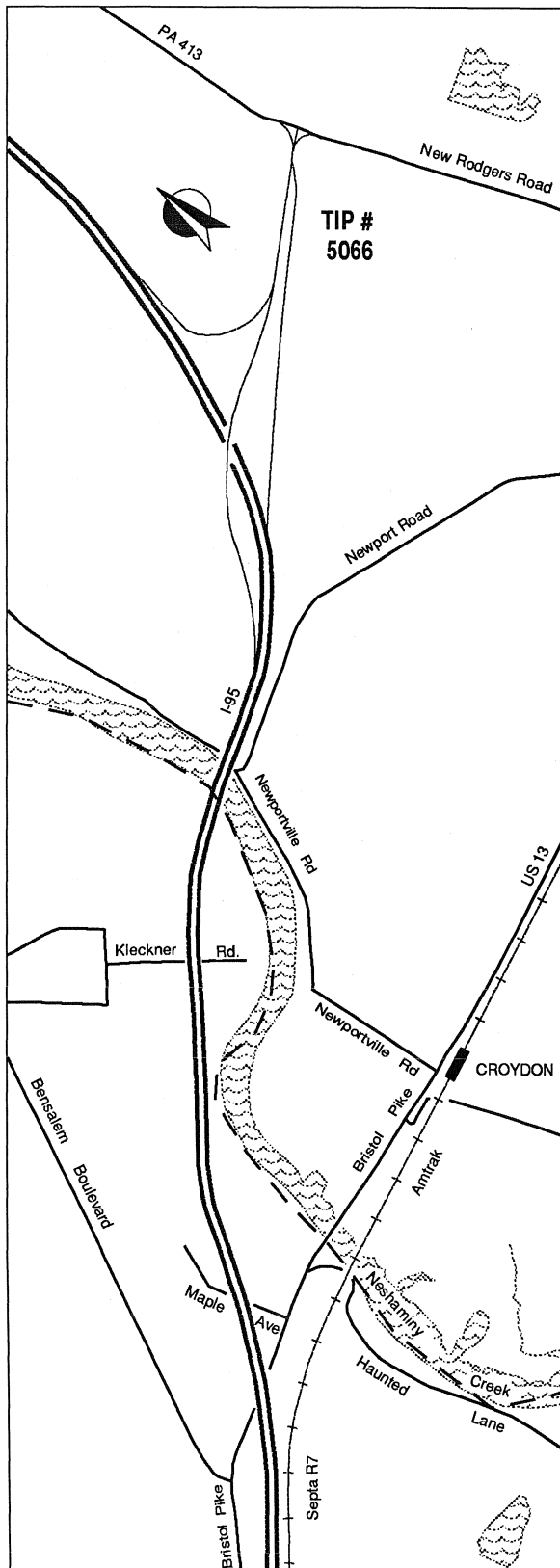
Let date: Mid 1991

Estimated Completion: Late 1992





INTERSTATE 95



Map 13

Planned Improvements

Scale: 1" = 2000'

Construct off-ramp

Southbound I-95 to PA 413

TIP # 5066 PMS # 061C024

\$8.0 M

Let date: early 1993 (estimated)

Estimated Completion: Mid 1995





INTERSTATE 95

Map 14

Planned Improvements

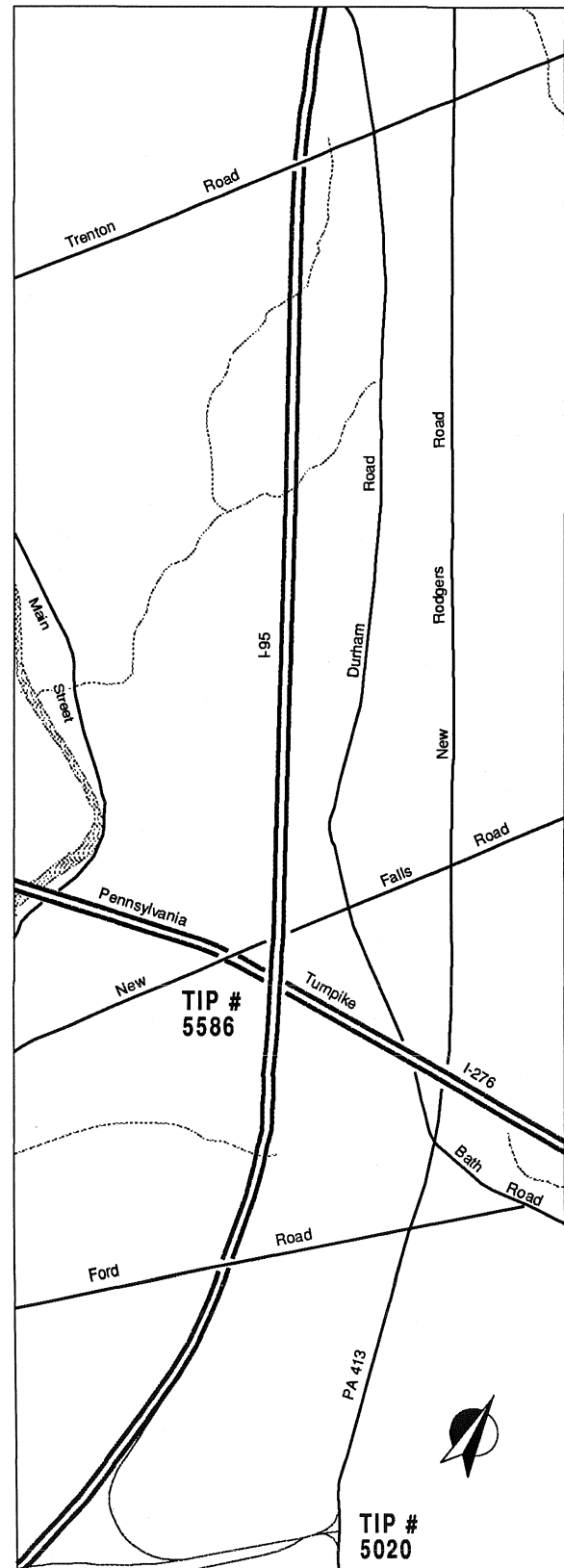
Scale: 1" = 2000'

Widen roadway

PA 413 (New Rodgers Road)
North of Ford Road to US 13 (Bristol Pike)
TIP # 5020 PMS # 061C025
\$11 M
Let date: Late 1993
Estimated Completion: Mid 1996

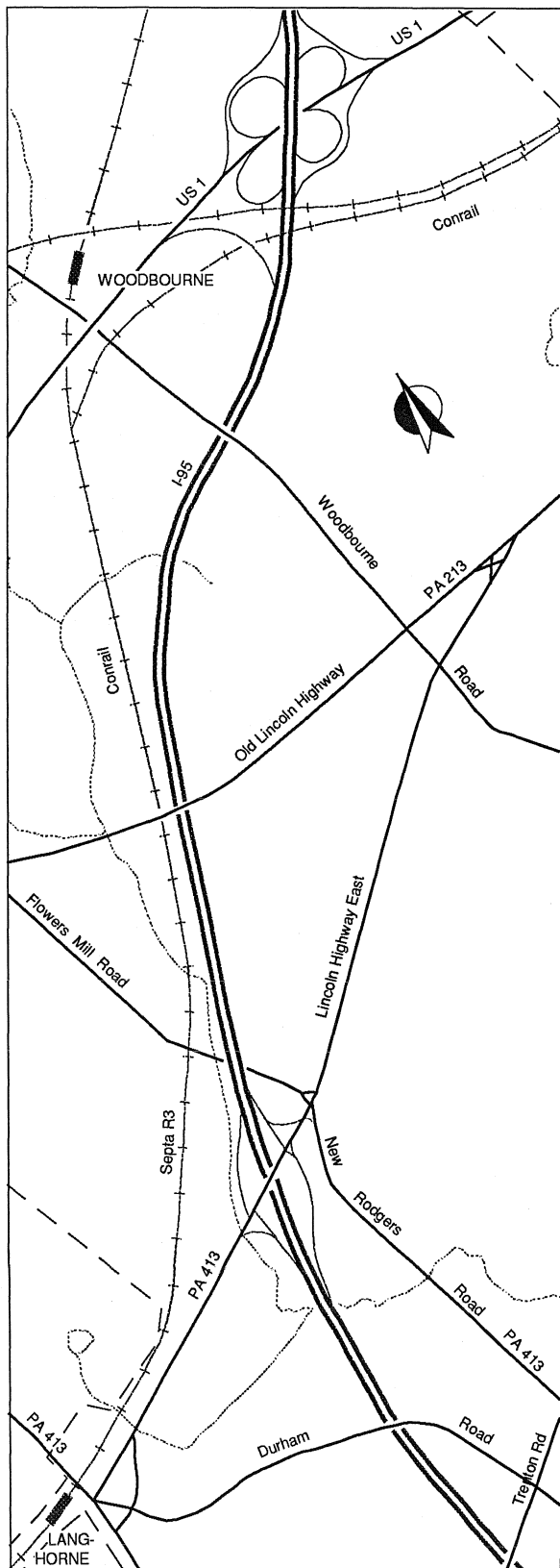
Construct Interchange w/ PA Turnpike; and new Delaware River Bridge

I-95 and I-276 (Pennsylvania Turnpike)
TIP # 5586 PMS # 061C072
\$526 M
Let date: Not available
Estimated Completion: Not available





INTERSTATE 95



Map 15

Planned Improvements

Scale: 1" = 2000'

NO PROJECTS

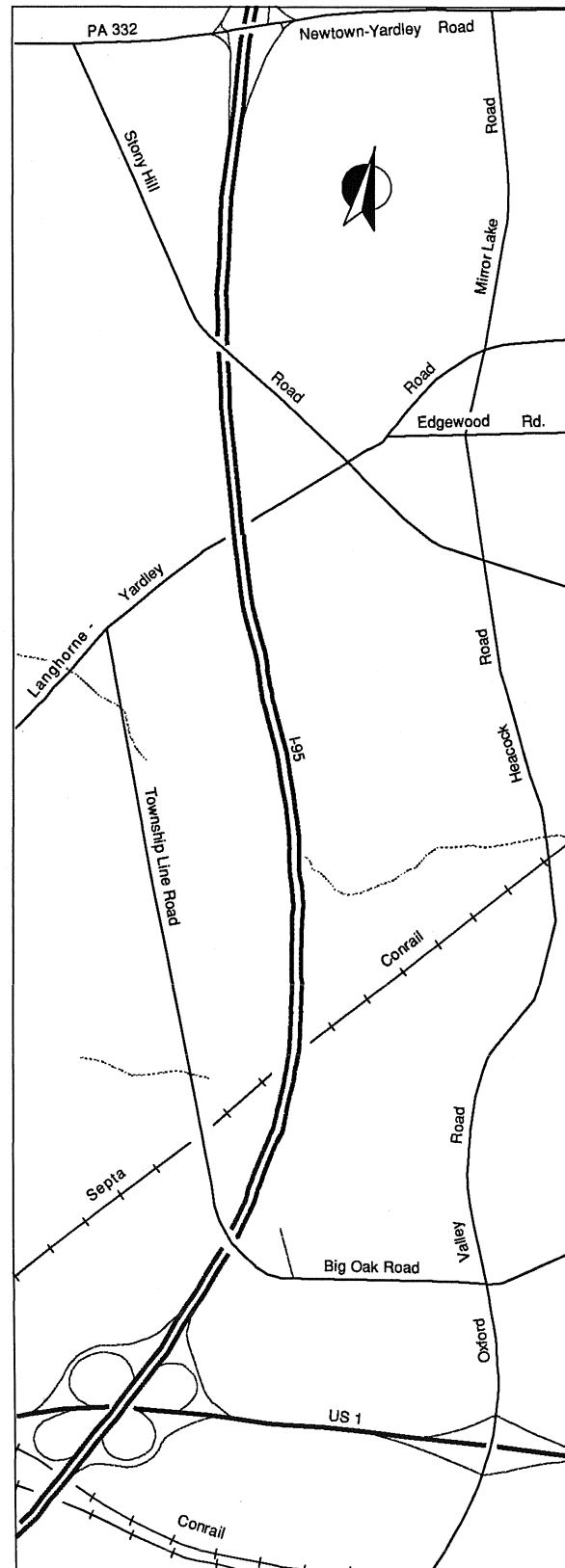




INTERSTATE 95

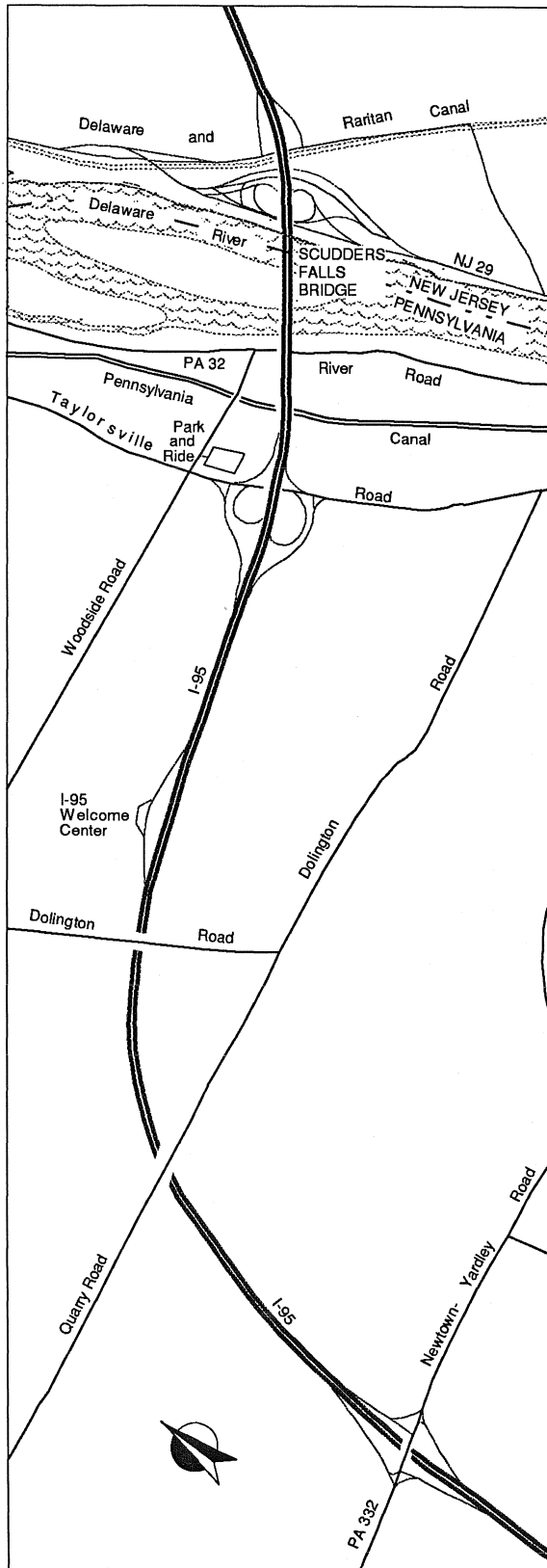
Map 16
Planned Improvements
Scale: 1" = 2000'

NO PROJECTS





INTERSTATE 95



Map 17
Planned Improvements

Scale: 1" = 2000'

NO PROJECTS



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NATURAL AND CULTURAL ENVIRONMENT

The natural and cultural environment maps identify noteworthy features located within close proximity to the roadway. Natural features displayed on the map include wetlands and flood plains. Cultural features are primarily historic.

Most of the wetlands information, including location and general configuration, was derived from the National Wetlands Inventory (NWI) Maps, issued by the United States Department of the Interior. Wetlands are identified by a pattern of horizontal lines. No distinction is made between the various classifications of wetlands (such as estuarine, palustrine, riverine, marine, or lacustrine).

It should be noted that the NWI maps are compiled on a very large scale and are therefore very general. An indication of wetlands on an NWI map suggests that wetlands are probably present to some extent on that particular site. Conversely, an indication that wetlands are not present suggests that the site is probably free of wetlands. However, any specific site which is proposed for development must be surveyed individually before a final determination can be made as to whether or not wetlands are actually present.

The flood plains delineated on the map represent the 100-year flood plain boundaries as identified on the Flood Insurance Rate Maps distributed by the Federal Emergency Management Agency. Flood plain areas are indicated by a pattern of vertical lines. Areas where flood plains and wetlands overlap are indicated by intersecting vertical and horizontal lines. Wetlands which are actually bodies of standing water are already depicted on the map.

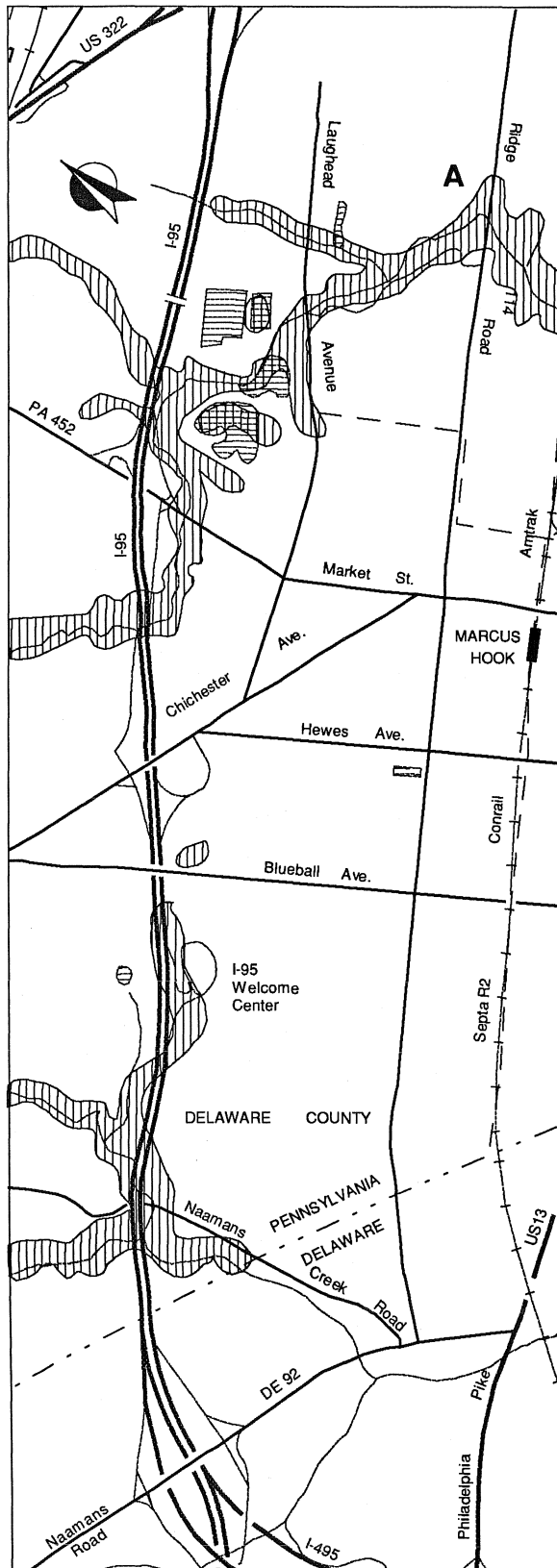
Historical features are also identified. These features include sites, structures, or districts which are significant in terms of American history, architecture, or culture. The general locations of the features are indicated on the map by large, upper-case letters. The site name, the year built, a general description and the type of ownership are provided in the margin text. Sites which are listed on the national Register of Historic Places are identified as such.

Other sites or buildings which have been determined to be eligible for the National Register are also identified. All public agencies are required to safeguard properties on the National Register as well as those which are or may be eligible for it.

Information for this section was gathered from DVRPC reports and field observations.



INTERSTATE 95



Map 1

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

A Widow Price House

4358 Ridge Avenue. Occupied by Major General Gaines in 1814 during threatened invasion by British. Municipal ownership.





INTERSTATE 95

Map 2

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

A Widener University (formerly Pennsylvania Military College)

Located between 13th and 17th Streets and Melrose Ave. and Chestnut Street. Nation's second-oldest military college. University.

B Deshong Mansion and Park

930 Avenue of the States. Private.

C Chester Courthouse (Old Chester Court House)

Market Street below 5th Street. Oldest public building in continuous use in the country. Built circa 1724. City of Chester.

D Crozer Seminary

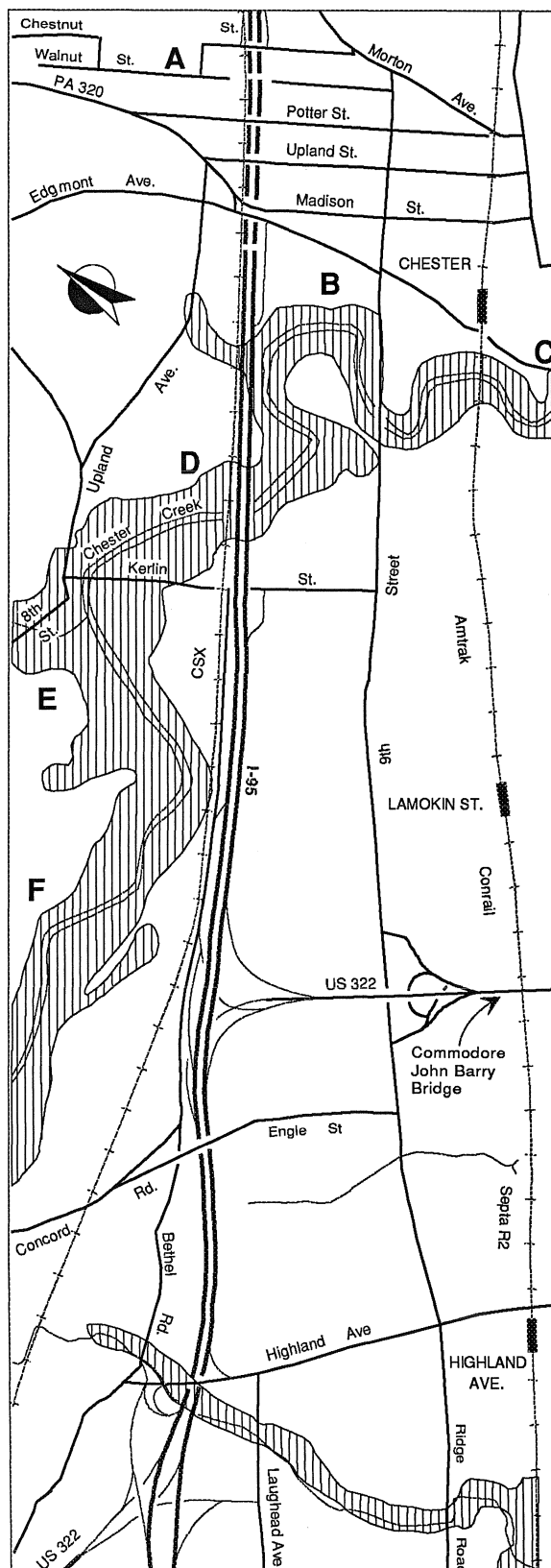
Crozer-Chester Medical Center. Private.

E Upland National Historic District

Boundaries: Chester Creek, Upland Ave., Hill St., 10th St., Main St. and Race Street.

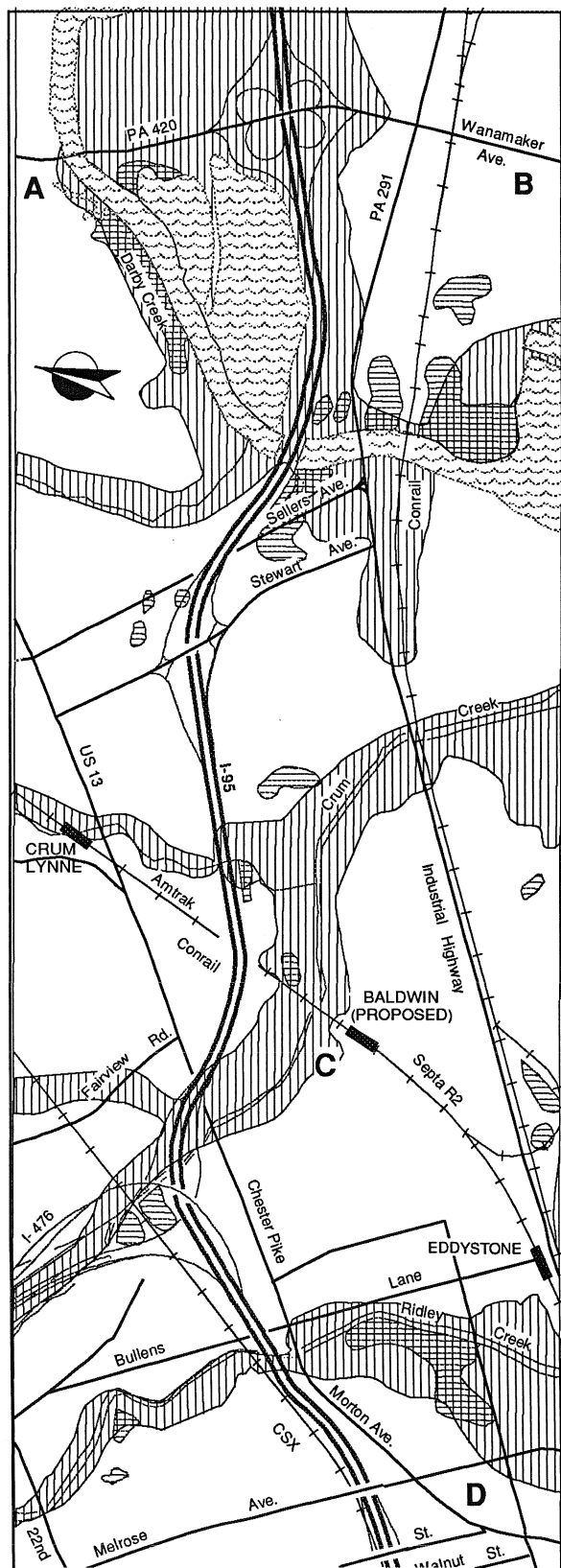
F Caleb Pusey House and Landingford Plantation

15 Race Street. Log house built in 1790; stone schoolhouse built 1849; grist and saw mill dating to circa 1770. Friends of Caleb Pusey House, Inc.





INTERSTATE 95



Map 3

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

A John Morton Homestead

Lincoln Avenue and Darby Creek. Swedish log house with later stone additions. Built circa 1654.

B Lazaretto

PA 420 off Wanamaker Avenue in Essington. Originally used as quarantine station and pest house to hospitalize persons coming up the Delaware River; now used as a seaplane and yacht club. Built circa 1799. Private.

C Baldwin Locomotive Works

US Route 13 (Chester Pike) and Simpson Street. Cruciform office building built after 1906. Private.

D Rochambeau Route

US Route 13 east of Chester City line at Morton and McDowell Streets. French troops camped here September 5, 1781 en route to Wilmington.





INTERSTATE 95

Map 4

Natural and Cultural Environment

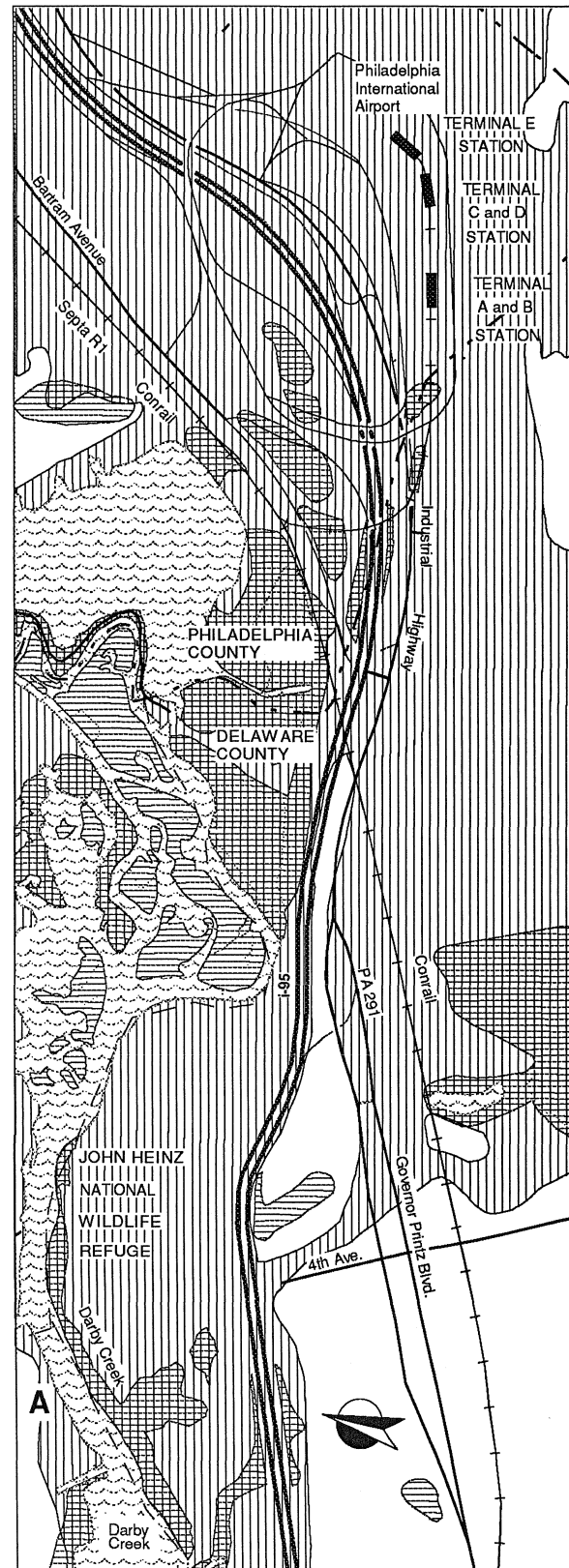
Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

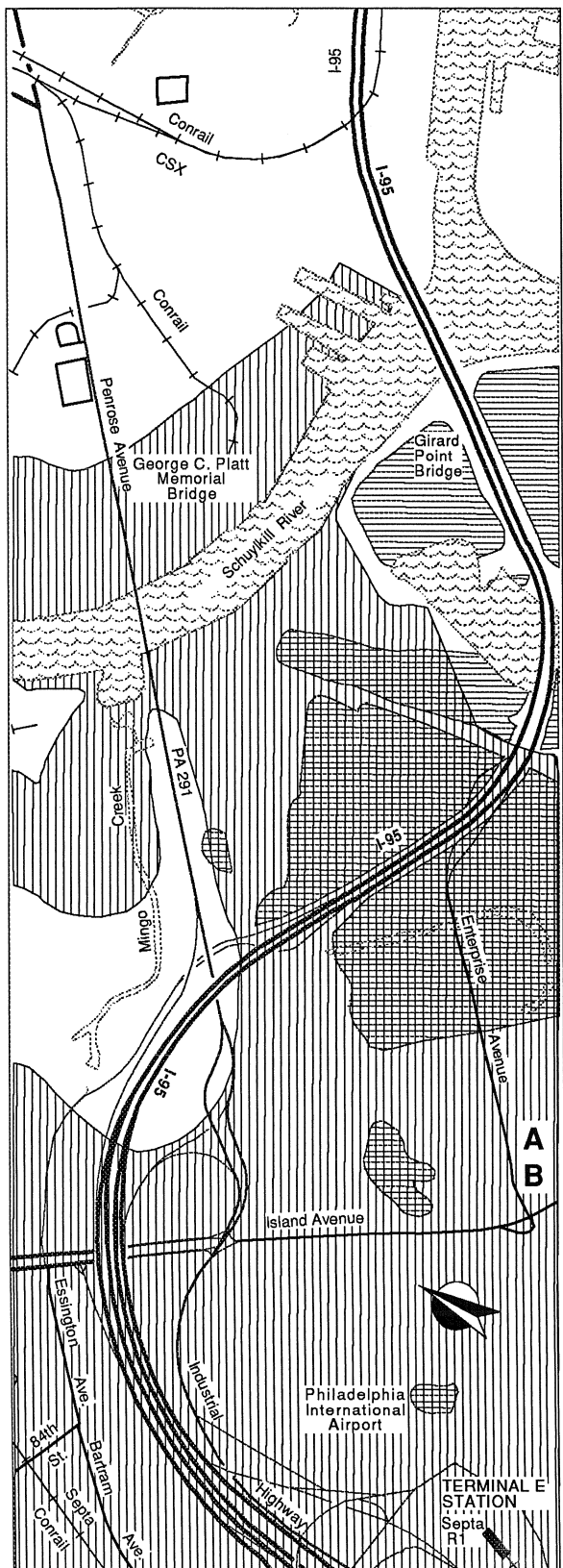
A Morton Mortonson Homestead

Muckinipattis and Darby Creeks. Restored early brick house built circa 1750.





INTERSTATE 95



Map 5

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

A Cannon Ball Farm

Penrose Ferry Road. Name results from the Battle for Fort Mifflin in 1777. Farmhouse built circa 1720. City of Philadelphia Water Department.

B Fort Mifflin

East of Philadelphia International Airport at foot of Fort Mifflin Road. Built to replace Mud Fort which defended against the British attack in the struggle to control navigation on the Delaware River. Built 1798. City of Philadelphia.





INTERSTATE 95

Map 6

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

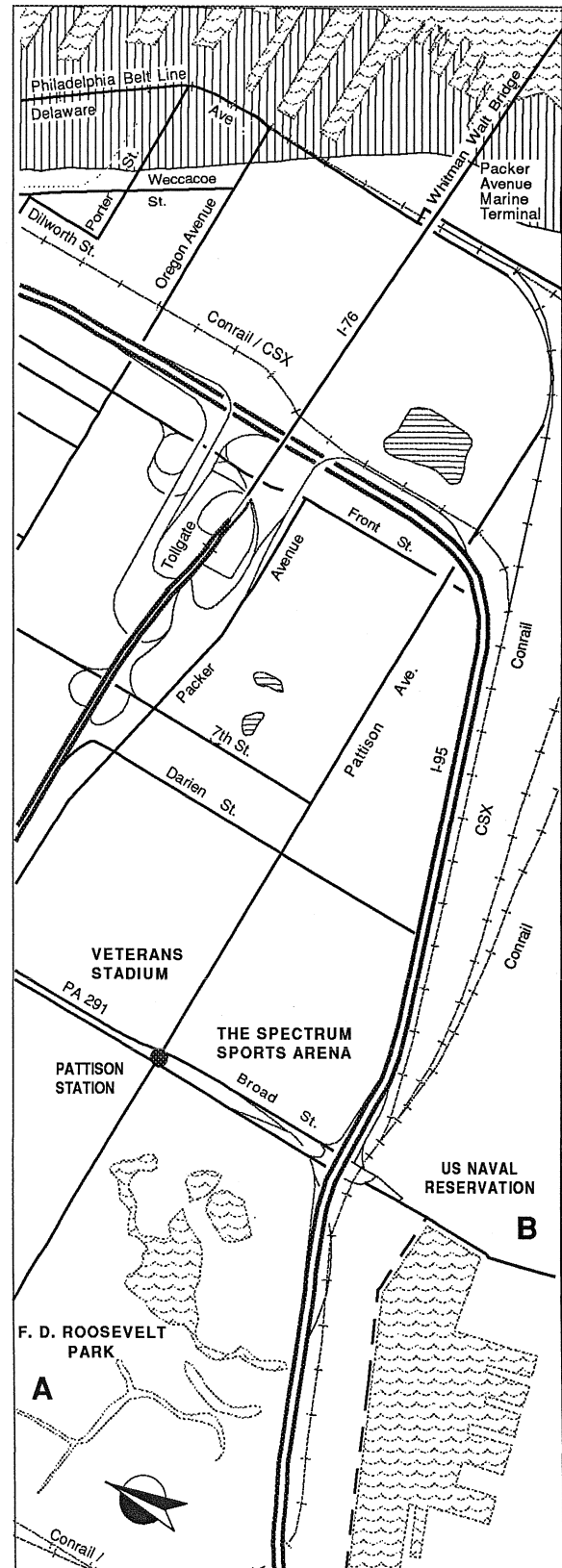
Vertical pattern: 100 - year flood plain

A Belair

Franklin Delanor Roosevelt Park, Broad Street and Pattison Avenue. At one time owned by Samuel Preston, Mayor of Philadelphia. Built in the early 18th century. Fairmount Park Commission.

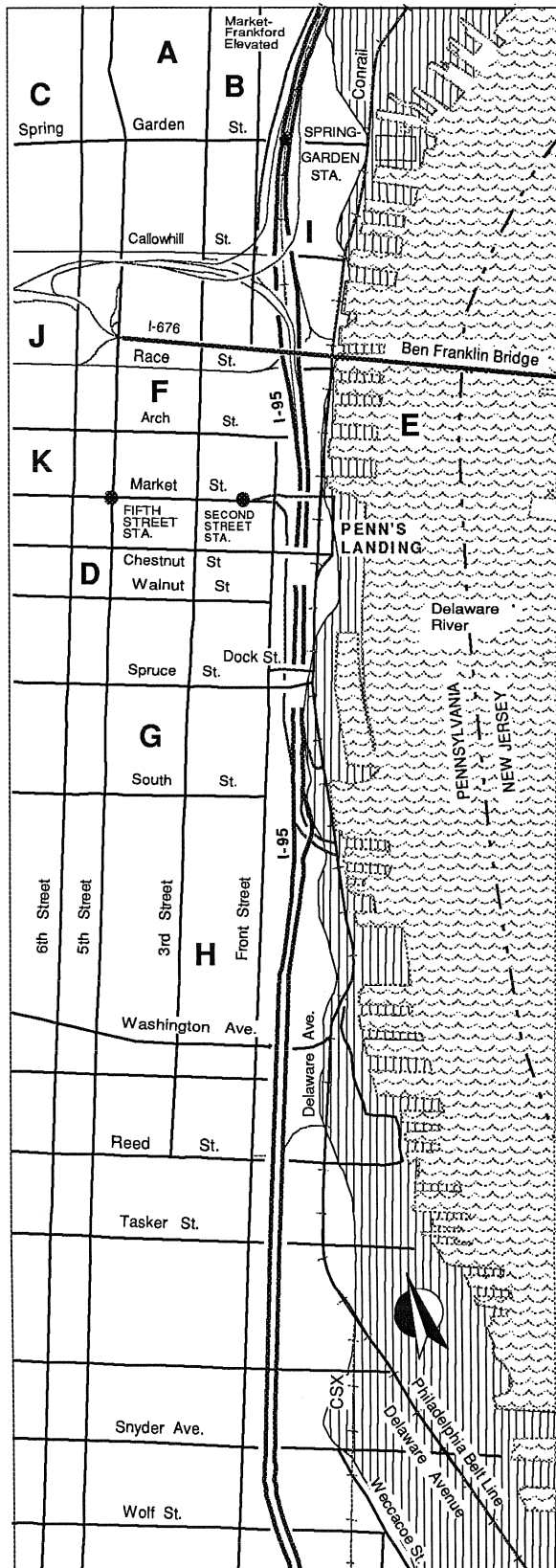
B Commandant's Quarters and Marine Barracks

Built circa 1901. Eligible for the National Register. Philadelphia Naval Base.





INTERSTATE 95



Map 7

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

A Northern Liberties National Historic District

Approximate boundaries: Brown St., Bodine St., Fairmount Ave., Galloway St., Green St., 7th St., Wallace St., 5th St., Fairmount Ave. and 6th St. to Brown Street.

B St. John's Episcopal Church (Romanian Orthodox)

Southwest corner of Brown and North American Streets. Built circa 1818. Church.

Edgar Allen Poe House

530 North 7th Street. Built circa 1830. Listed on the National Register. Private.

C Independence National Historic Park

2nd St. to 6th St. between Walnut and Chestnut Streets.

D Girard Group (Piers 3 and 5 North)

Delaware Avenue near Market Street. Private.

E Old City National Historic District

Approximate boundaries: Front St., Walnut St., 2nd St., Chestnut St., 5th St., Arch St., 4th St., New St., 5th St., Callowhill St., 2nd St., and Vine St. to Front Street. Includes Elfreth's Alley.

F Society Hill National Historic District

Approximate boundaries: South St., 8th St., Walnut St. and Front St. to South Street. Includes Washington Square and Drinkers Court.

G Southwark National Historic District

Approximate boundaries: Delaware Ave., Washington Ave., 5th St., Lombard St., Front St. and Catherine St. to Delaware Avenue.

I West Shipyard

1676 boatyard. Eligible for National Register.

J Franklin Square

I-676 to Race Street between 6th and 7th Streets.

K Free Quaker Meeting House

5th and Arch Streets. Built 1783. Commonwealth of Pennsylvania.





INTERSTATE 95

Map 8

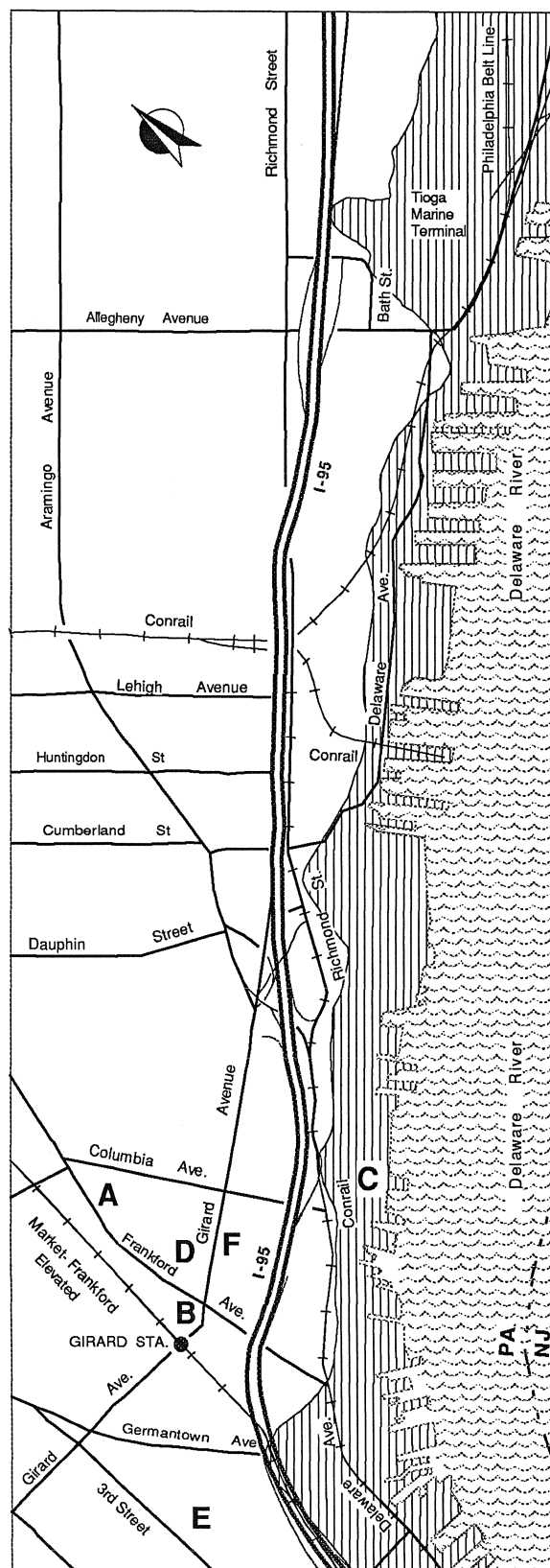
Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

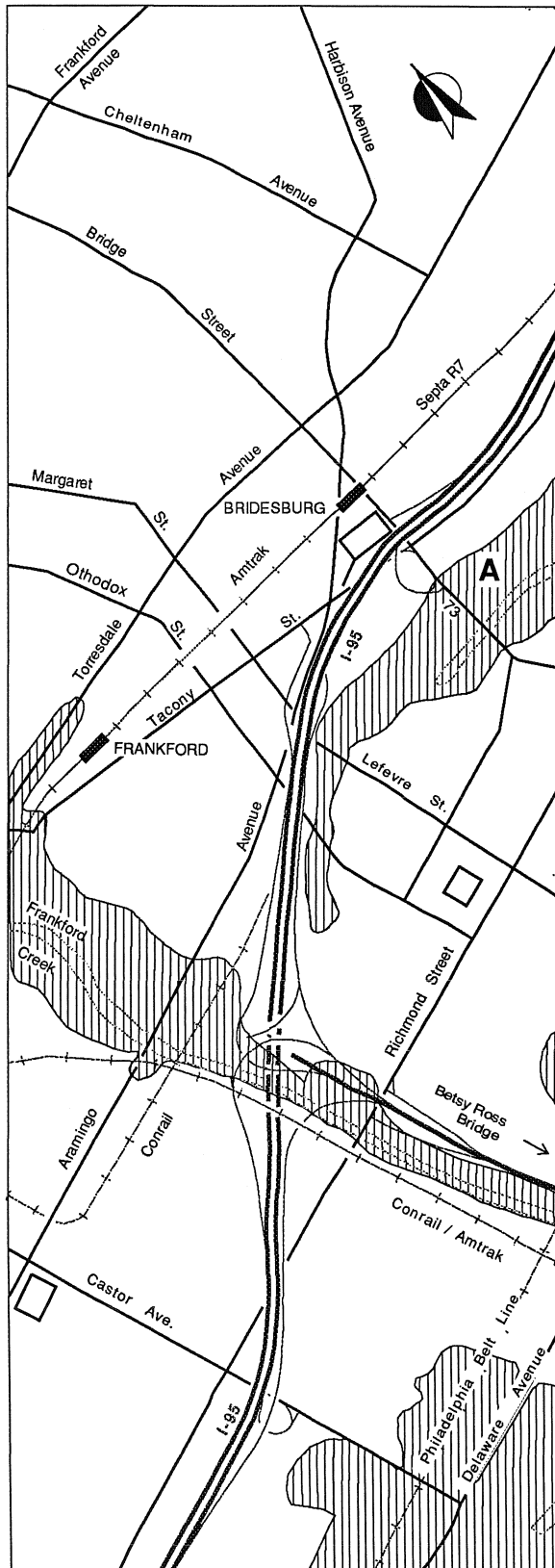
Vertical pattern: 100 - year flood plain

- A St. Laurentius Church**
1608 Berks Street, Fishtown. Founded in 1882; the oldest Polish Roman Catholic Parish in Philadelphia. Built 1887 to 1889. Church.
- B Kensington National Bank (First Pennsylvania)**
Frankford and Girard Avenues. Designed by Frank Furness. Built in 1875. First Pennsylvania Company.
- C Penn Treaty Monument and Park**
Penn Treaty Park, Beach Street. Monument indicates the spot where a great elm once stood that supposedly sheltered William Penn and the Indians when they made the famous treaty. City of Philadelphia.
- D Fishtown National Historic District (Proposed)**
South of Norris Street, east of Frankford Avenue to Delaware River.
- E Mifflin School**
808-818 North 3rd Street.
- F Green Tree Tavern**
260-262 East Girard Avenue.





INTERSTATE 95



Map 9

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

A Frankford Arsenal

Public institution. Built circa 1830.

Federal (United States Armed Forces).





INTERSTATE 95

Map 10

Natural and Cultural Environment

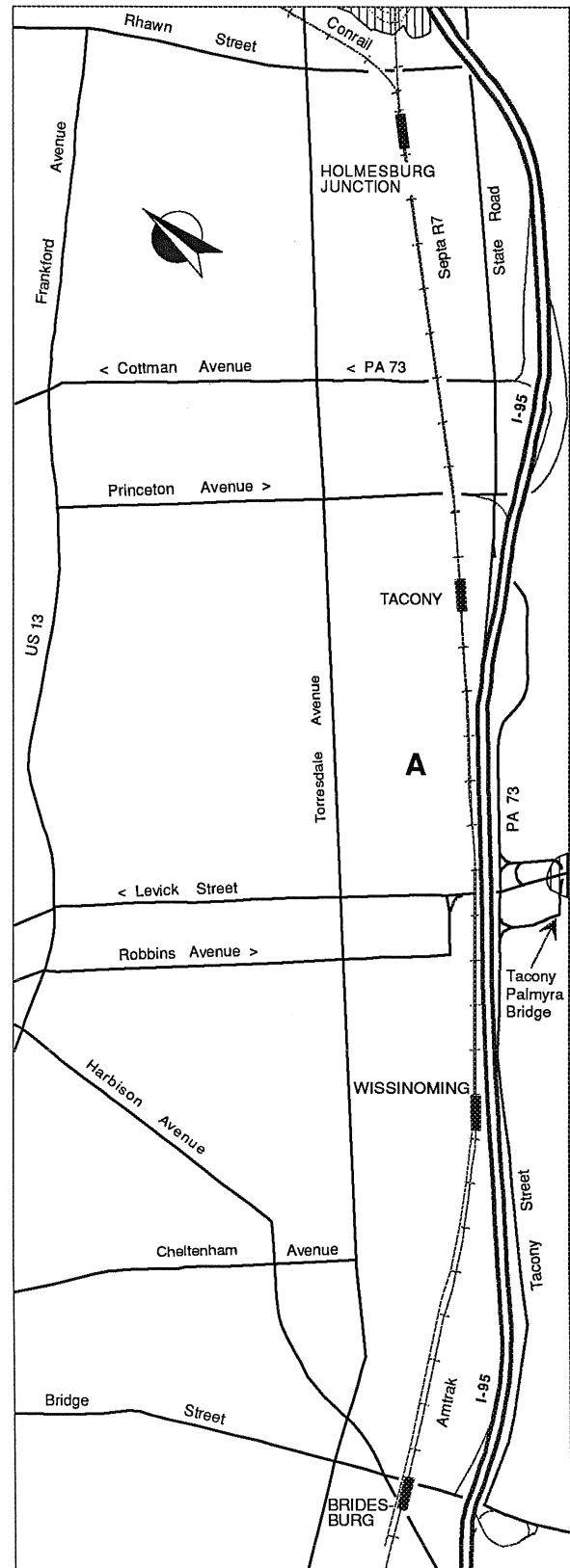
Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

A Music Hall

4815 Longshore Avenue.





INTERSTATE 95



Map 11

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

A Glen Foerd

Torresdale at Delaware River. House and garden built circa 1850. Lutheran Retreat.

B Pennypack Bridge

Frankford Avenue over Pennypack Creek. Oldest extant bridge in Philadelphia. Arches built in 1697.





INTERSTATE 95

Map 12

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

A Vandegrift Burying Ground

Bristol Pike (US 13) and Cornwells Avenue, Andalusia. Cemetery established by early Dutch settlers. Circa 1776.

B Half-Way House

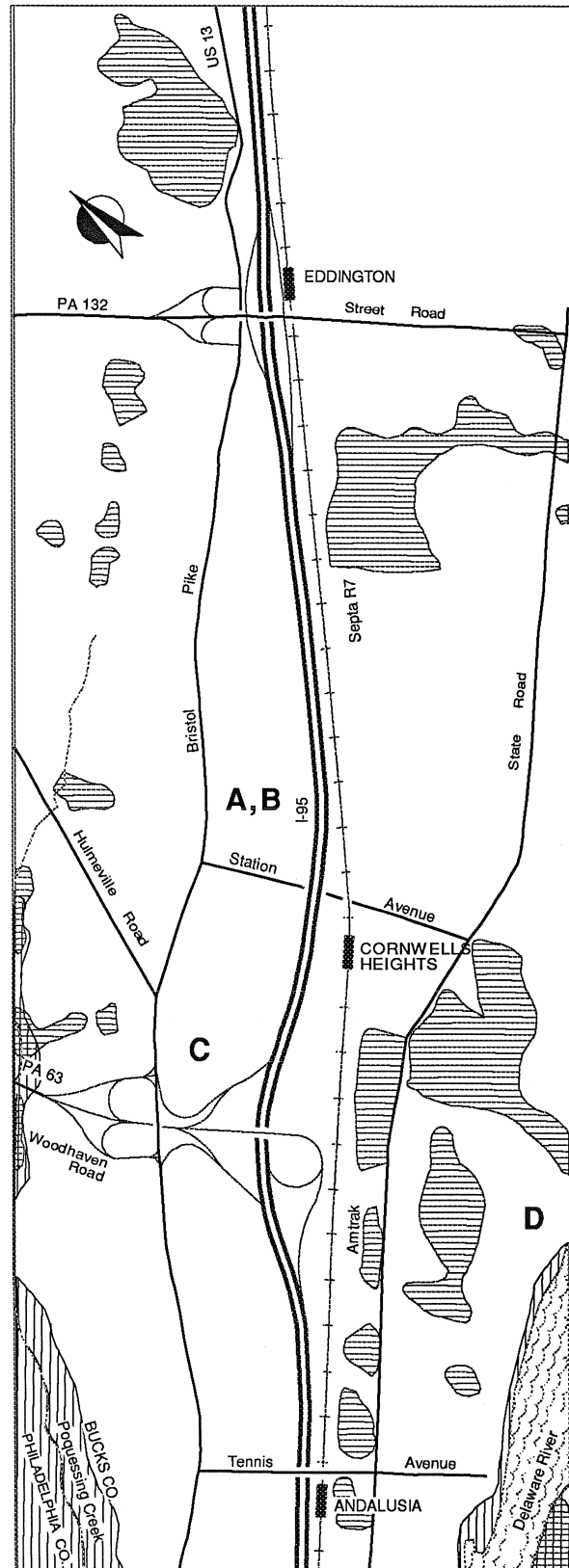
Bristol Pike (US 13) and Cornwells Avenue, Andalusia. House was licensed as an inn in 1744. Private.

C St. Elizabeth's Convent

Intersection of Interstate 95 and PA 63.

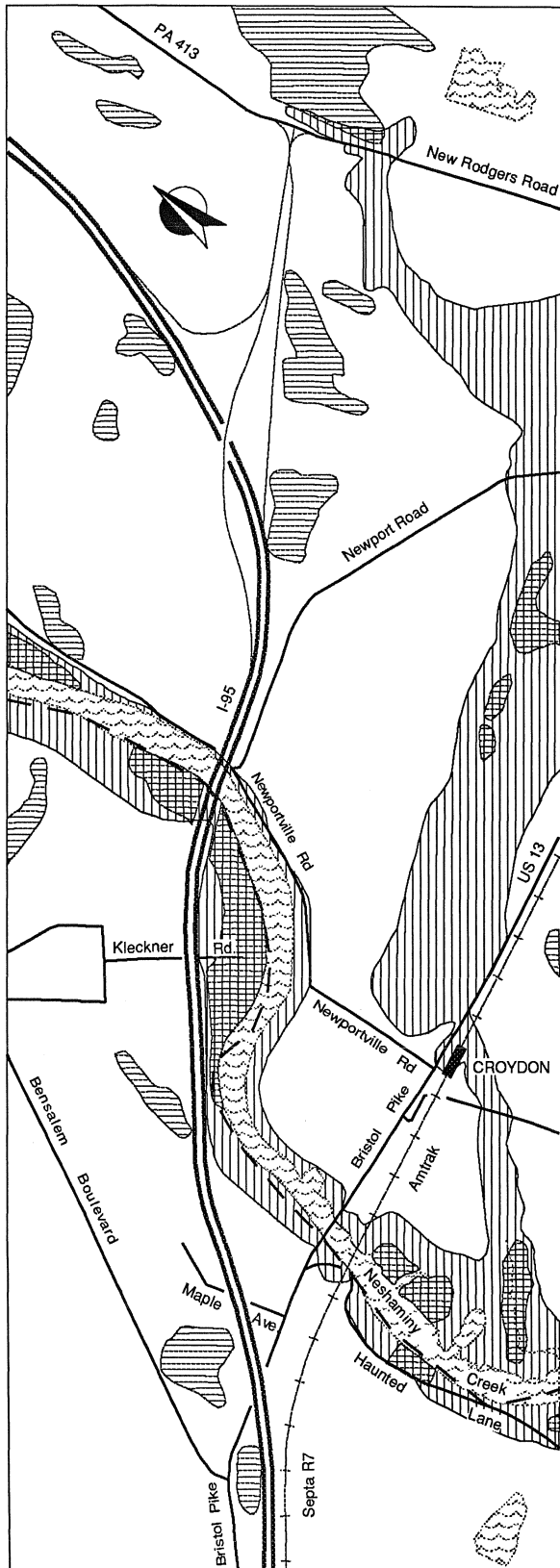
D Andalusia, Nicholas Biddle Estate

Off State Road, about 3/4 mile north of Philadelphia. Bought by John Craig in 1795, who named the estate "Andalusia". National Register Entry.





INTERSTATE 95



Map 13

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain





INTERSTATE 95

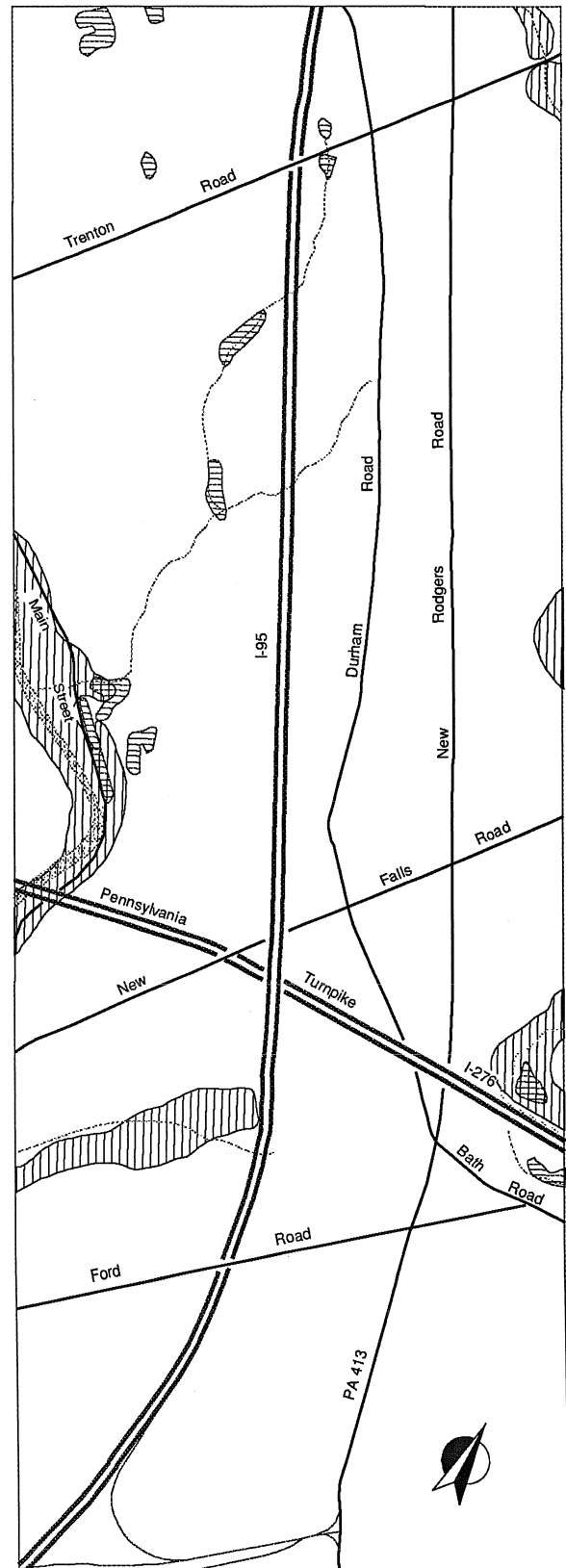
Map 14

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain





INTERSTATE 95

Map 16

Natural and Cultural Environment

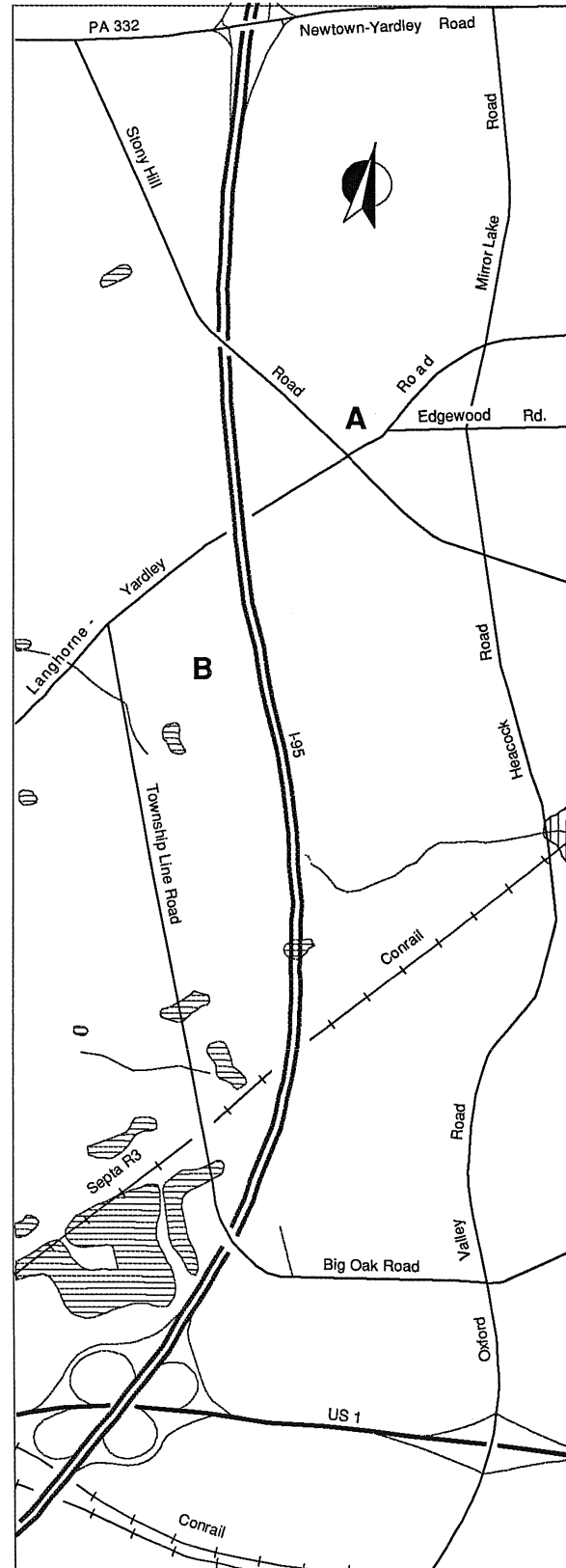
Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

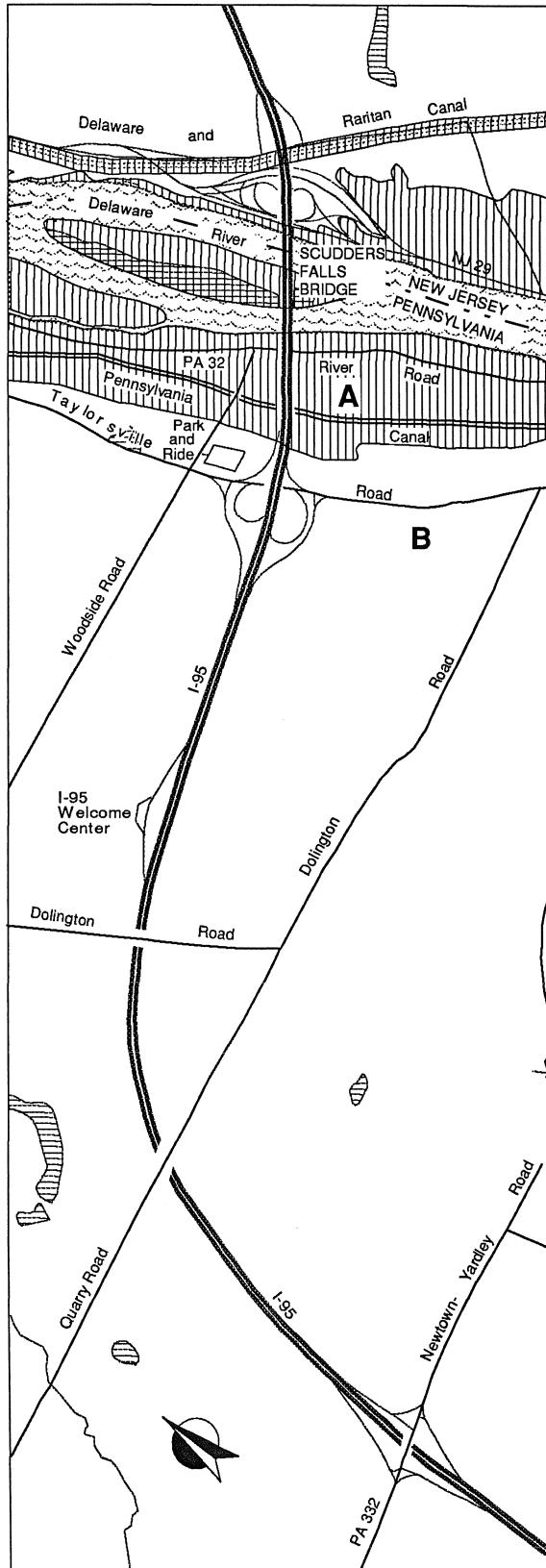
A Edgewood Historic District
Representative crossroads village. Private.

B Amos Palmer House
18th century farmhouse. Private.





INTERSTATE 95



Map 17

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

A Pennsylvania Canal

Canal and locks. Completed 1837.
Bureau of State Parks.

B Prospect Farm

Dolington Road, northwest of Yardley. Built by
William Yardley. 1682. Private.



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TRANSIT AND TRAFFIC DATA

These maps illustrate those transit routes which either cross over or under Interstate Route 95. Reference is made in the accompanying text to the route number, the origin and destination points, the road each route travels at the point of intersection, and whether the crossing is over or under I-95. All points of intersection are shown. There are no local transit routes currently using I-95. There are, however, long-distance carriers which operate scheduled service along I-95.

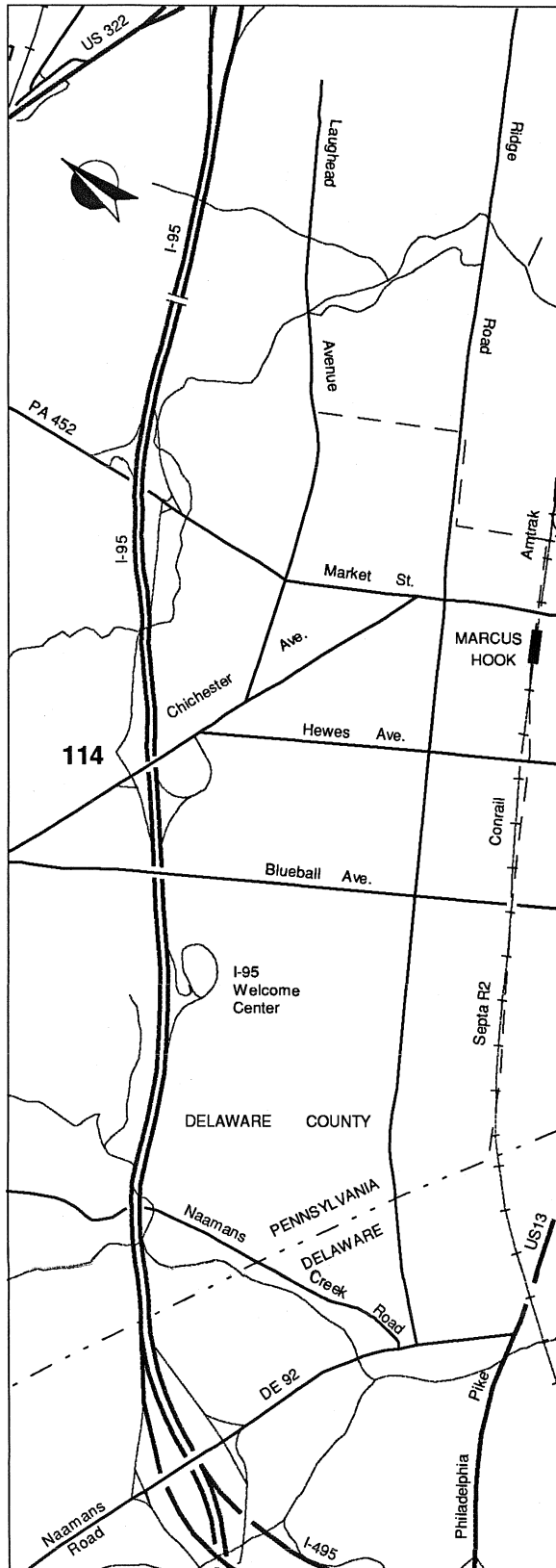
Average Annual Daily Traffic (AADT) counts are shown on the individual maps.

Information for this section was provided by Southeastern Pennsylvania Transportation Authority.





INTERSTATE 95



Map 1

Transit and Traffic Data

Scale: 1" = 2000'

Note: 1990 AADT:74,300

114 Darby Terminal to Boothwyn
crosses over at Chichester Ave.





INTERSTATE 95

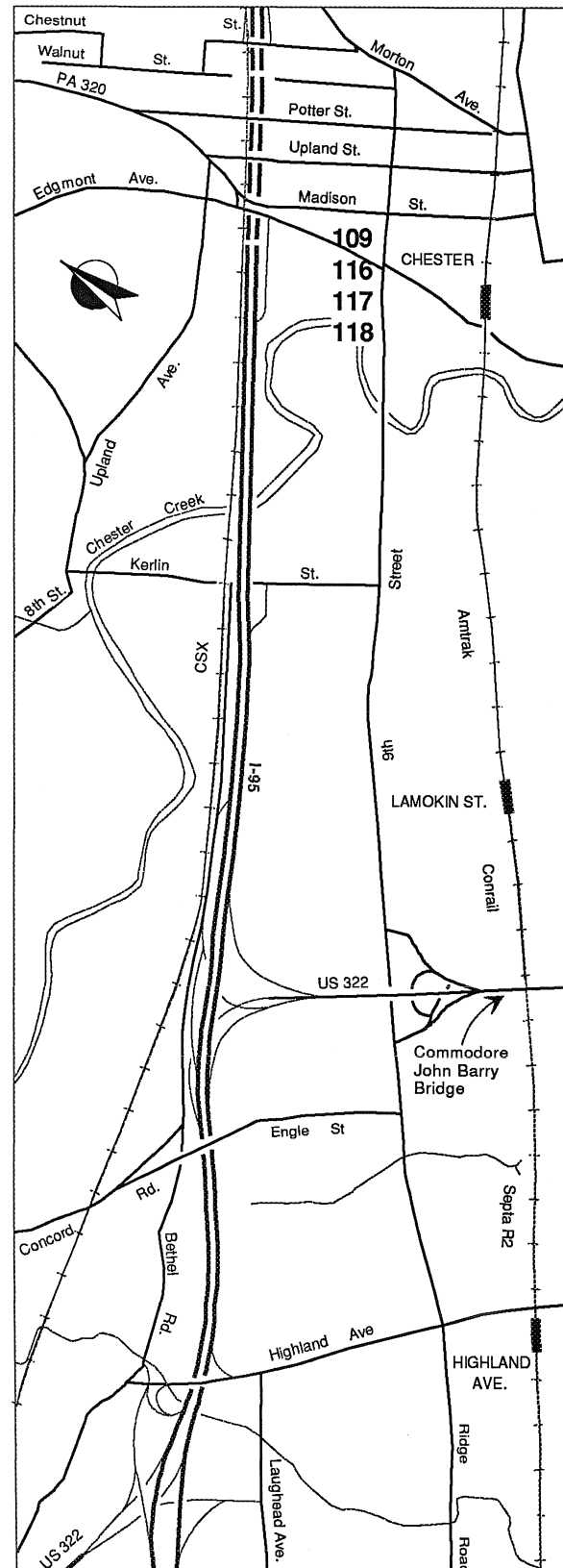
Map 2

Transit and Traffic Data

Scale: 1" = 2000'

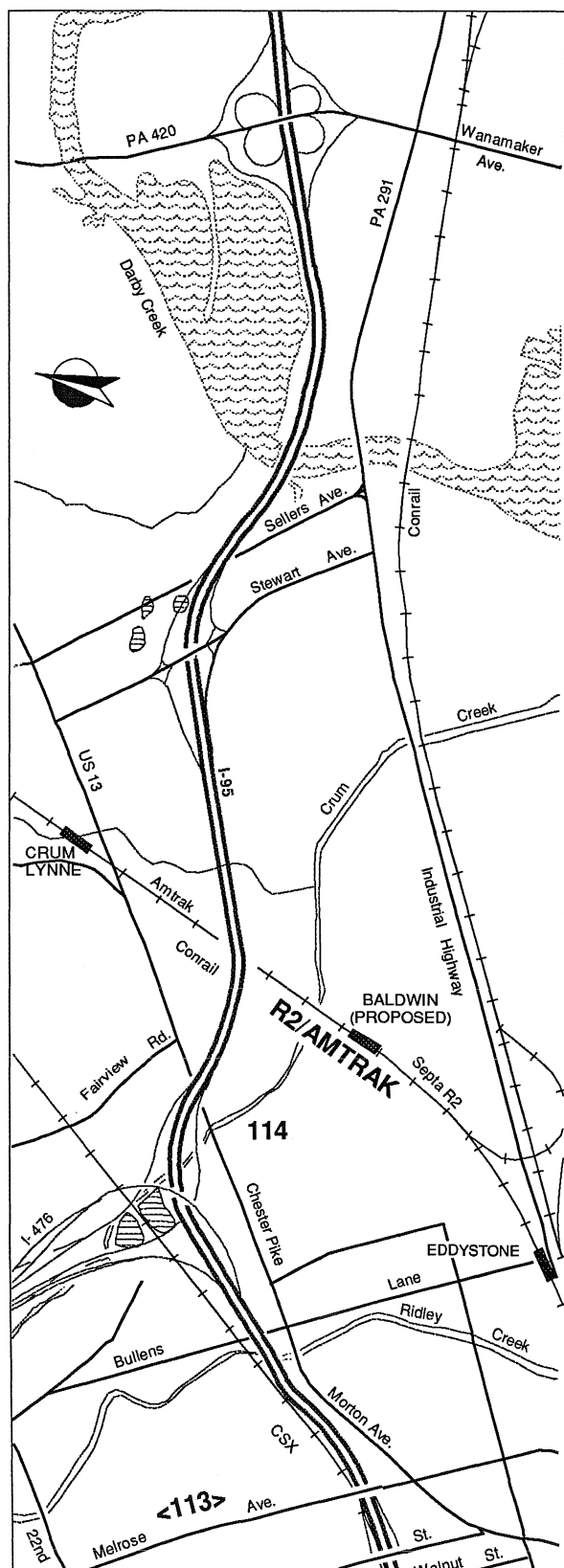
Note: 1990 AADT: 109,200

- 109** 69th Street Terminal to Chester
crosses over at Edgemont Avenue
- 116** Chester to Granite Run Mall
crosses over at Edgemont Avenue
- 117** Feltonville to West Chester
Note: 1990 AADT: 109,200
crosses over at Edgemont Avenue
- 118** Chester to King of Prussia
crosses over at Edgemont Avenue





INTERSTATE 95



Map 3

Transit and Traffic Data

Scale: 1" = 2000'

Note: 1990 AADT south of I-476: 98,800. 1990 AADT between I-476 and Stewart Avenue: 106,900. 1990 AADT north of PA 420: 110,800.

113 69th Street Terminal to Marcus Hook crosses over at Melrose Avenue

114 Darby Terminal to Boothwyn crosses under at Chester Pike

R2/AMTRAK Regional Rail to Wilmington and Intercity Rail to Washington, DC crosses under east of Chester Pike





INTERSTATE 95

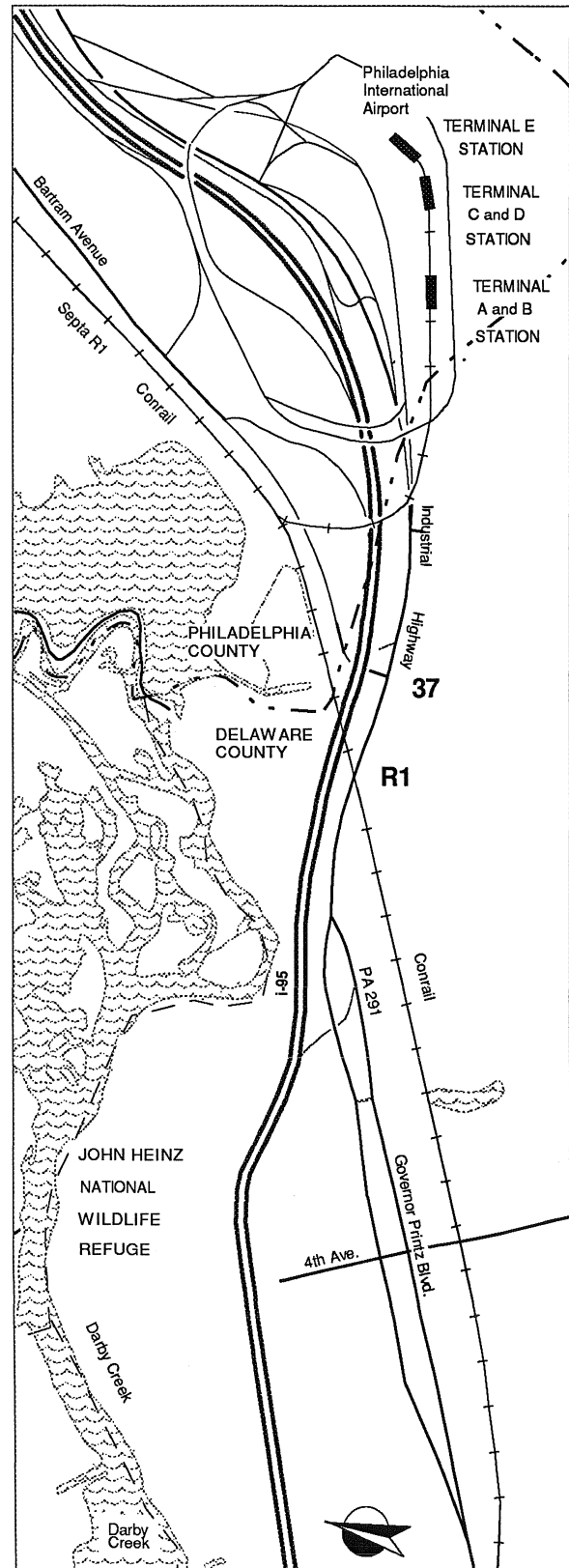
Map 4

Transit and Traffic Data

Scale: 1" = 2000'

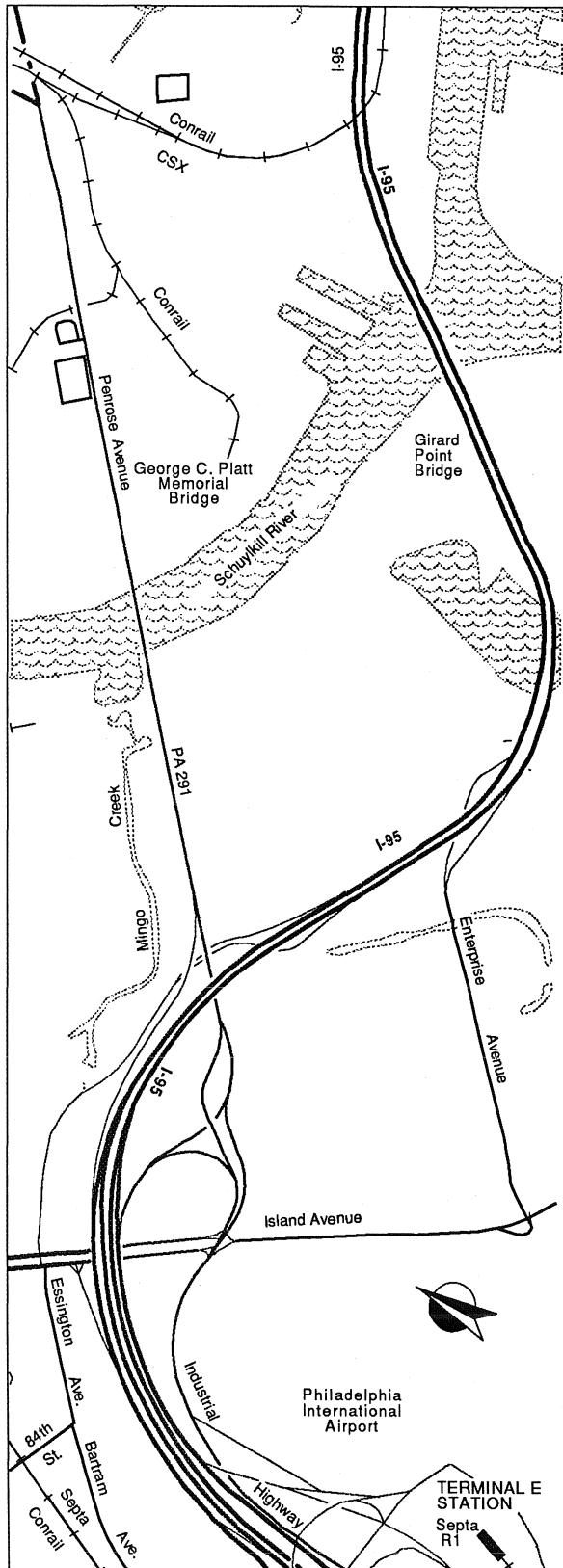
Note: 1990 AADT: 110,800.

- 37** South Philadelphia to Chester crosses under at Bartram Avenue
- R1** Regional Rail to Philadelphia International Airport crosses under west of Bartram Avenue





INTERSTATE 95



Map 5

Transit and Traffic Data

Scale: 1" = 2000'

Note: 1990 AADT south of Phila International Airport: 110,800. 1990 AADT north of Enterprise Avenue: 97,900.

NO TRANSIT





INTERSTATE 95

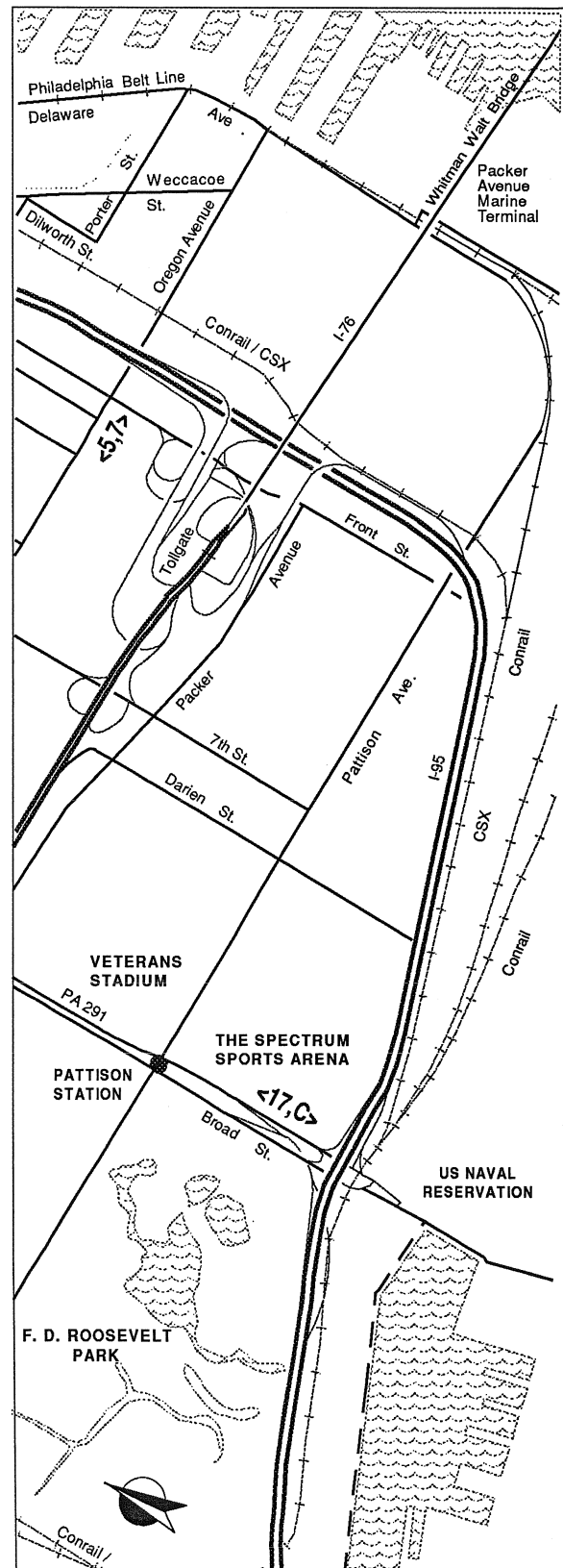
Map 6

Transit and Traffic Data

Scale: 1" = 2000'

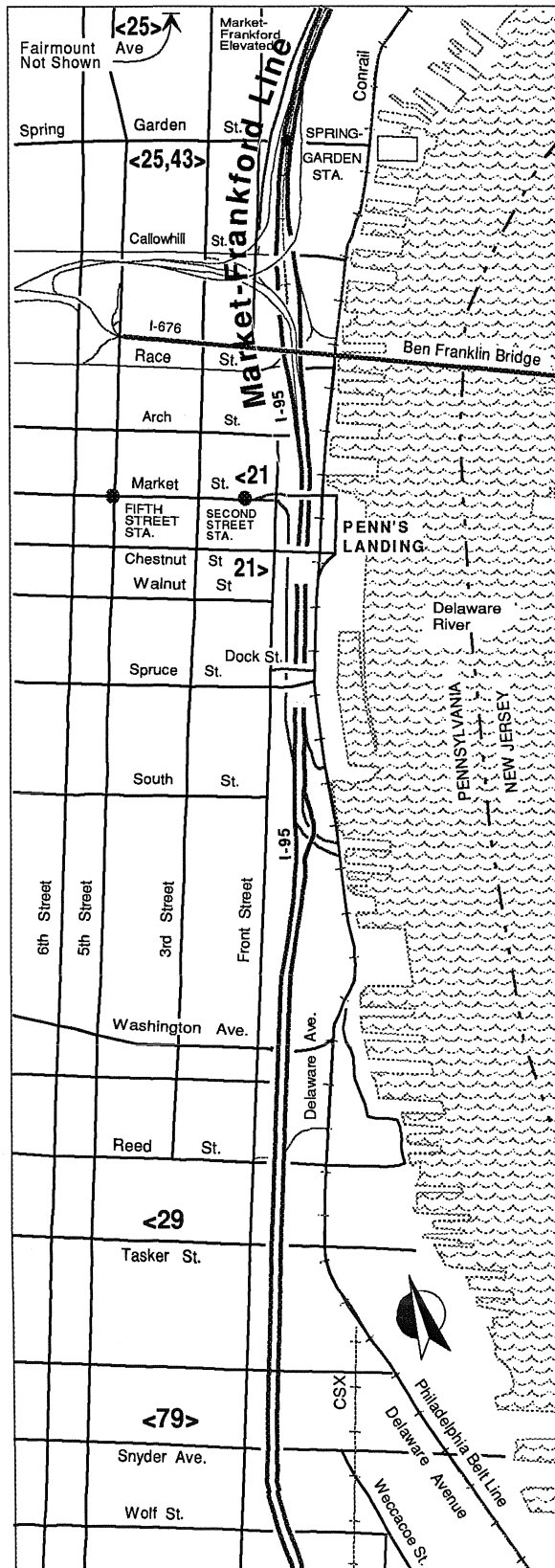
Note: 1990 AADT south of Broad Street: 97,900.
1990 AADT north of Broad Street: 94,700.

- 5** Frankford Terminal to South Philadelphia crosses under at Oregon Avenue
- 7** Strawberry Mansion and Tioga to South Philadelphia crosses under at Oregon Avenue
- 17** South Philadelphia to Penn's Landing crosses under at Broad Street and at 11th Street (not shown)
- C** West Oak Lane and Fern Rock to Center City and South Philadelphia crosses under at Broad Street





INTERSTATE 95



Map 7

Transit and Traffic Data

Scale: 1" = 2000'

Note: 1990 AADT south of Vine Street: 88,300.
1990 AADT north of Vine Street: 138,500.

- 21 69th Street Terminal to Penn's Landing crosses over at Market Street and at Chestnut Street
- 25 Northern Liberties to Bridesburg crosses under at Fairmount Avenue and at Spring Garden Street
- 29 South Philadelphia Crosstown crosses under at Morris Street and at Tasker Street
- 43 Parkside to Northern Liberties crosses under at Spring Garden Street
- 79 South Philadelphia Crosstown crosses under at Snyder Avenue

Market-Frankford Line Light Rail

69th Street Terminal to Frankford Terminal travels along I-95 between Market Street and Germantown Avenue





INTERSTATE 95

Map 8

Transit and Traffic Data

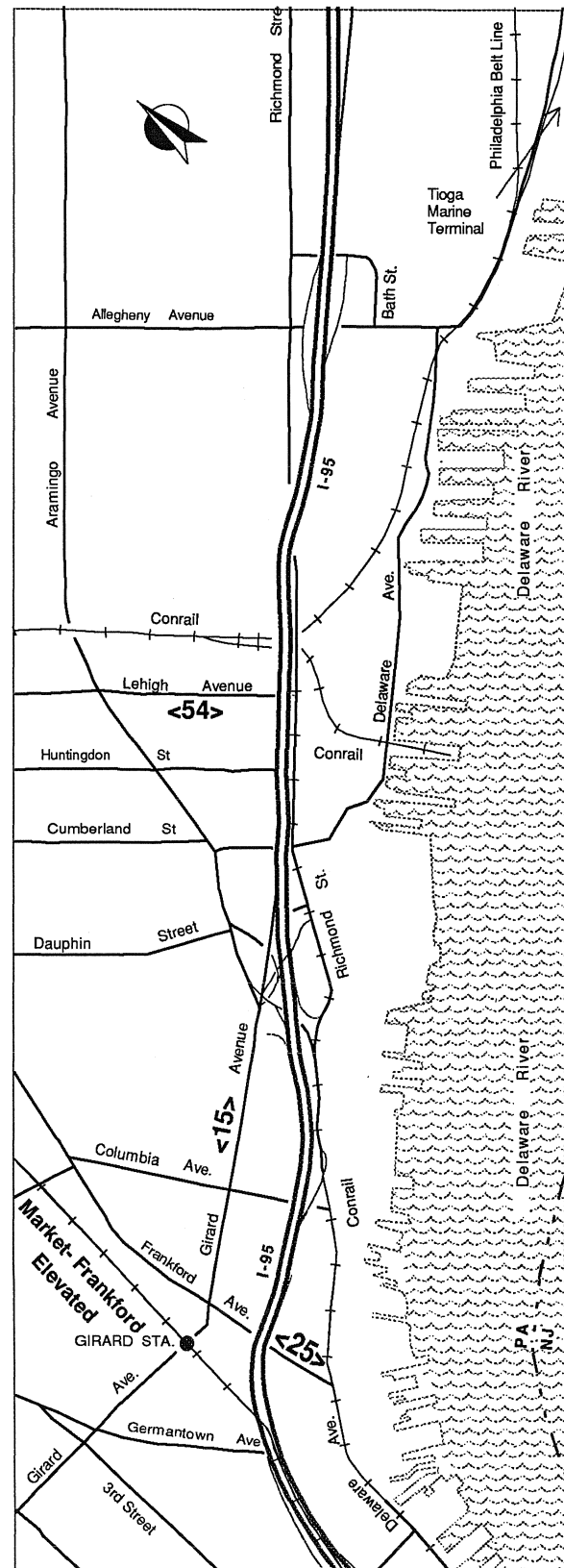
Scale: 1" = 2000'

Note: 1990 AADT south of Girard Avenue 138,500.
1990 AADT north of Girard Avenue: 151,100.

- 15** Port Richmond to Haddington crosses under at Girard Avenue
- 25** Northern Liberties to Bridesburg crosses under at Frankford Avenue
- 54** Port Richmond to Strawberry Mansion crosses under at Lehigh Avenue

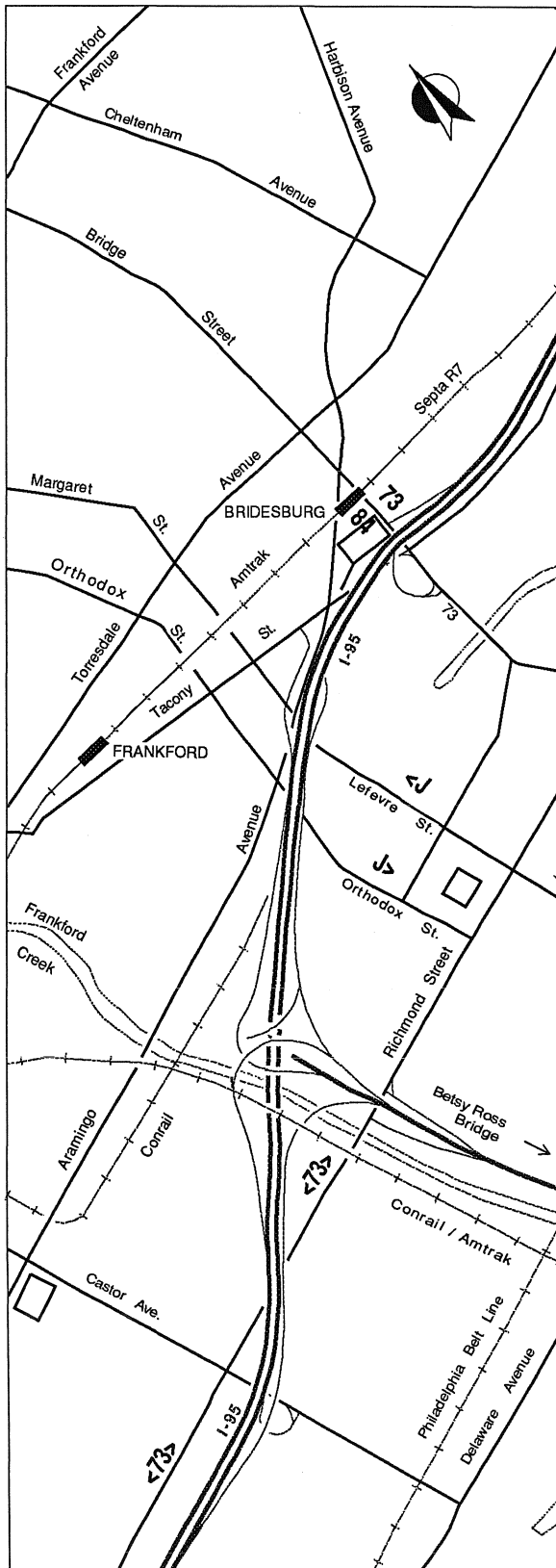
Market-Frankford Line Light Rail

69th Street Terminal to Frankford Terminal
travels along I-95 between Market Street and
Germantown Avenue





INTERSTATE 95



Map 9

Transit and Traffic Data

Scale: 1" = 2000'

Note: 1990 AADT north of Bridge Street: 126,600.

- 73** Frankford Terminal to Port Richmond crosses under at Bridge Street and at Richmond Street
- 84** Frankford Terminal to Franklin Mills and Somerton crosses under at Bridge Street
- J** Bridesburg to Germantown crosses under at Lefevre Street and at Orthodox Street





INTERSTATE 95

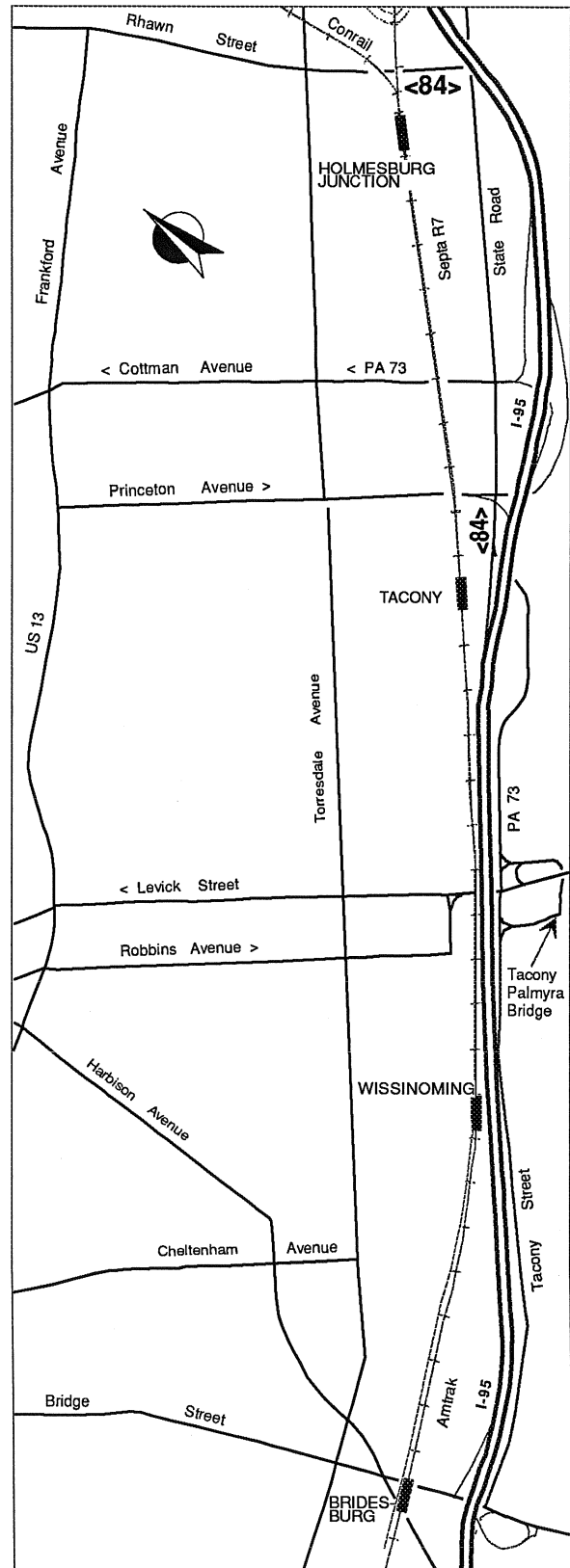
Map 10

Transit and Traffic Data

Scale: 1" = 2000'

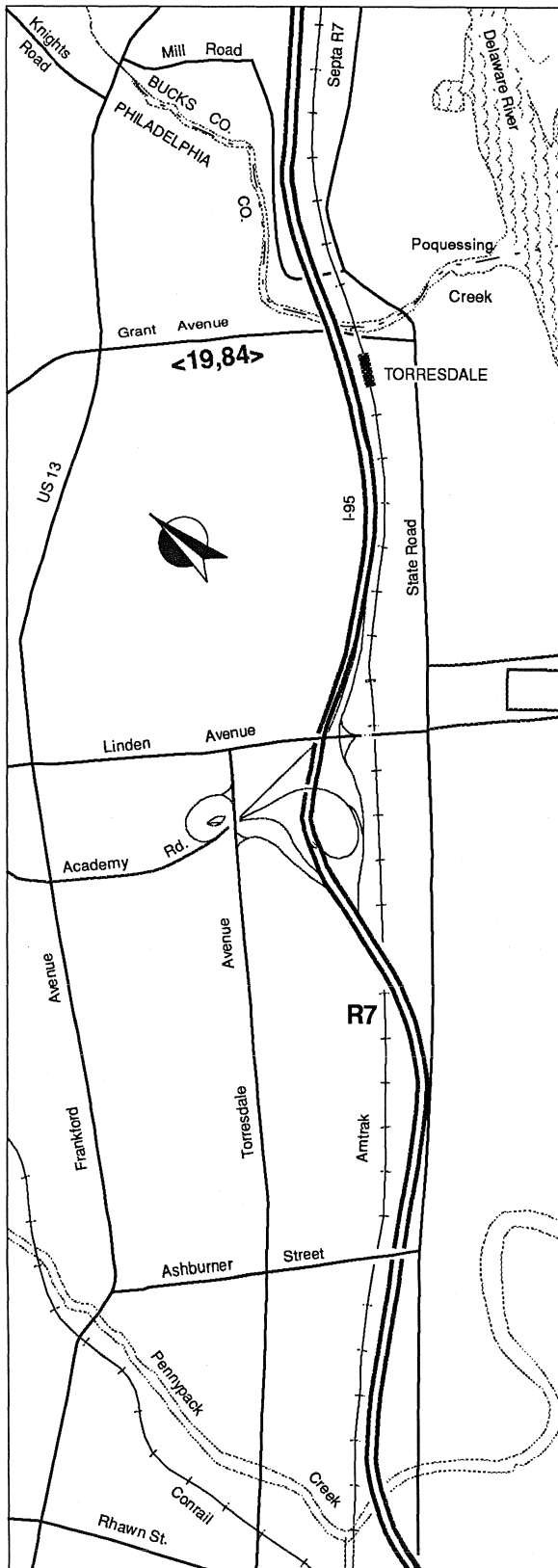
Note: 1990 AADT south of Betsy Ross Bridge: 126,600. 1990 AADT north of Cottman Avenue: 148,600.

84 Frankford Terminal to Franklin Mills and Somerton crosses under at Rhawn Street and State Road





INTERSTATE 95



Map 11

Transit and Traffic Data

Scale: 1" = 2000'

Note: 1990 AADT south of Academy Road: 148,600.

- 19** Frankford Terminal to Torresdale Station crosses under at Grant Avenue
- 84** Frankford Terminal to Franklin Mills and Somerton crosses under at Grant Avenue
- R7** Regional Rail to Trenton crosses under north of Ashburner Street





INTERSTATE 95

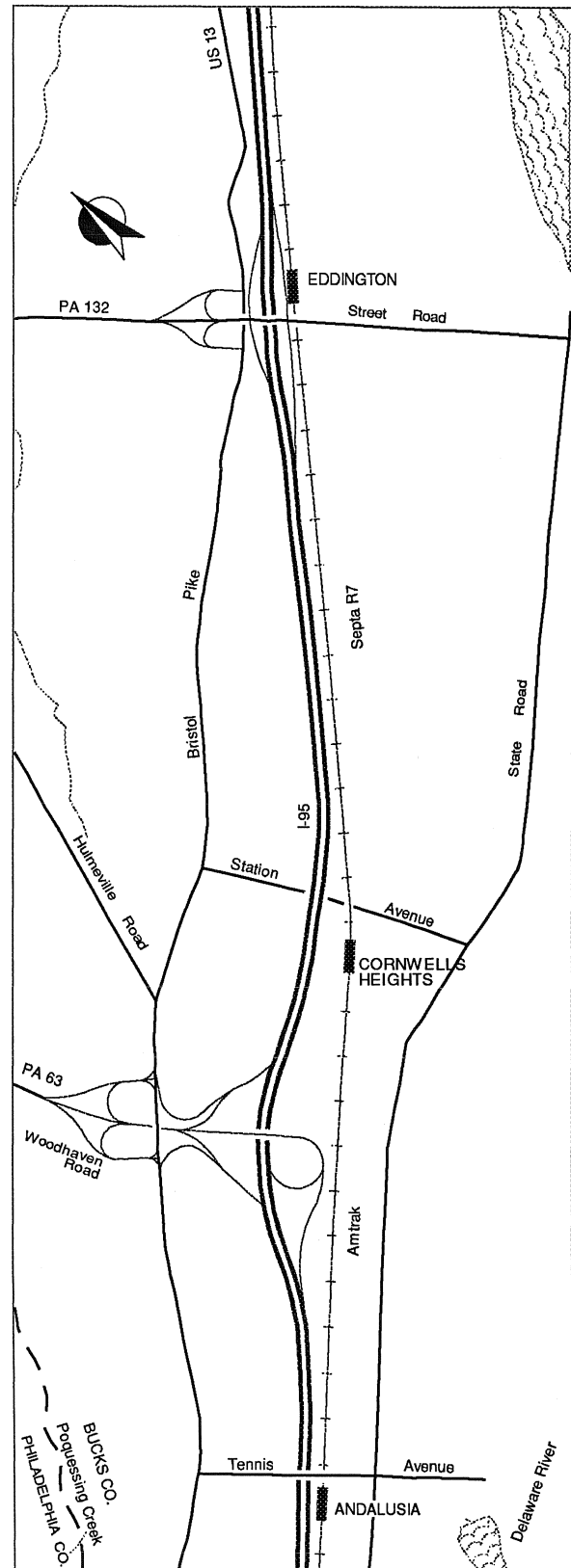
Map 12

Transit and Traffic Data

Scale: 1" = 2000'

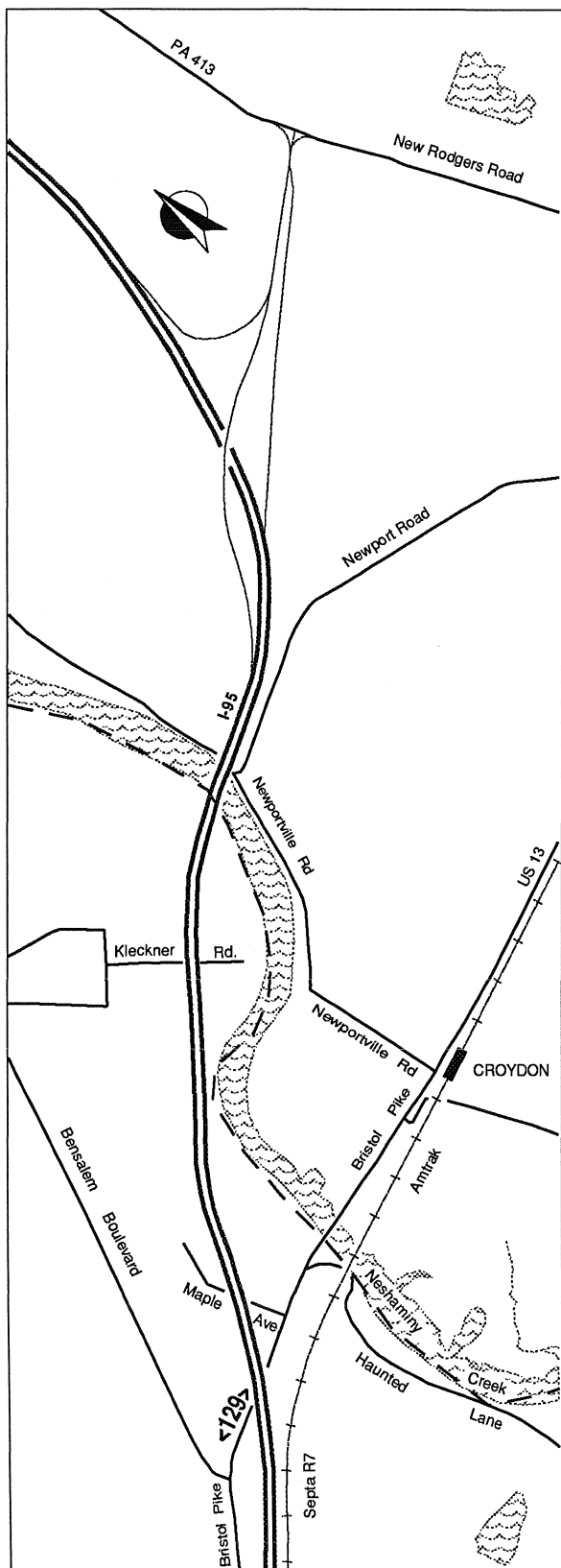
Note: 1990 AADT: 82,800.

NO TRANSIT





INTERSTATE 95



Map 13

Transit and Traffic Data

Scale: 1" = 2000'

Note: 1990 AADT: 65,600.

129 Torresdale to Oxford Valley Mall crosses under at Bristol Pike (US 13)





INTERSTATE 95

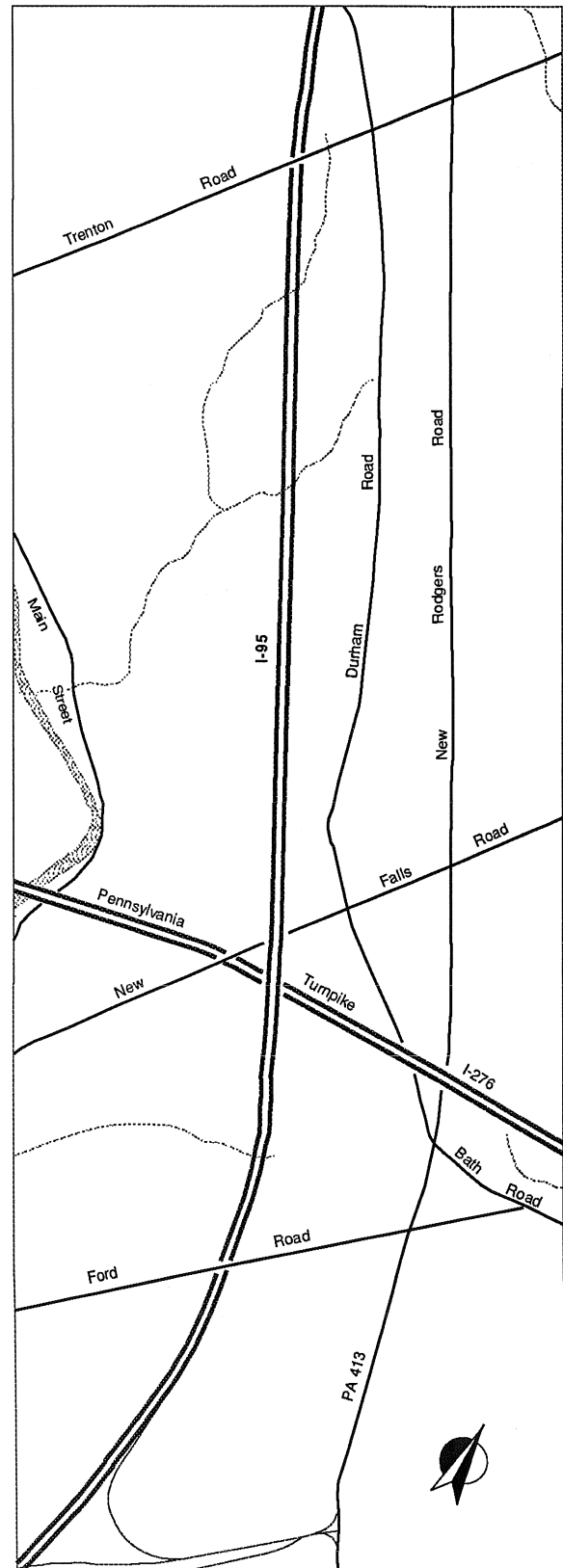
Map 14

Transit and Traffic Data

Scale: 1" = 2000'

Note: 1990 AADT: 65,600

NO TRANSIT





INTERSTATE 95

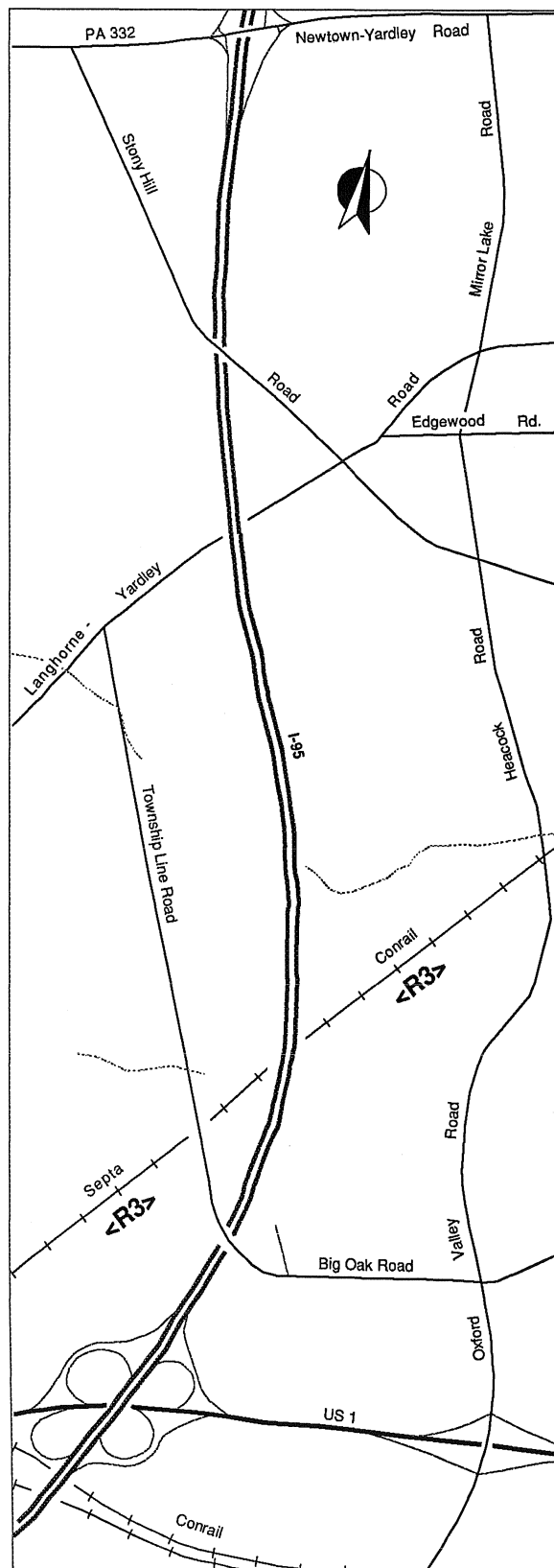
Map 16

Transit and Traffic Data

Scale: 1" = 2000'

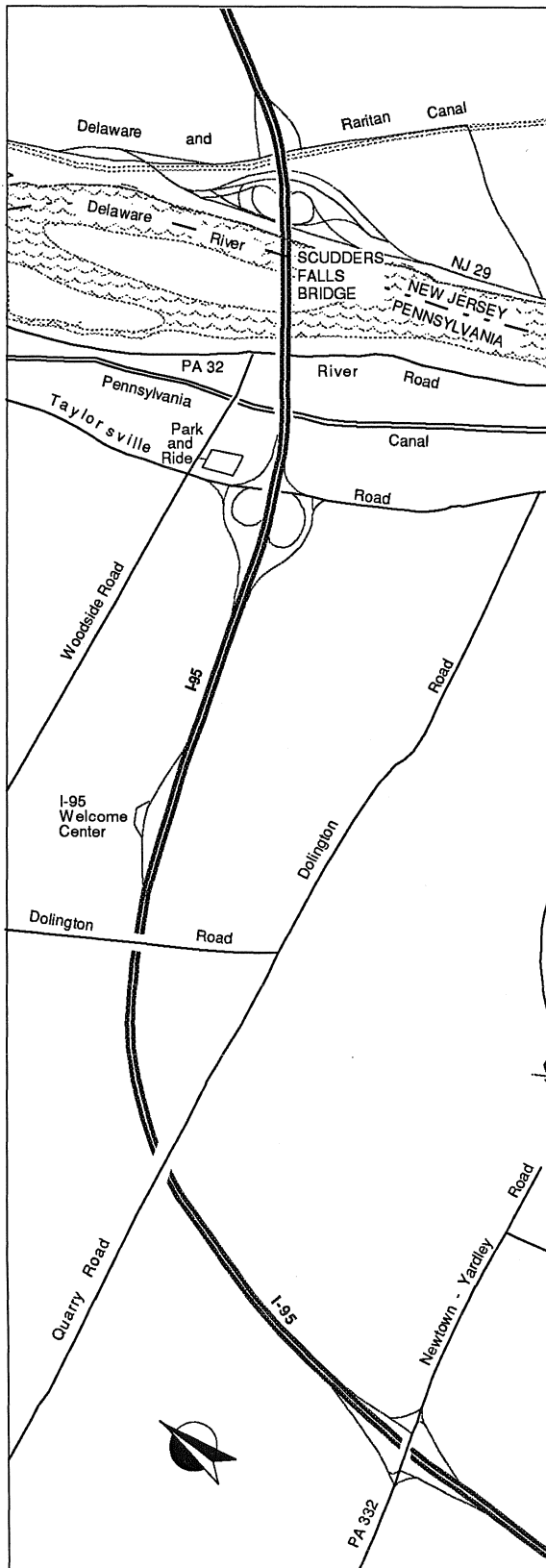
Note: 1990 AADT south of US 1 Business: 44,200

R3 Regional Rail to West Trenton crosses under north of Big Oak Road





INTERSTATE 95



Map 17

Transit and Traffic Data

Scale: 1" = 2000'

Note: 1990 AADT: 52,400

NO TRANSIT



SECTION THREE

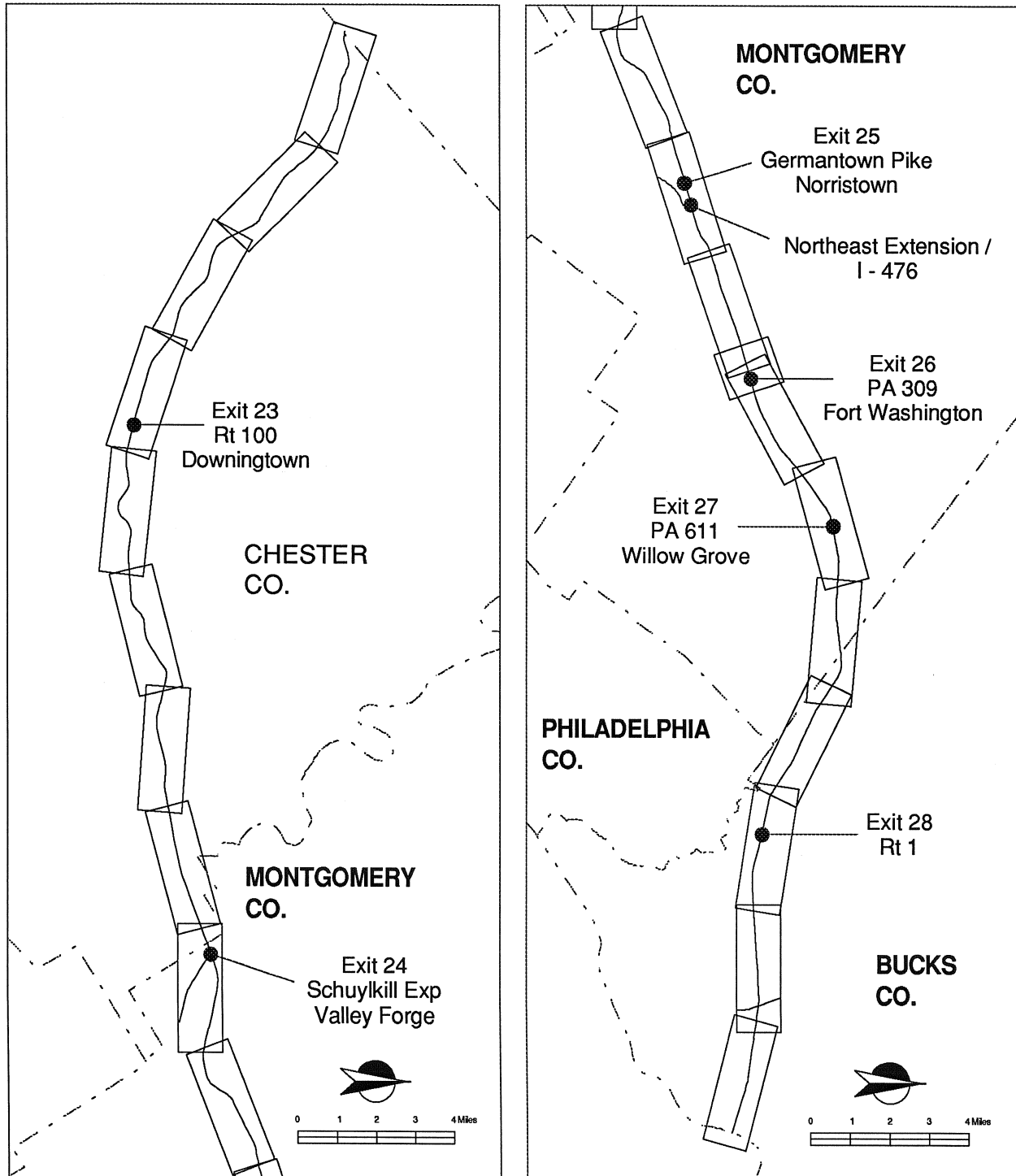


THE PENNSYLVANIA TURNPIKE



THE PENNSYLVANIA TURNPIKE

Figure 4



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Thank You

DESIGN FEATURES

This series of maps inventories the current physical attributes, including physical characteristics and structural information, of the Pennsylvania Turnpike. The roadway itself is two lanes each direction west of the Plymouth Meeting interchange, three lanes by direction east of that point to the Philadelphia interchange, and two lanes by direction east of the Philadelphia interchange to the end of the Turnpike at the Delaware River. Travel lanes are 12' in width, with median strip and wide shoulders the entire length of the road. The three lane section is surrounded for most of its length by sound barrier walls.

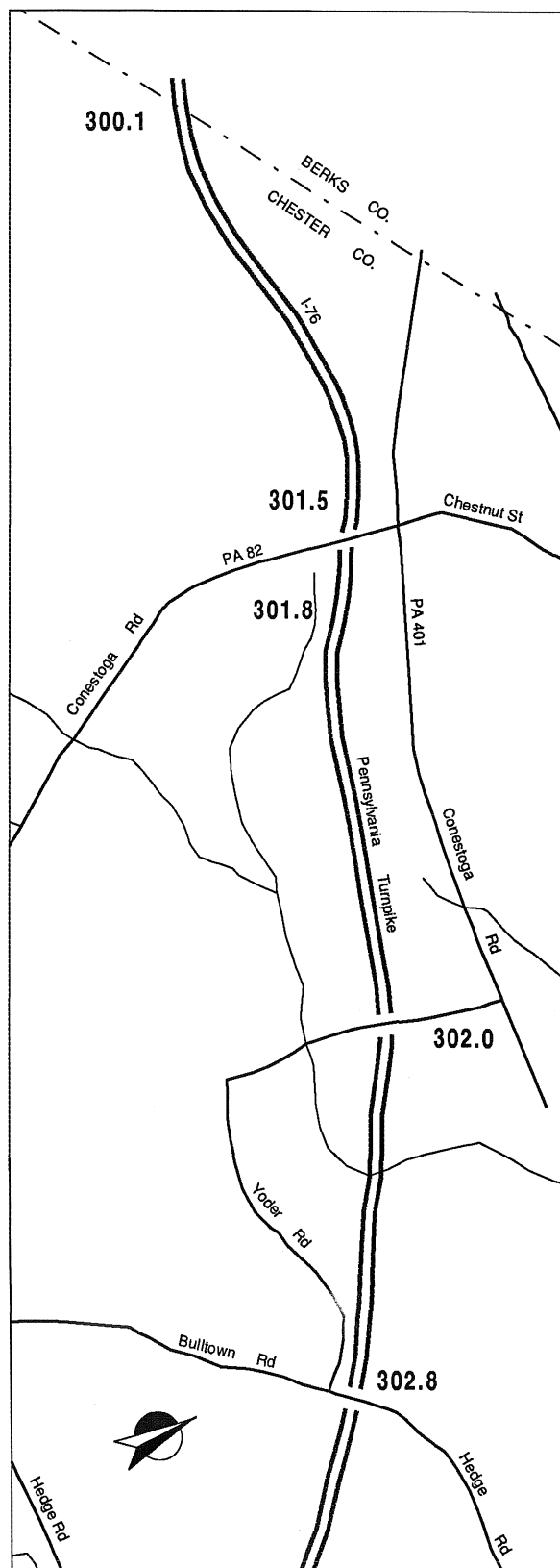
Turnpike milepost markers on this highway run west to east, and are shown on these maps accordingly. General information about the section of road shown in each map is located underneath the title block. Milepost numbers posted on the maps correspond to locations of structures which are described beside each map. It should be noted that, as of this writing, milepost marker points had not yet been assigned to the I-476/Northeast Extension ramps crossing the mainline Turnpike. Structure locations are identified and described on that map, however, exact location are not available.

Specific information regarding structures is provided as necessary. Citations are made for either bridges or overpasses. For bridges, structure number, span length, and deck width are given, as well as information regarding the underneath facility. For structures overpassing the Pennsylvania Turnpike, the distance between piers is given in exact measurements (where available) or is listed according the location of the piers. Structure number and description of facility carried are also given. Due to the interstate status of this highway, there are no sub-standard weight or clearance structures.

Information on these maps was gleaned from a combination of construction drawings, Pennsylvania Turnpike Authority data sheets, and field observations.



THE PENNSYLVANIA TURNPIKE



Map 1

Design Features

Scale: 1" = 2000'

Note: Two EBD and two WBD 12' travel lanes. Shoulder width 10' and variable. Median width 10'. Combination concrete barrier and guide rail barrier in median.

300.1 Overpass

Township Line Road
EB# 700
Structure Length: 90'
Turnpike Width: 78'

301.5 Overpass

PA 82 SR0082 LR328
EB# 701
Structure Length: 92'
Turnpike Width: 78'

301.8 Bridge

Over Abandoned Conrail (not shown)
EB# 702
Span Length: 152'
Deck Width: 68.5'

302.0 Bridge

over Yoder Road
EB# 703
Span Length: 31'
Deck Width: 69.3'

302.8 Overpass

Bulltown Road SR 0345 LR 15144
EB# 704
Structure Length: 88'
Turnpike Width: 78'





THE PENNSYLVANIA TURNPIKE

Map 2

Design Features

Scale: 1" = 2000'

Note: Two EBD and two WBD 12' travel lanes. Shoulder width 10' and variable. Median width 10'. Combination concrete barrier and guiderail barrier in median.

303.3 Overpass

Hedge Road

EB# 705

Structure Length: 88'

Turnpike Width: 78'

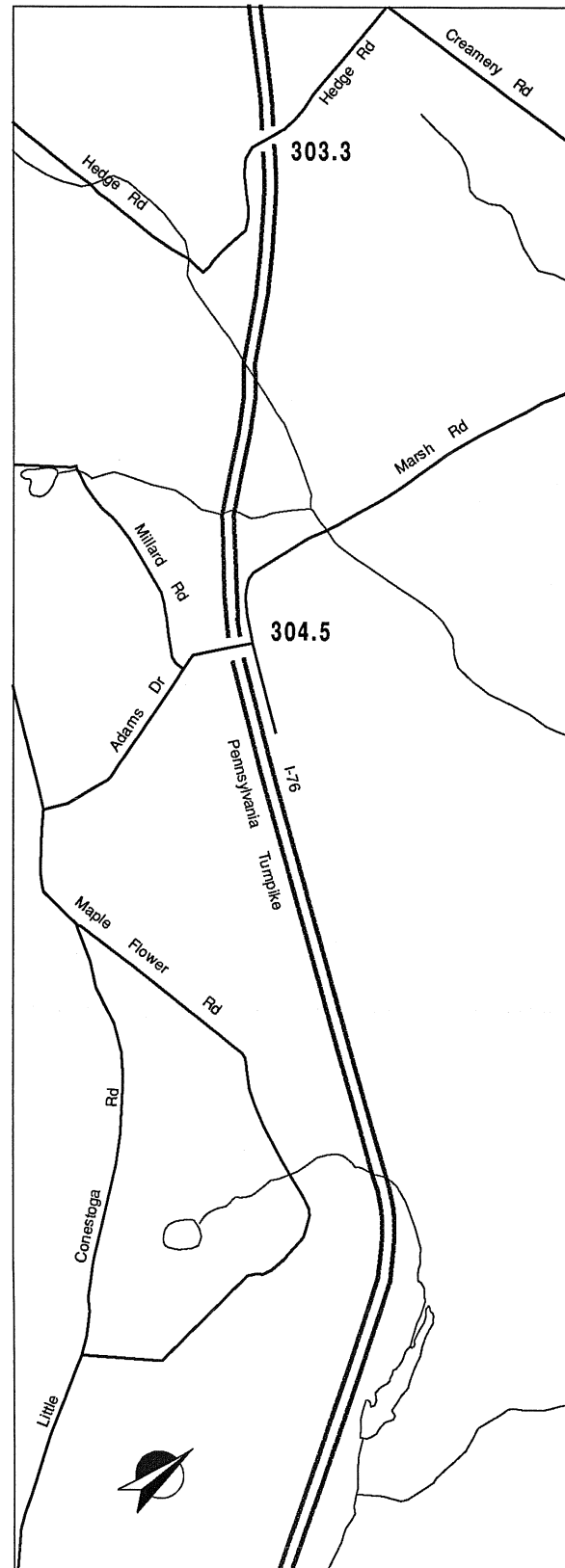
304.5 Overpass

Adams Drive

EB# 707

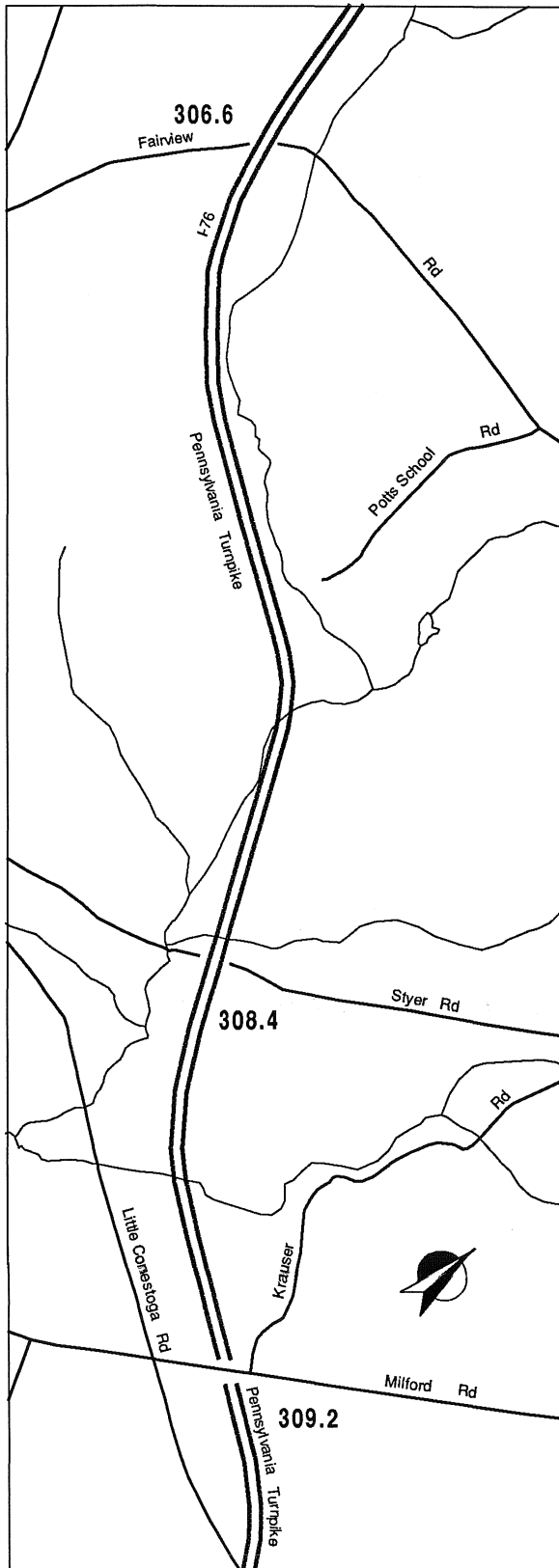
Structure Length: 82'

Turnpike Width: 78'





THE PENNSYLVANIA TURNPIKE



Map 3

Design Features

Scale: 1" = 2000'

Note: Two EBD and two WBD 12' travel lanes. Shoulder width 10' and variable. Median width 10'. Combination concrete barrier and guiderail barrier in median.

306.6 Bridge

Over Fairview Road SR4031 LR15148

EB# 708

Span Length: 43'

Deck Width: 72.3'

308.4 Bridge

Over Styer Road

EB# 715

Span Length: 26'

Deck Width: 81'

309.2 Overpass

Milford Road SR4045 LR15203

EB# 717

Structure Length: 93'

Turnpike Width: 78'





THE PENNSYLVANIA TURNPIKE

Map 4

Design Features

Scale: 1" = 2000'

Note: Two EBD and two WBD 12' travel lanes. Shoulder width 10' and variable. Median width 10'. Combination concrete barrier and guiderail barrier in median.

309.6 Bridge

Over Little Conestoga Road SR4016 LR15052

EB# 718

Span Length: 46'

Deck Width: 71.8'

310.3 Overpass

Park Road SR4035 LR15018

EB# 719

Structure Length: 101'

Turnpike Width: 78.69'

311.3 Bridge

Over Pottstown Pike PA100 SR0100 LR147

EB# 720

Span Length: 178'

Deck Width: 70'

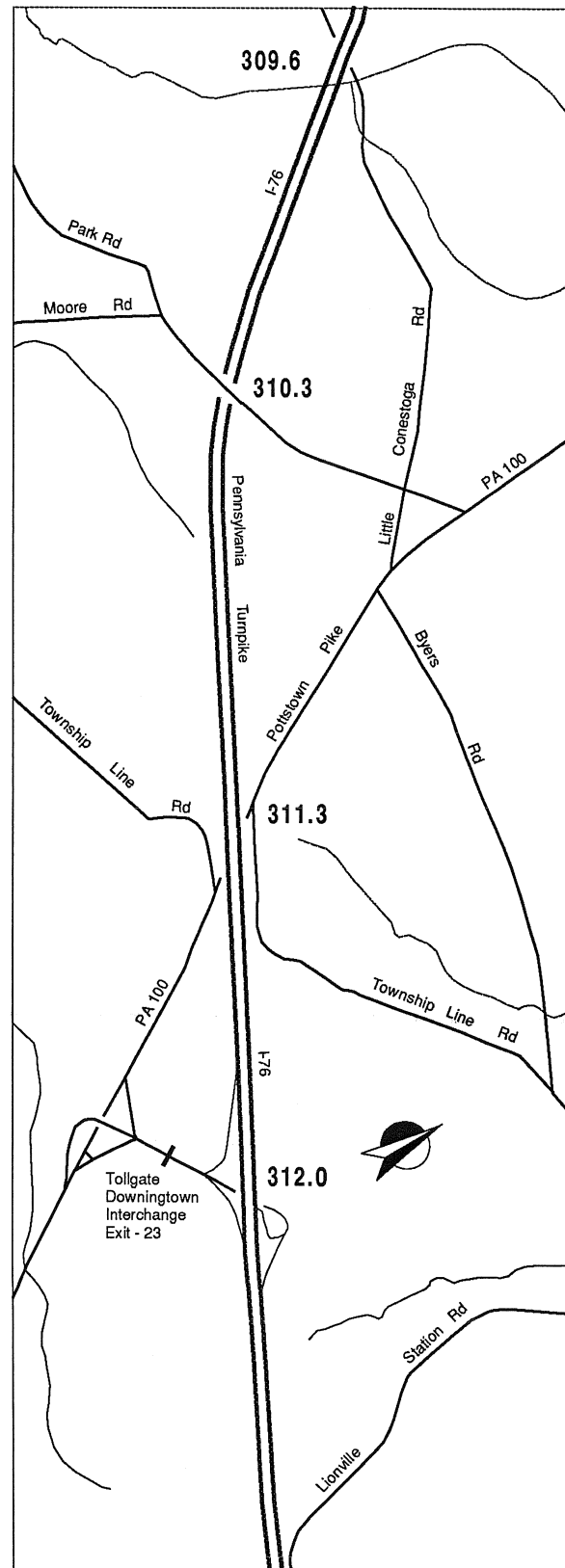
312.0 Bridge

Over Downingtown Interchange

DB# 721

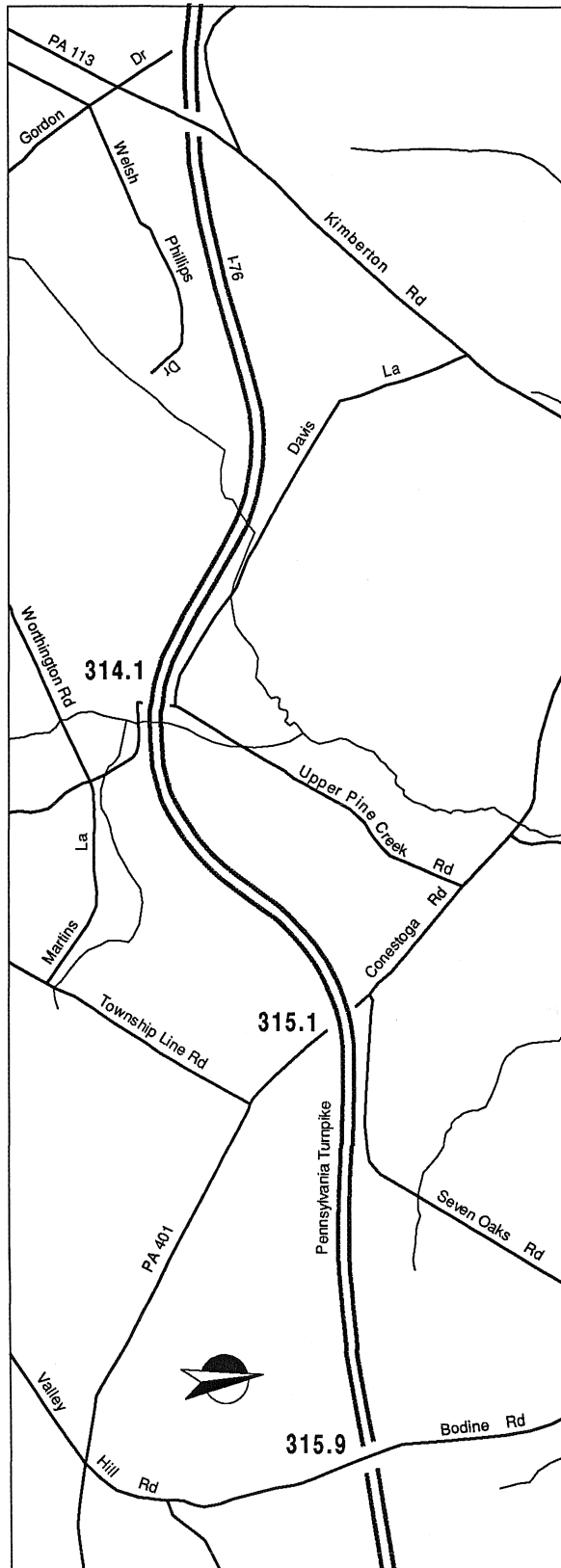
Span Length: 52'

Deck Width: 88'





THE PENNSYLVANIA TURNPIKE



Map 5

Design Features

Scale: 1" = 2000'

Note: Two EBD and two WBD 12' travel lanes. Shoulder width 10' and variable. Median width 10'. Combination concrete barrier and guiderail barrier in median.

312.9 Overpass

Kimberton Road SR0113 LR270

EB# 722

Structure Length: 102'

Turnpike Width: 89'

314.1 Bridge

Over Upper Pine Creek Road

EB# 724

Span Length: 25'

Deck Width: 68.6'

315.1 Bridge

Over Conestoga Road SR0401 LR15139

EB# 726

Span Length: 34'

Deck Width: 68.6'

315.9 Overpass

Bodine Road

EB# 728

Structure Length: 83'

Turnpike Width: n/a





THE PENNSYLVANIA TURNPIKE

Map 6

Design Features

Scale: 1" = 2000'

Note: Two EBD and two WBD 12' travel lanes. Shoulder width 10' and variable. Median width 10'. Combination concrete barrier and guiderail barrier in median.

316.6 Overpass

Valley Hill Road SR 1021 LR 15054

EB# 729

Structure Length: 122'

Turnpike Length: 78'

317.4 Overpass

Yellow Springs Road

EB# 730

Structure Length: 82'

Turnpike Width: 78'

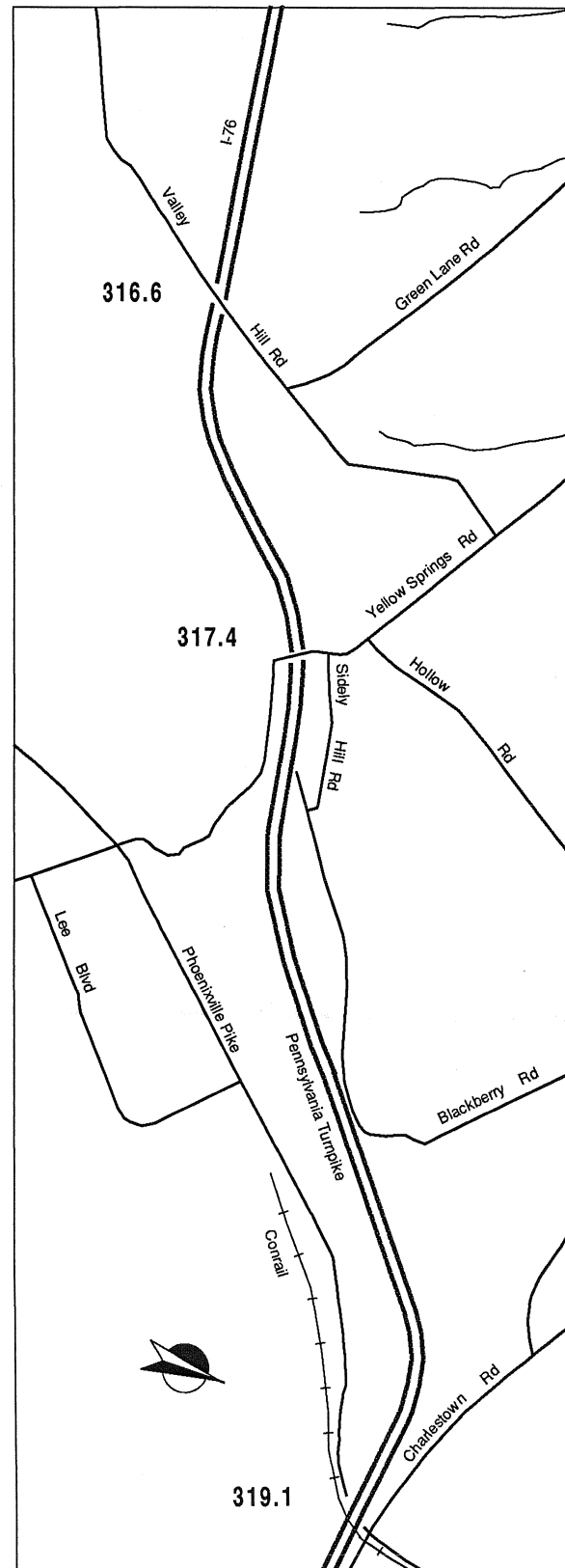
319.1 Bridge

Over West Chester Road SR 1003 LR 202 and Conrail

EB# 731

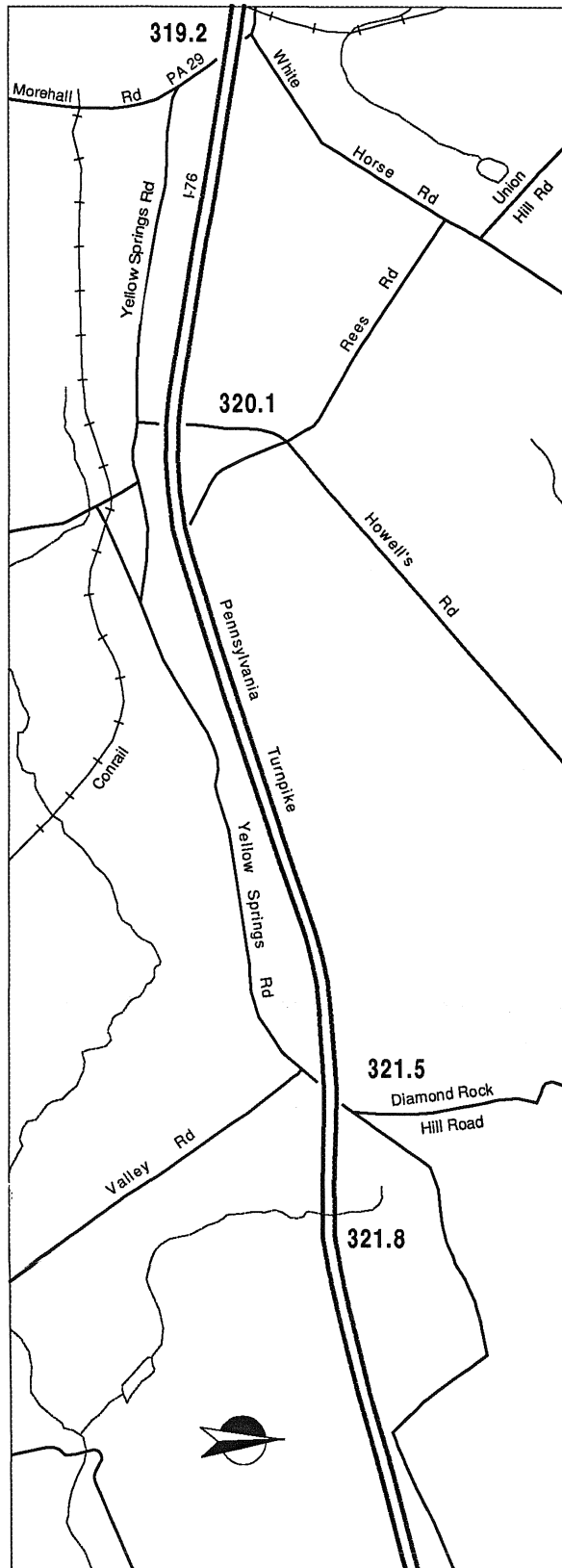
Span Length: 274'

Deck Width: 53.3'





THE PENNSYLVANIA TURNPIKE



Map 7

Design Features

Scale: 1" = 2000'

Note: Two EBD and two WBD 12' travel lanes. Shoulder width 10' and variable. Median width 10'. Combination concrete barrier and guiderail barrier in median.

319.2 Bridge

Over Morehall Road SR 0029 LR15134

EB# 732

Span Length: 32'

Deck Width: 68.8'

320.1 Bridge

Over Howell Road

EB# 733

Span Length: 30'

Deck Width: 67.7'

321.5 Bridge

Over Yellow Springs Road SR 1005 LR 15050

EB# 734

Span Length: 54'

Deck Width: 67.8'





THE PENNSYLVANIA TURNPIKE

Map 8

Design Features

Scale: 1" = 2000'

Note: Two EBD and two WBD 12' travel lanes. Shoulder width 10' and variable. Median width 10'. Combination concrete barrier and guiderail barrier in median.

322.6 Overpass

Mill Road

EB# 735

Structure Length: 93'

Turnpike Width: 83.19'

323.4 Bridge

Over Valley Creek

EB# 736

Span Length: 64'

Deck Width: 68.2'

323.7 Bridge

Over Wilson Road

EB# 737

Span Length: 32'

Deck Width: 68'

324.3 Overpass

Valley Forge Road PA 252 SR 0252 LR 142

EB# 739

Structure Length: 95'

Turnpike Width: 79.5'

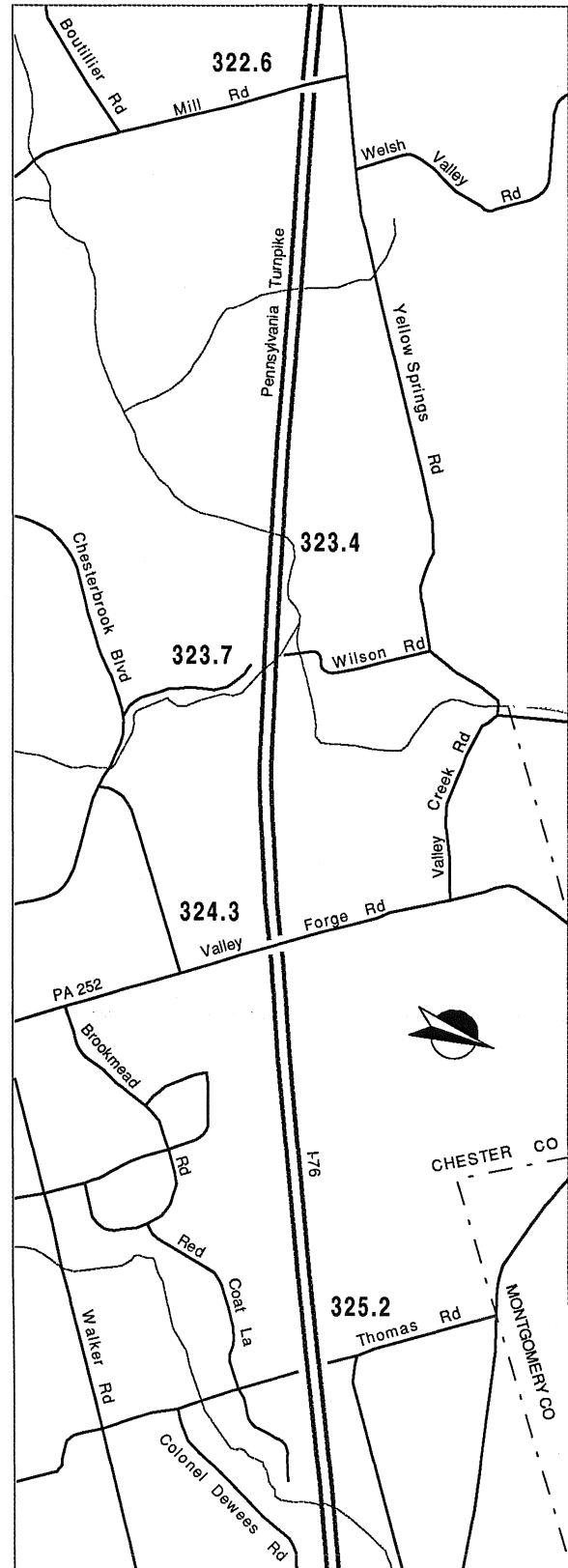
325.2 Bridge

Over Thomas Road

EB# 740

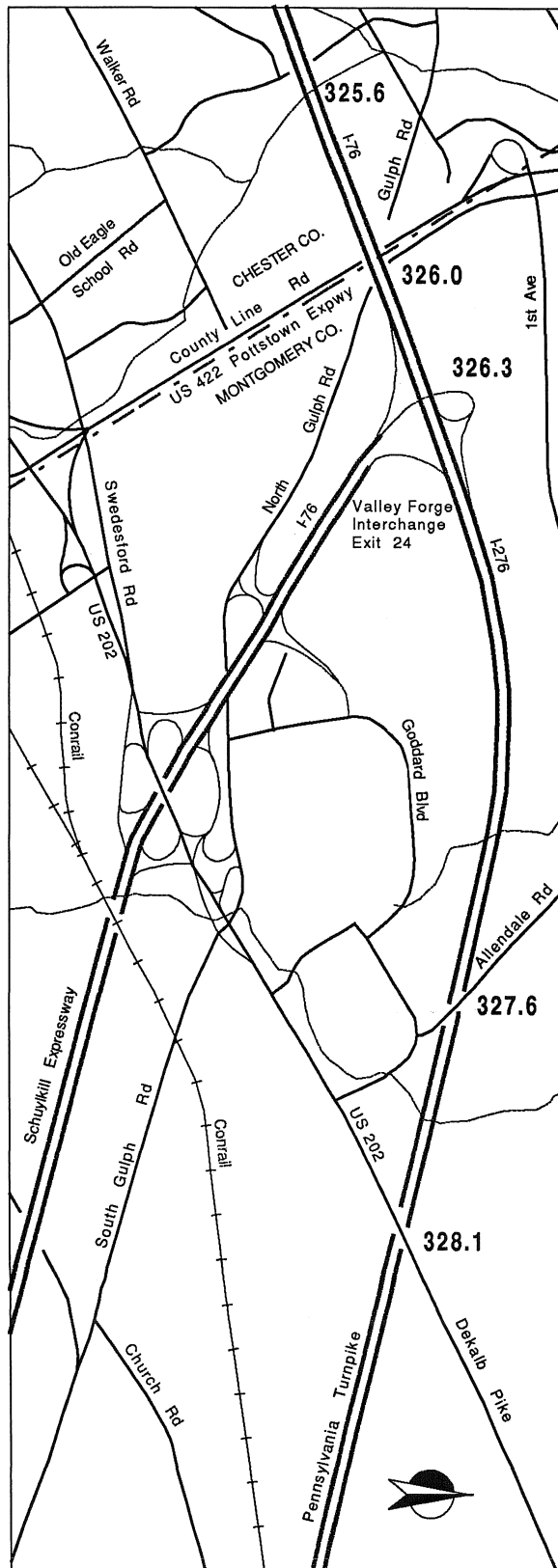
Span Length: 28'

Deck Width: 69.2'





THE PENNSYLVANIA TURNPIKE



Map 9

Design Features

Scale: 1" = 2000'

Note: Two EBD and two WBD 12' travel lanes. Shoulder width 10' and variable. Combination concrete barrier and guiderail barrier in median.

325.6 Bridge

Over Glenhardie Road
EB# 742
Span Length: 30'
Deck Width: 69.3'

326.0 Bridge

Over North Gulph Road SR 3039 LR 201;
Guthrie Road; and US 422
SR 0422 LR 1046
EB# 744
Span Length: 302'
Deck Width: 70'

326.3 Bridge

Over Valley Forge Interchange
DB# 101
Span Length: 120'
Deck Width: 88'

327.6 Overpass

Allendale Road
DB# 103
Structure Length: 188'
Deck Width: 84'

328.1 Overpass

Dekalb Pike US 202 SR 0202 LR 143
DB# 106
Structure Length: 250'
Turnpike Width: 84'





THE PENNSYLVANIA TURNPIKE

Map 10

Design Features

Scale: 1" = 2000'

Note: Two EBD and two WBD 12' travel lanes. Shoulder width 10' and variable. Median width 10'. Combination concrete barrier and guiderail barrier in median.

329.0 Bridge

Over Henderson Road
SR 3029 LR 46145
DB# 107
Span Length: 113'
Deck Width: 64'

329.2 Bridge

Over PECO Railroad ROW
(not shown)
DB# 108
Span Length: 207'
Deck Width: 64'

329.4 Bridge

Over SEPTA Route 100
DB# 109
Span Length: 285'
Deck Width: 62'

329.8 Bridge

Over Crooked Lane
SR 3027 LR 46137
DB# 110
Span Length: 122'
Deck Width: 63'

330.3 Overpass

Flint Hill Road
SR 3023 LR 46178
DB# 112
Structure Length: 93'
Turnpike Width: 84'

330.4 Bridge

Over Schuylkill River; Conrail;
SEPTA; River Road
PA 23 SR 0023; and
Colbert Street
LR 225 SR 0023
DB# 113
Span Length: 1228'
Deck Width: 54.7'

331.5 Bridge

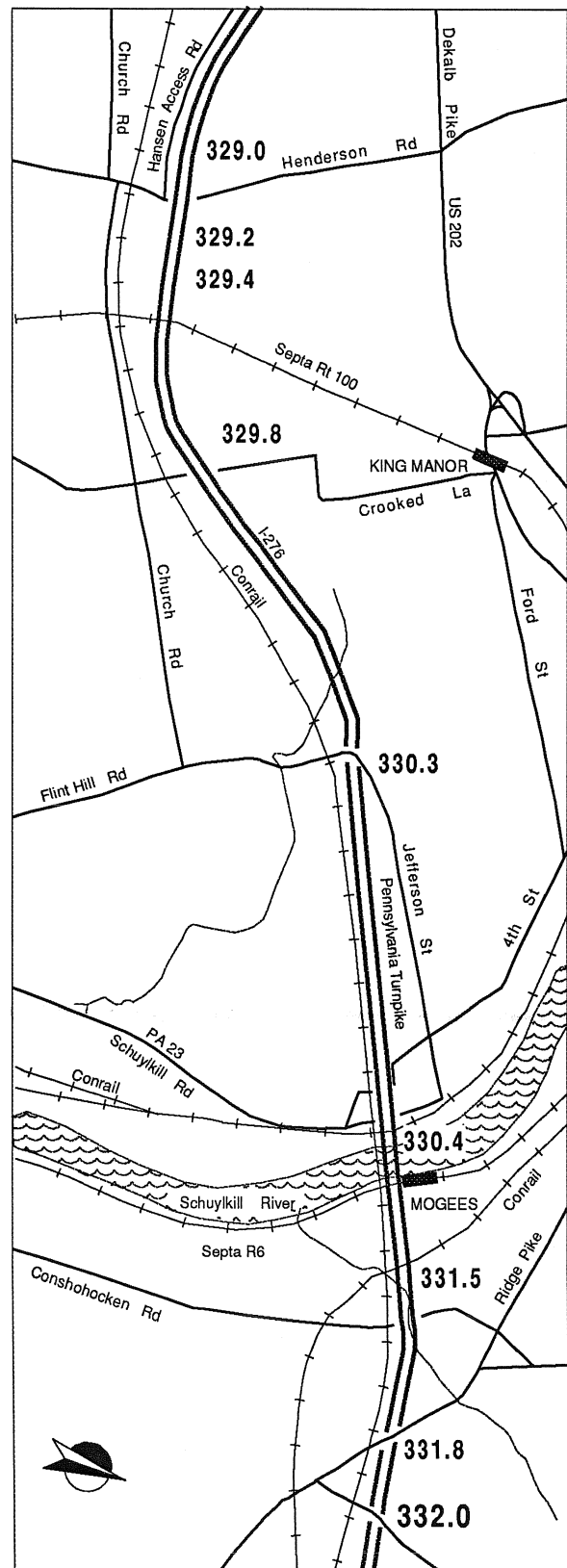
Over Conrail; Diamond Run;
and Conshohocken Road
SR 3013 LR 46107
DB# 114
Span Length: 631'
Deck Width: 56'

331.8 Overpass

Ridge Pike
DB# 116
Structure Length: 204'
Turnpike Width: 84'

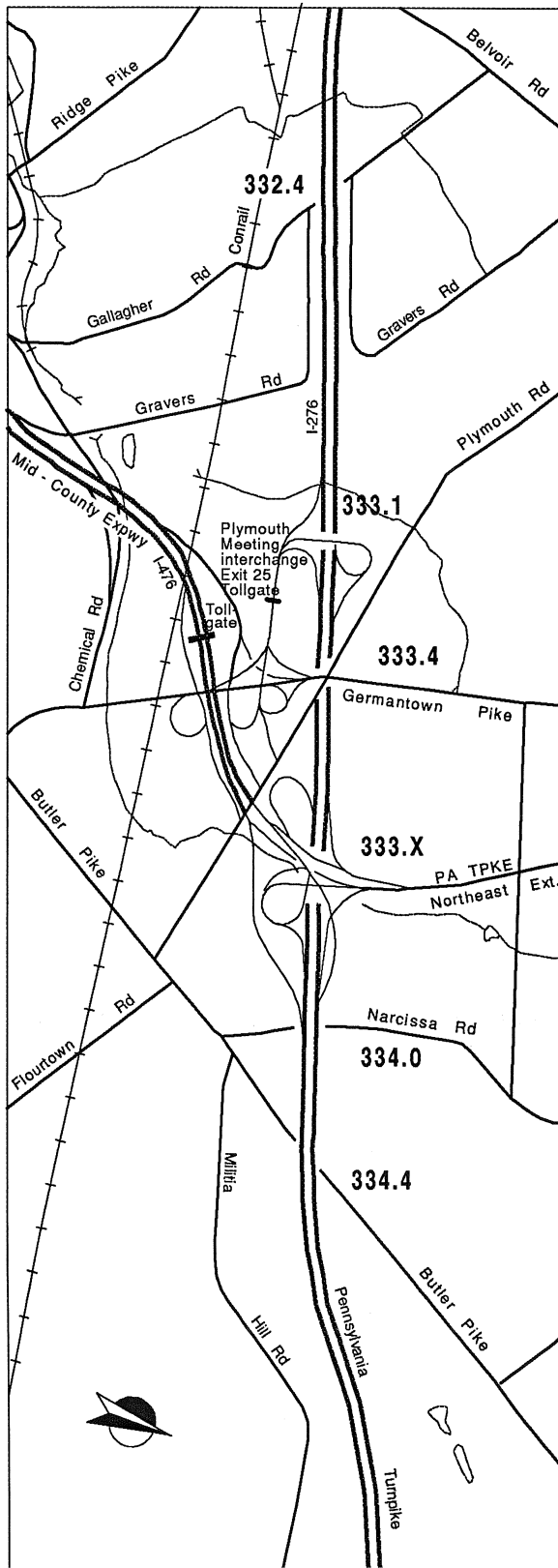
332.0 Overpass

Belvoir Road
SR 3017 LR 40104
DB# 117
Structure Length: 192'
Turnpike Width: 84'





THE PENNSYLVANIA TURNPIKE



Map 11

Design Features

Scale: 1" = 2000'

Note: Two EBD and two WBD 12' travel lanes. East of Norristown interchange three EBD and three WBD 12' travel lanes. Shoulder width 10' and variable. Median width 10'. Combination concrete barrier and guiderail barrier in median. East of Norristown interchange, noise barriers and retaining walls are common.

332.4 Bridge

Over Gallagher Road
DB# 119
Span Length: 38'
Deck Width: 70'

333.1 Overpass

Norristown Interchange
DB# 122
Structure Length: 167'
Turnpike Width: 96'

333.4 Overpass

Plymouth Road
SR 3007 LR 46087
DB# 123
Structure Length: 105'
Turnpike Width: n/a

333.4 Overpass

Germantown Pike
SR 3053 LR 145
DB# 124
Structure Length: n/a
Turnpike Width: n/a

333.X Overpass

EBD Main Line Ramp to NBD
NE Extension (PA 9)
DB# N/A
Structure Length: N/A

333.X Overpass

NBD I-476 ramp to NBD
NE Extension (PA 9)
DB# N/A
Structure Length: N/A

333.X Overpass

WBD Main Line Ramp
to SBD I-476
DB# N/A
Structure Length: N/A

334.0 Bridge

Over Narcissa Road
DB# 127, 127W
Span Length: 31'
Deck Width: 126.2'

334.4 Bridge

Over Butler Pike
DB# 128, 128W
Span Length: 72'
Deck Width: 163.1'

Note: Mile Markers will be posted when ramp construction is complete.





THE PENNSYLVANIA TURNPIKE

Map 12

Design Features

Scale: 1" = 2000'

Note: Three EBD and three WBD 12' travel lanes. Shoulder width 10' and variable. Median width 10'. Combination concrete barrier and guiderail barrier in median. Noise barriers and retaining walls are common.

335.6 Bridge

Over Stenton Avenue
SR 3003 LR 46086;
and Lorraine Run
DB# 129, 129W, 130
Span Length: 128'
Deck Width: 105.1'

336.5 Overpass

Joshua Road
SR 3014 LR 46102
DB# 132
Structure Length: 162'
Turnpike Width: 84'

336.9 Bridge

Over Skippack Pike PA 73
SR 0073 LR 197
DB# 134W
Span Length: 73'
Deck Width: 104.4'

337.1 Bridge

Over Wissahickon Creek
DB# 135W
Span Length: 248'
Deck Width: 102.8'

337.2 Bridge

Over Bethlehem Pike
SR 2018 LR 153
DB# 135A
Span Length: 185'
Deck Width: 103.9'

338.0 Bridge

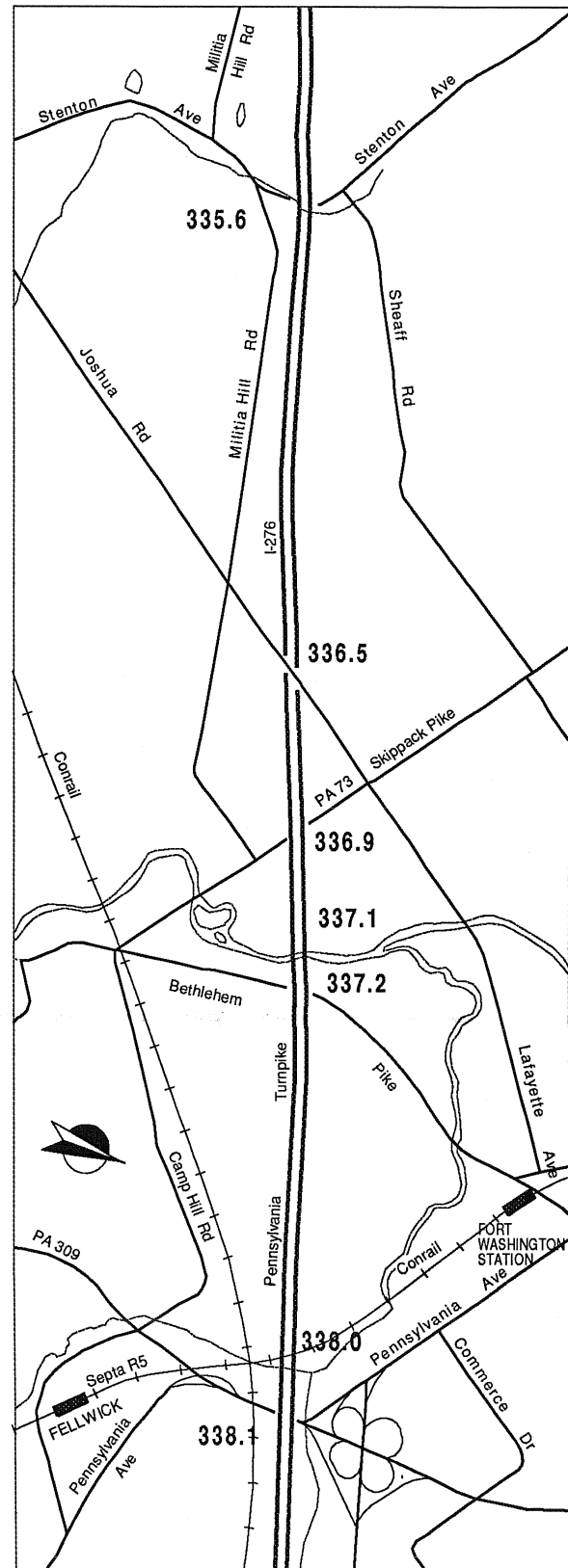
Over SEPTA R-5
DB# 136
Span Length: 171'
Deck Width: 103.8'

338.0 Bridge

Over Sandy Run
DB# 137
Span Length: n/a
Deck Width: n/a

338.1 Bridge

Over PA 309
SR 0309 LR 782
DB# 138, 138W
Span Length: 160'
Deck Width: 103.2'





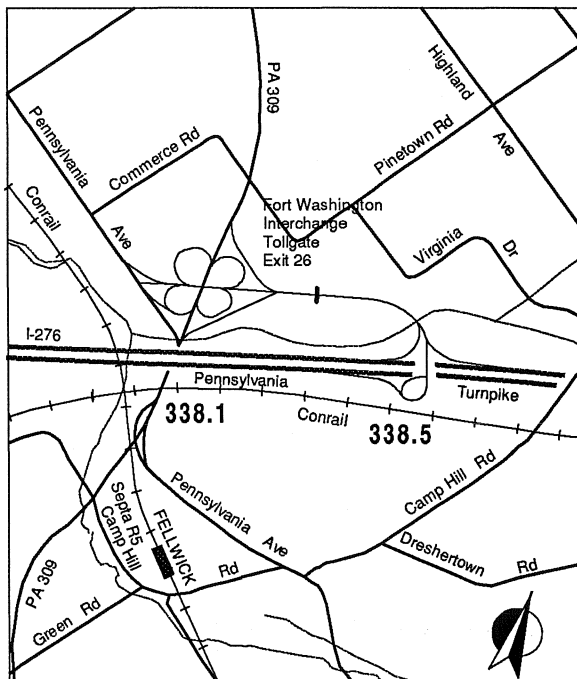
THE PENNSYLVANIA TURNPIKE

Map 12A

Design Features

Scale: 1" = 2000'

Note: Three EBD and three WBD 12' travel lanes. Shoulder width 10' and variable. Median width 10'. Combination concrete barrier and guiderail barrier in median. Noise barriers and retaining walls are common.



338.1 Bridge

Over PA 309

SR 0309 LR 782

DB# 138, 138W

Span Length: 160'

Deck Width: 103.2'

338.5 Overpass

Fort Washington Interchange

DB# 140A

Structure Length: 206'

Turnpike Width: 130'





THE PENNSYLVANIA TURNPIKE

Map 13

Design Features

Scale: 1" = 2000'

Note: Three EBD and three WBD 12' travel lanes. Shoulder width 10' and variable. Median width 10'. Combination concrete barrier and guiderail barrier in median. Noise barriers and retaining walls are common.

338.5 Overpass

Fort Washington Interchange

DB# 140A

Structure Length: 206'

Turnpike Width: 130'

338.9 Overpass

Camp Hill Road

DB# 141

Structure Length: 116'

Turnpike Width: 84'

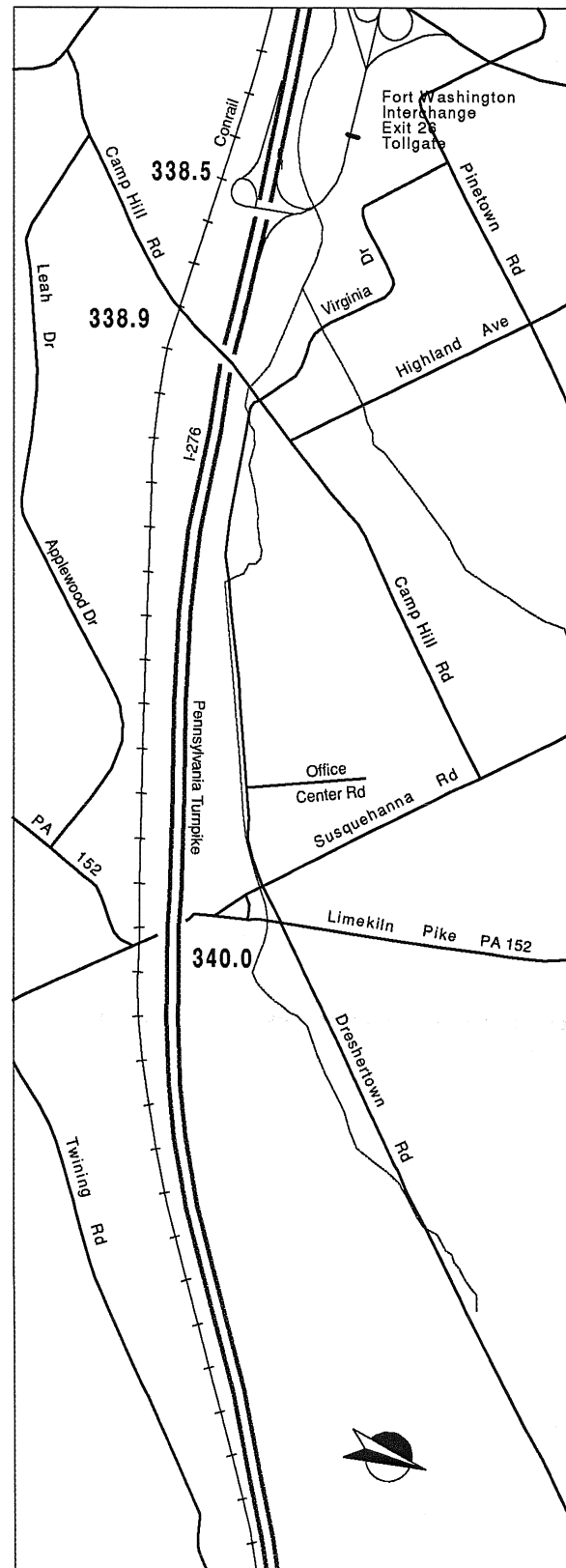
340.0 Bridge

Over Limekiln Pike PA 152 SR 0152 LR 373

DB# 146, 146W

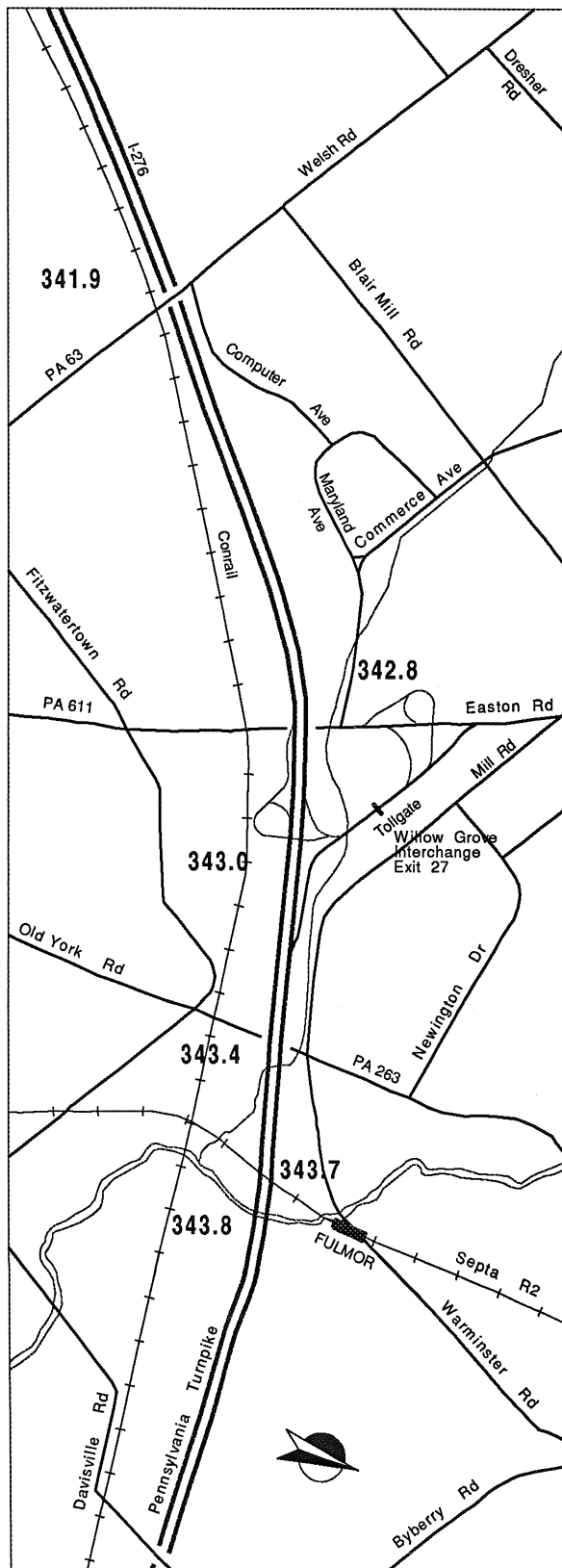
Span Length: 270'

Deck Width: 103'





THE PENNSYLVANIA TURNPIKE



Map 14

Design Features

Scale: 1" = 2000'

Note: Three EBD and three WBD 12' travel lanes. Shoulder width 10' and variable. Median width 10'. Combination concrete barrier and guiderail barrier in median. Noise barriers and retaining walls are common.

341.9 Overpass

Welsh Road PA 63 SR 0063 LR 198
DB# 154/S-6537
Structure Length: 160'
Turnpike Width: 84'

342.8 Bridge

Over Easton Road PA 611 SR 0611 LR 151
DB# 155
Span Length: 163'
Deck Width: 121.6'

343.0 Overpass

Willow Grove Interchange
DB# 157A
Structure Length: 212'
Turnpike Width: 110.75'

343.4 Bridge

Over Old York Road PA 263 SR 0263 LR 155
and Pennypack Creek
DB# 160, 160W
Span Length: 160'
Deck Width: 103'

343.7 Bridge

Over SEPTA R-2
DB# 162, 162W
Span Length: 115'
Deck Width: 103'

343.8 Bridge

Over Pennypack Creek
DB# 163, 163W
Span Length: 185'
Deck Width: 64'





THE PENNSYLVANIA TURNPIKE

Map 15

Design Features

Scale: 1" = 2000'

Note: Three EBD and three WBD 12' travel lanes. Shoulder width 10' and variable. Median width 10'. Combination concrete barrier and guiderail barrier in median. Noise barriers and retaining walls are common.

344.5 Overpass

Davisville Road
SR 2042 LR 46094
DB# 164
Structure Length: 166'
Turnpike Width: 84'

345.1 Overpass

Byberry Road
SR 2009 LR A-26
DB# 165
Structure Length: 161'
Turnpike Width: 84'

345.4 Bridge

Over Pioneer Road
DB# 167, 167W
Span Length: 31'
Deck Width: 103.4'

345.9 Bridge

Over SEPTA Newtown Line
DB# 170, 170W
Span Length: 226'
Deck Width: 103'

346.3 Overpass

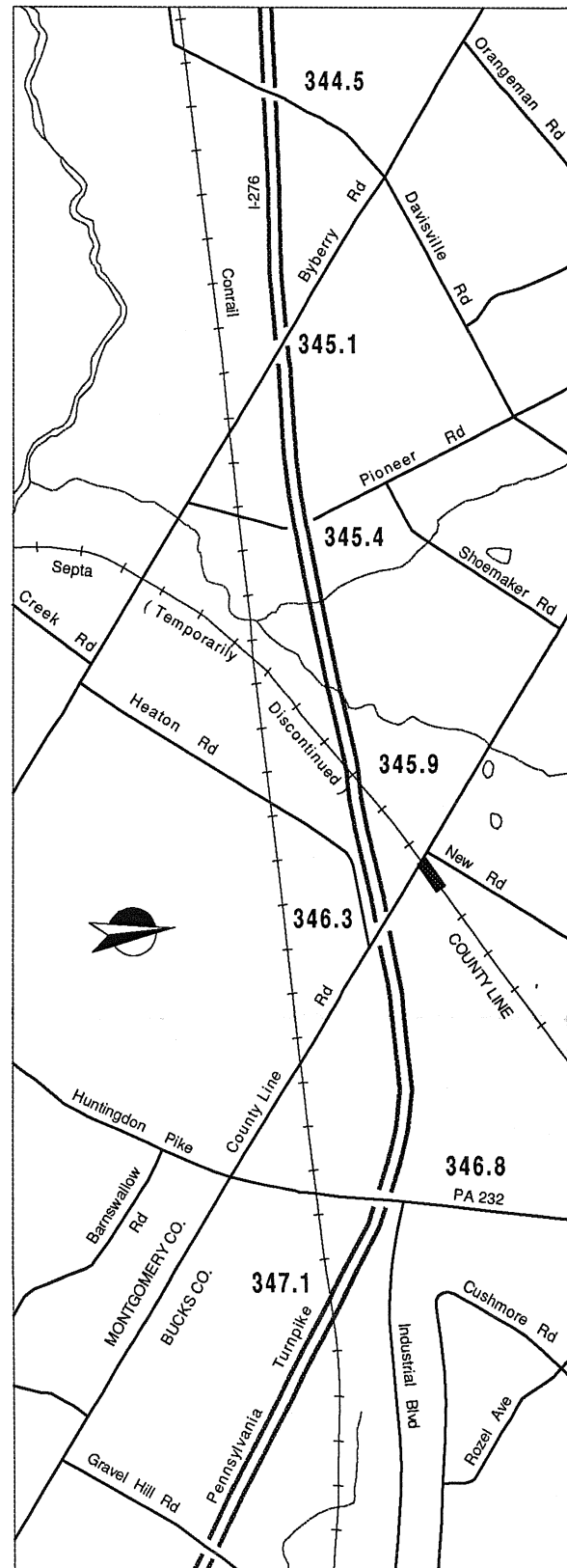
County Line Road
SR 2038 LR 09033
DB# 201
Structure Length: 132'
Turnpike Width: 84'

346.8 Overpass

Huntingdon Pike PA 232
SR 0232 LR 362
DB# 203
Structure Length: 150'
Turnpike Width: 84'

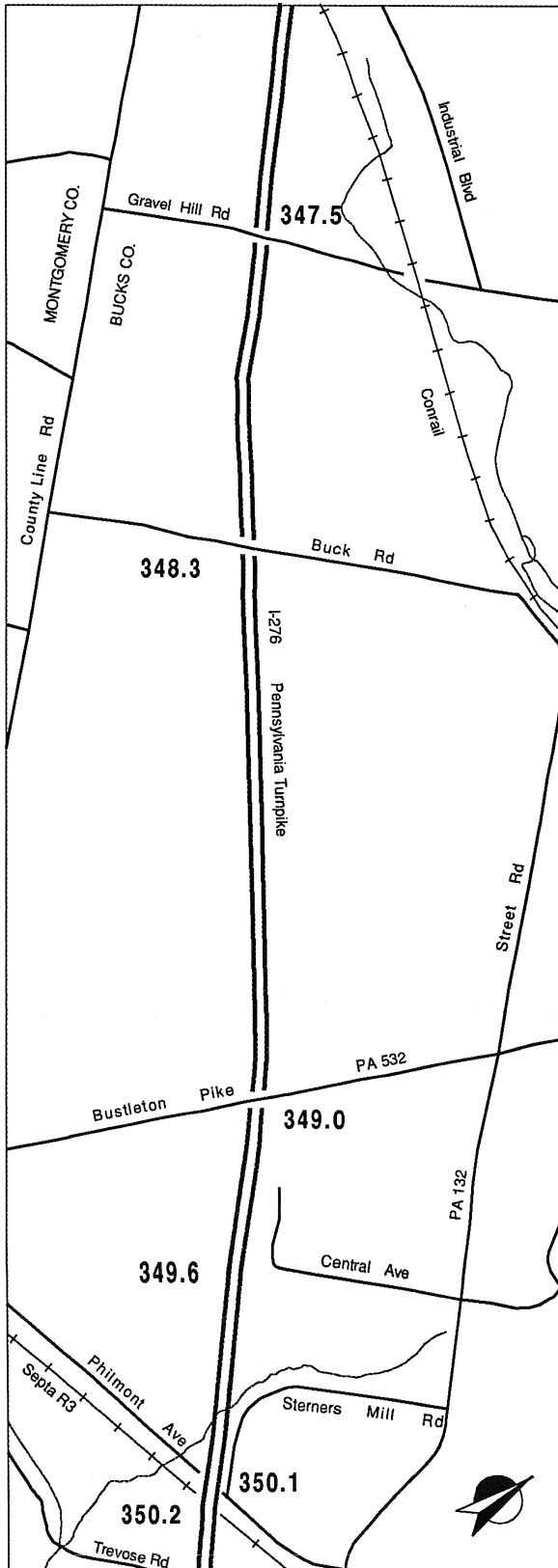
347.1 Bridge

Over Conrail
DB# 204, 204W
Span Length: 272'
Deck Width: 102.9'





THE PENNSYLVANIA TURNPIKE



Map 16

Design Features

Scale: 1" = 2000'

Note: Three EBD and three WBD 12' travel lanes. Shoulder width 10' and variable. Median width 10'. Combination concrete barrier and guiderail barrier in median. Noise barriers and retaining walls are common.

347.5 Overpass

Gravel Hill Road

DB# 207

Structure Length: 154'

Turnpike Width: 84'

348.3 Overpass

Buck Road

DB# 210

Structure Length: 155'

Turnpike Width: 84'

349.0 Overpass

Bustleton Pike PA 532 SR 0532 LR 326

DB# 213 S-9281

Structure Length: 95'

Turnpike Width: 84'

349.6 Overpass

Harding Avenue (not shown)

DB# 214

Structure Length: 150'

Turnpike Width: 84'

350.1 Bridge

Over Philmont Avenue SR 2040 LR 09182

DB# 216, 216W

Span Length: 138'

Deck Width: 103.2'

350.2 Bridge

Over SEPTA R-3

DB# 217, 217W

Span Length: 192'

Deck Width: 103'





THE PENNSYLVANIA TURNPIKE

Map 17

Design Features

Scale: 1" = 2000'

Note: Three EBD and three WBD 12' travel lanes. East of 351.1, two EBD and two WBD 12' travel lanes. Shoulder width 10' and variable. Median width 10'. Combination concrete barrier and guiderail barrier in Median. Noise barriers and retaining walls are common west of 351.1.

350.4 Bridge

Over Trevoise Road
SR 2043 LR 09007
DB# 219, 219W
Span Length: 37'
Deck Width: 103.3'

350.9 Bridge

Over Conrail
DB# 220, 220W
Span Length: 247'
Deck Width: 102.8'

351.1 Bridge

Over Street Road PA 132
SR 0132 LR 252
DB# 222 S-9154
Span Length: 258'
Deck Width: 104.5'

351.2 Bridge

over Old Lincoln Highway
SR 2037 LR 281
DB# 223
Span Length: 57'
Deck Width: 138.8'

351.1 Note:

EBD Mainline Width decreases 12' at this point.
Three EBD travel lanes reduce to two.

351.3 Note:

WBD Mainline width increases 12' at this point.
Two WBD travel lanes increase to three.

351.4 Overpass

Philadelphia Interchange
DB# 224
Structure Length: 151'
Turnpike Width: 98'

351.6 Overpass

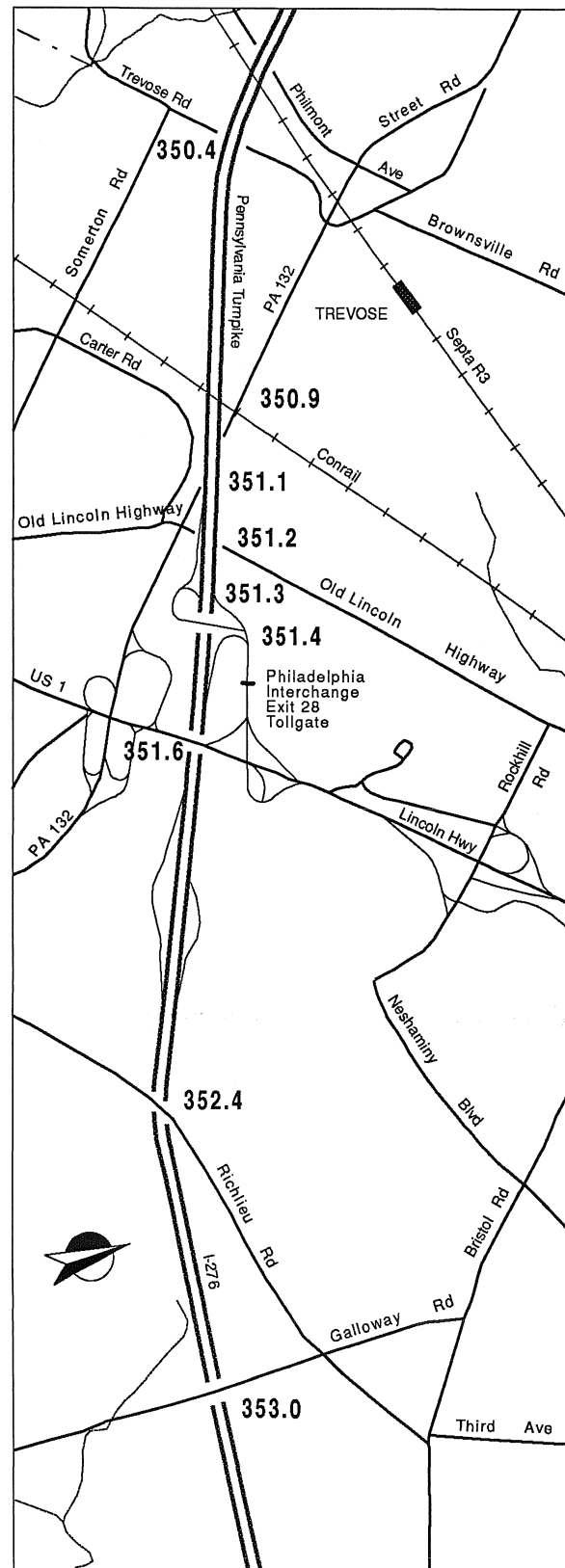
US 1
SR 0001 LR 281 Par.
DB# 225
Structure Length: 99'
Turnpike Width: 89.75'

352.4 Overpass

Richlieu Road
SR 2035 LR 09125
DB# 229
Structure Length: 183'
Turnpike Width: 84.77'

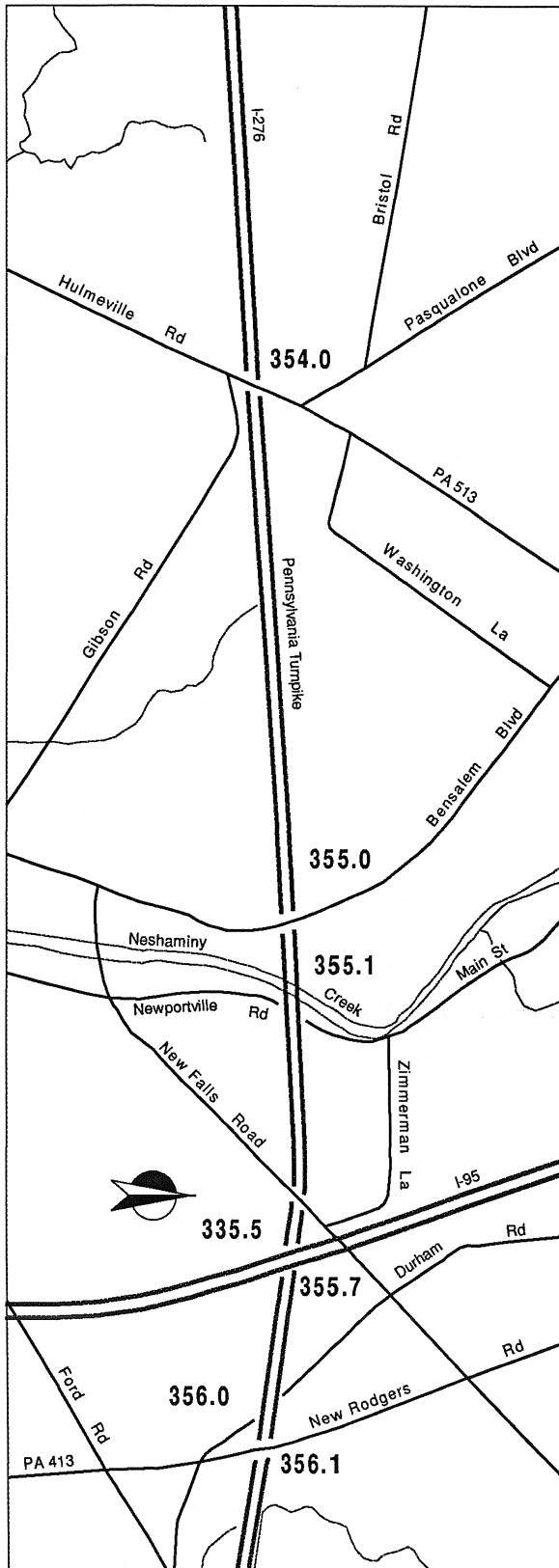
353.0 Overpass

Galloway Road
SR 2023 LR 09181
DB# 234
Structure Length: 153'
Turnpike Width: 84'





THE PENNSYLVANIA TURNPIKE



Map 18

Design Features

Scale: 1" = 2000'

Note: Two EBD and two WBD 12' travel lanes. Shoulder width 10' and variable. Median width 10'. Combination concrete barrier and guiderail barrier in median.

354.0 Overpass

Hulmeville Road PA 513
SR 513 LR 484
DB# 237
Structure Length: 159'
Turnpike Width: 84'

355.7 Overpass

I-95 SR 0095 LR 1000
S# 8665
Structure Length: n/a
Turnpike Width: n/a

355.0 Overpass

Bensalem Boulevard
SR 2015 LR 150sp.
DB# 239
Structure Length: 93'
Turnpike Width: 84'

356.0 Bridge

Over Durham Road
SR 2049 LR 09172
DB# 243
Span Length: 69'
Deck Width: 70'

355.1 Bridge

Over Neshaminy Creek
and Main Street
SR 2027 LR A276
DB# 241
Span Length: 508'
Deck Width: 56'

356.1 Overpass

New Rodgers Road PA 413
SR 0413 LR 09113
DB# 244
Structure Length: 53'
Turnpike Width: 67'

355.5 Overpass

New Falls Road
SR 2006 LR 09012
DB# 242
Structure Length: 130'
Turnpike Width: 84.67'





THE PENNSYLVANIA TURNPIKE

Map 19

Design Features

Scale: 1" = 2000'

Note: Two EBD and two WBD 12' travel lanes. Shoulder width 10' and variable. Median width 10'. Combination concrete barrier and guiderail barrier in median.

357.1 Overpass

Bristol Road SR 2029 LR 09017

DB# 247

Structure Length: 156'

Turnpike Width: 84'

357.3 Bridge

Over Black Ditch Creek

DB# 248

Span Length: 122'

Deck Width: 64'

357.5 Bridge

Over Delaware Valley Interchange

DB# 250

Span Length: 44'

Deck Width: 83.7'

357.9 Bridge

Over Green Lane

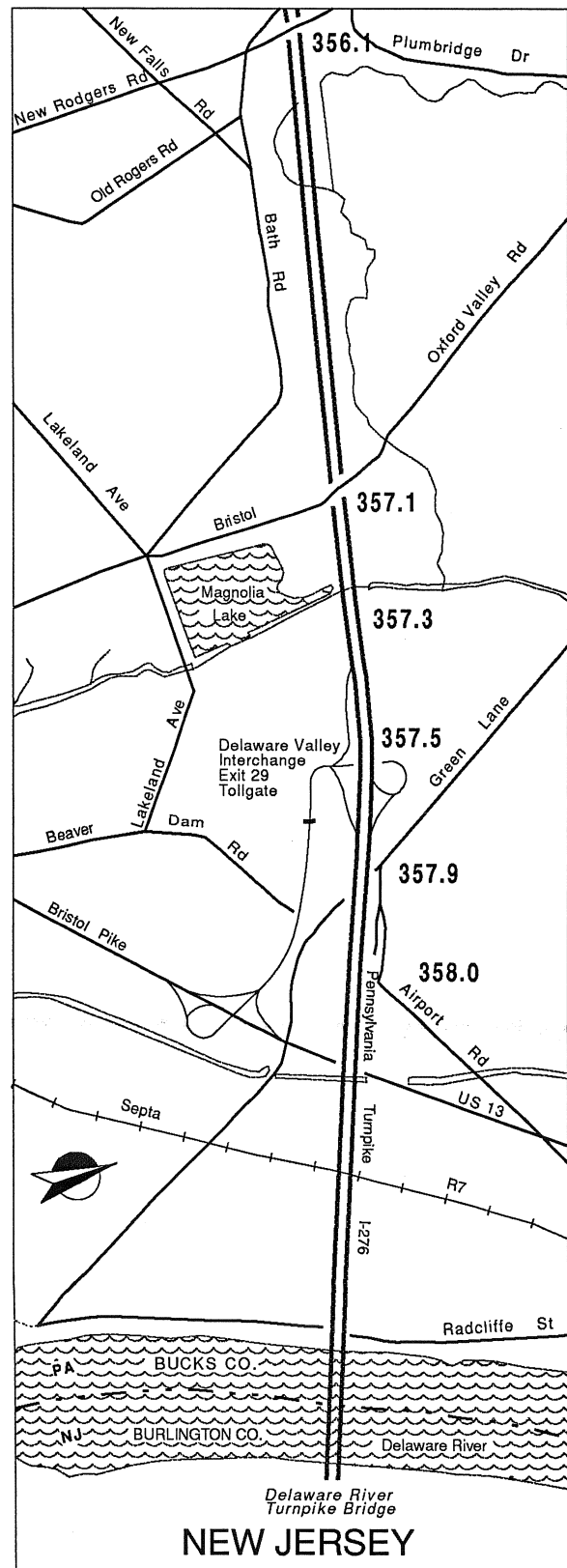
DB# 249

Span Length: 96'

Deck Width: 104.8'

358.0 Note:

Begin Toll Plaza for Delaware River
Turnpike Bridge



Print
on
green

ADJACENT LAND USE

The adjacent land use maps identify the land use on parcels immediately adjacent to The Pennsylvania Turnpike's right of way. The purpose of this inventory is to highlight land use considerations which may impact, positively or negatively, upon large-scale roadway modifications.

Nine different land use categories are used to designate adjacent land uses. These categories are general in nature so as to facilitate their use, but sufficiently specific to allow for a meaningful designation of land use. The nine categories are:

- Agricultural
- Cemetery
- Commercial/Industrial
- Institutional
- Parkland
- Residential
- Transportation
- Vacant (parcels over 10 acres identified as such)
- Water

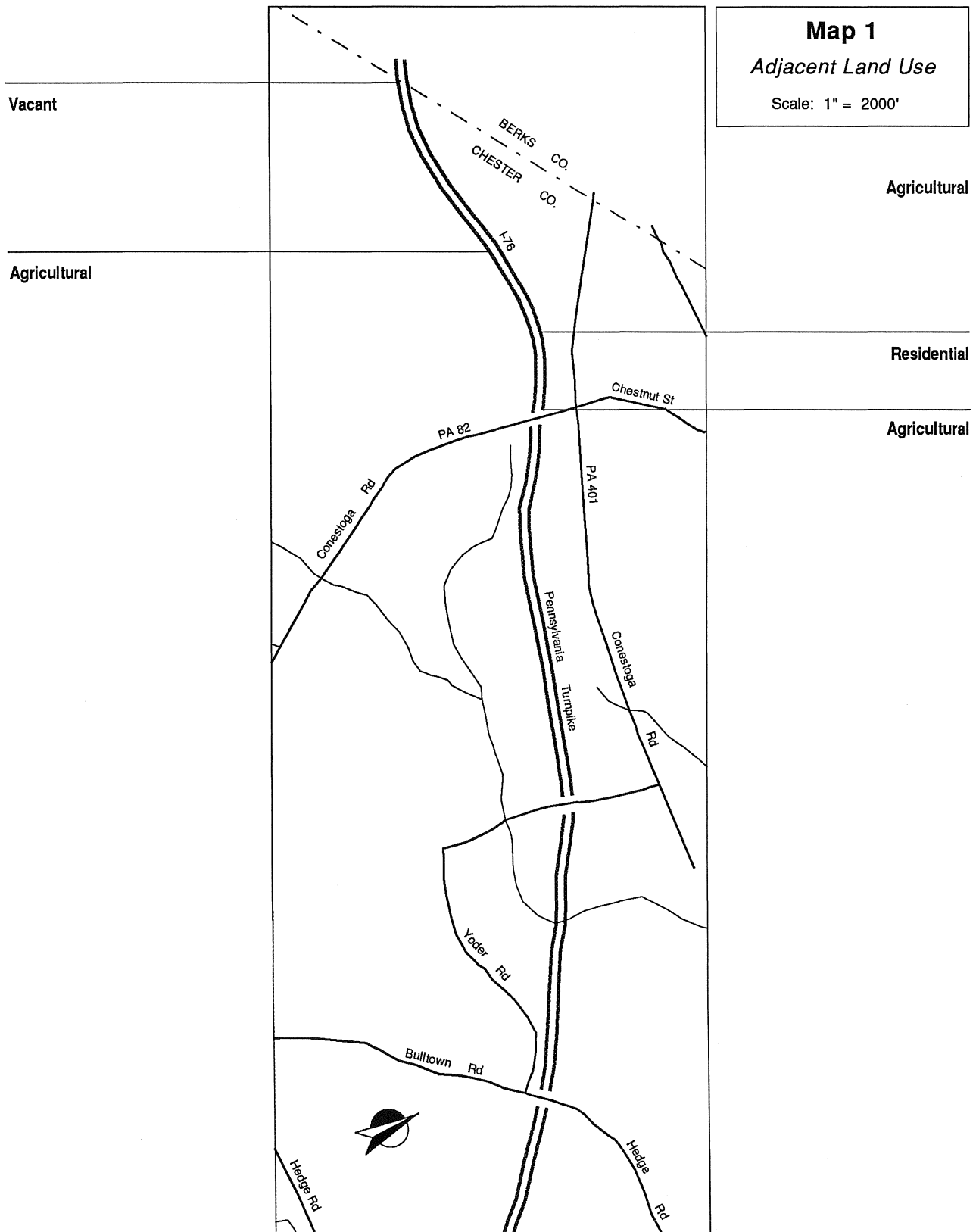
Adjacent land uses are identified on the map along both sides of the highway. The land use noted at the top of each map extends towards the bottom of the page until a different land use is cited under a horizontal line.

Special conditions and anecdotal information that may provide additional insight into the nature of adjacent parcels is provided where appropriate. The presence of special structures, including the proper names of identifiable places located adjacent to the roadway is also noted.

Information presented on this map was assembled from DVRPC in-house data, including aerial photographs, and field views.



THE PENNSYLVANIA TURNPIKE





THE PENNSYLVANIA TURNPIKE

Map 2
Adjacent Land Use
Scale: 1" = 2000'

Agricultural

Vacant

Residential
(Top Ridge)

Vacant

Residential
(Wallace Woods)

Vacant



Agricultural

Commercial/Industrial

Agricultural

Vacant

Agricultural

Commercial/Industrial

Vacant





THE PENNSYLVANIA TURNPIKE

Vacant

Map 3
Adjacent Land Use

Scale: 1" = 2000'

Vacant

Water
(Marsh Creek)

Water
(Marsh Creek)

Residential

Parkland
(Marsh Creek State Park)

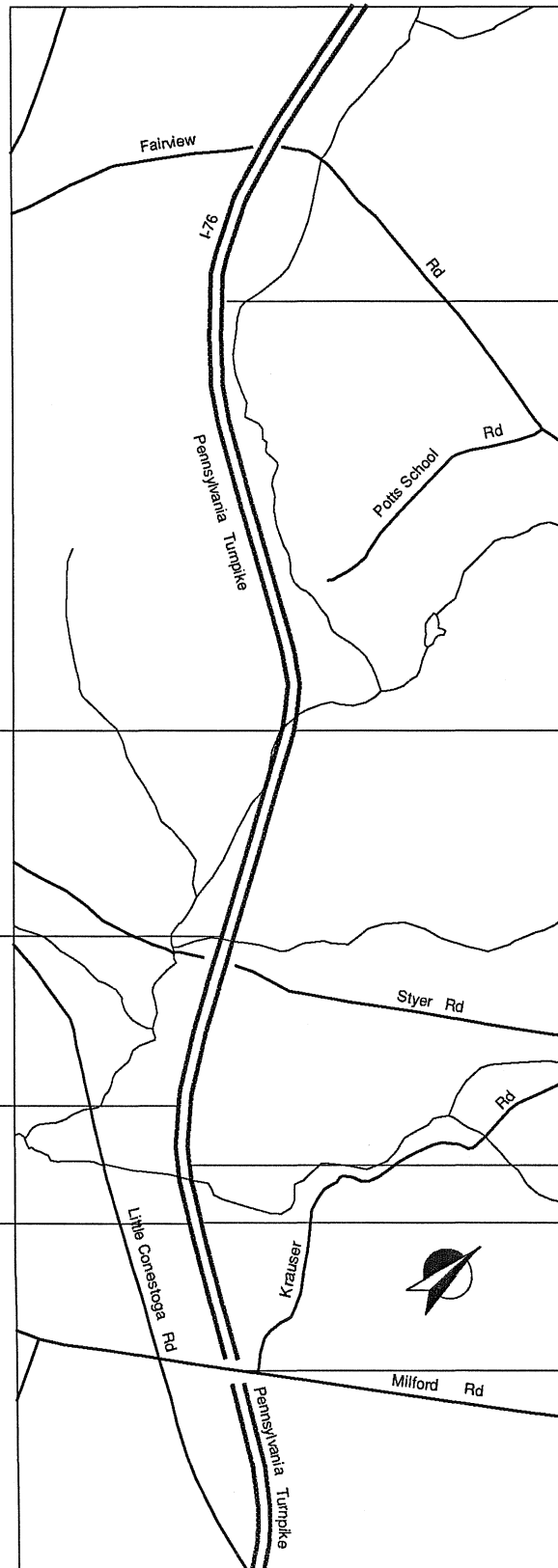
Residential
(Cheryl Ridge)

Vacant

Agricultural

Residential

Agricultural





THE PENNSYLVANIA TURNPIKE

Map 4
Adjacent Land Use
Scale: 1" = 2000'

Agricultural

Parkland
(Upper Uwchlan Township Park)

Vacant

Agricultural

Commercial/Industrial
(Eagleview Corporate Park)

Transportation
(Downingtown Exit 23;
Route 100)

Agricultural

Agricultural

Residential

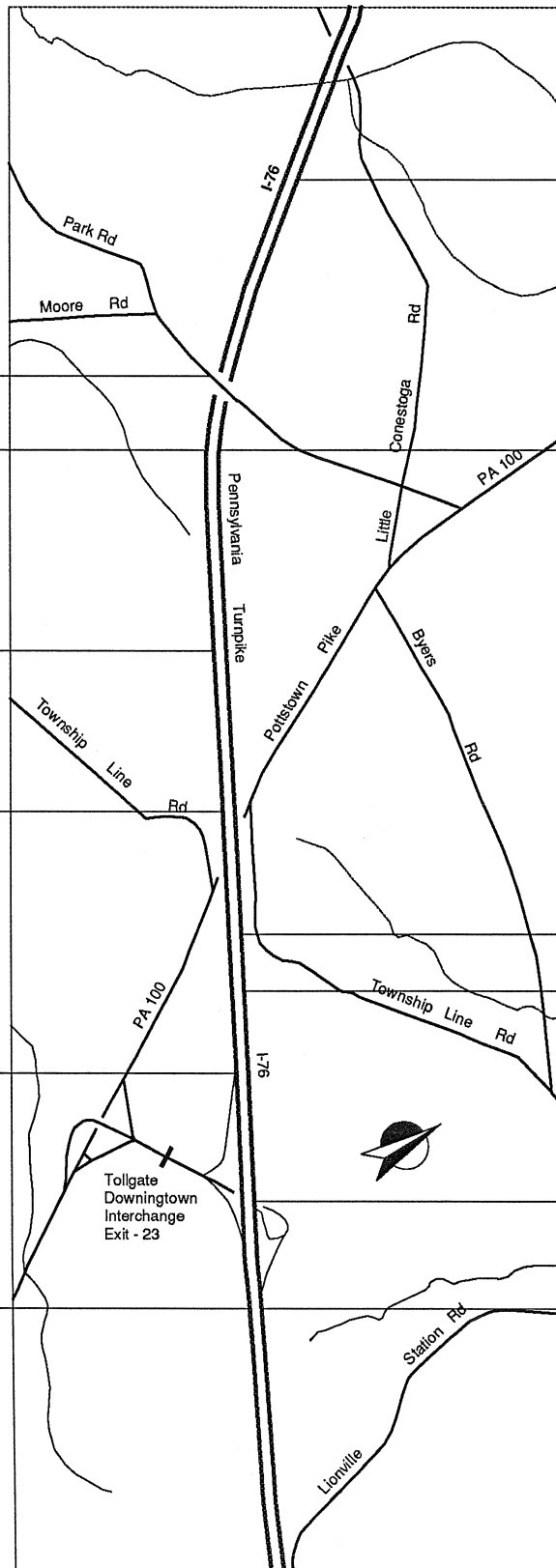
Commercial/Industrial

Residential

Agricultural

Vacant

Agricultural





THE PENNSYLVANIA TURNPIKE

Commercial/Industrial
(Stoneridge Business Center;
Marsh Creek Corporate Center;
Pickering Creek Industrial Park)

Map 5

Adjacent Land Use

Scale: 1" = 2000'

Residential
(Pickering Pointe; Liongate;
Holly Tree Estates)

Vacant

Agricultural

Transportation
(Davis Lane; agricultural
adjacent to roadway)

Residential

Vacant

Agricultural

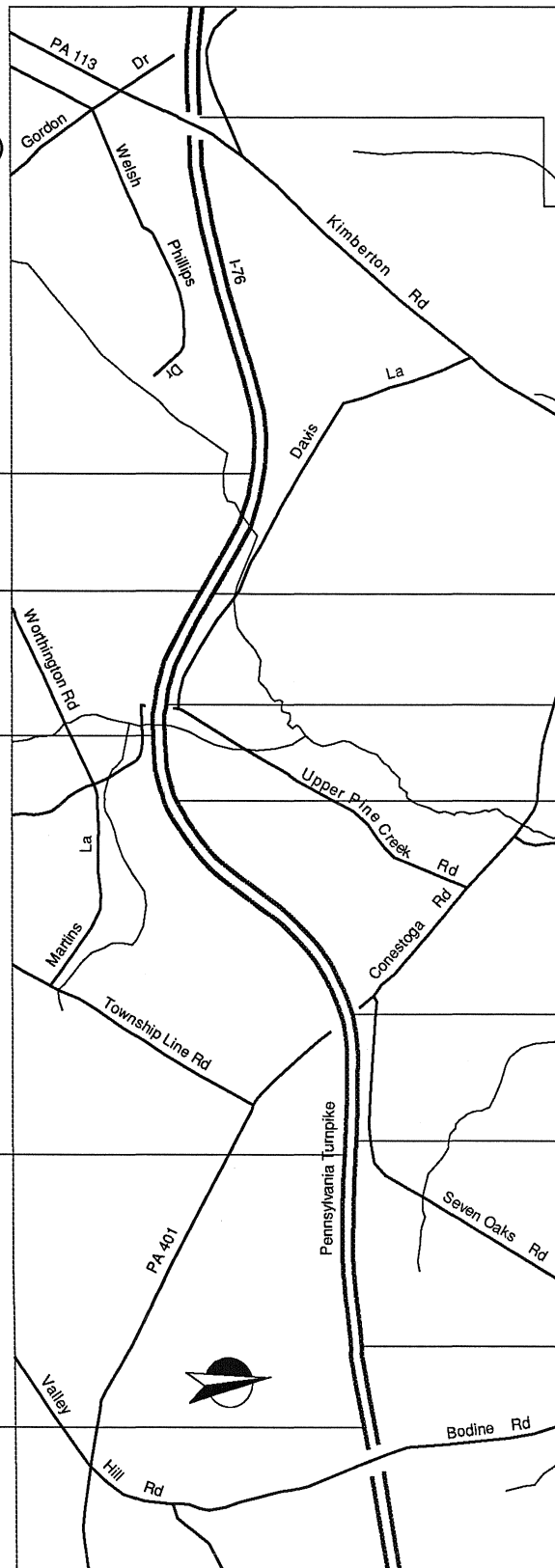
Agricultural

Transportation
(Seven Oaks Road;
commercial/industrial
adjacent to roadway)

Agricultural

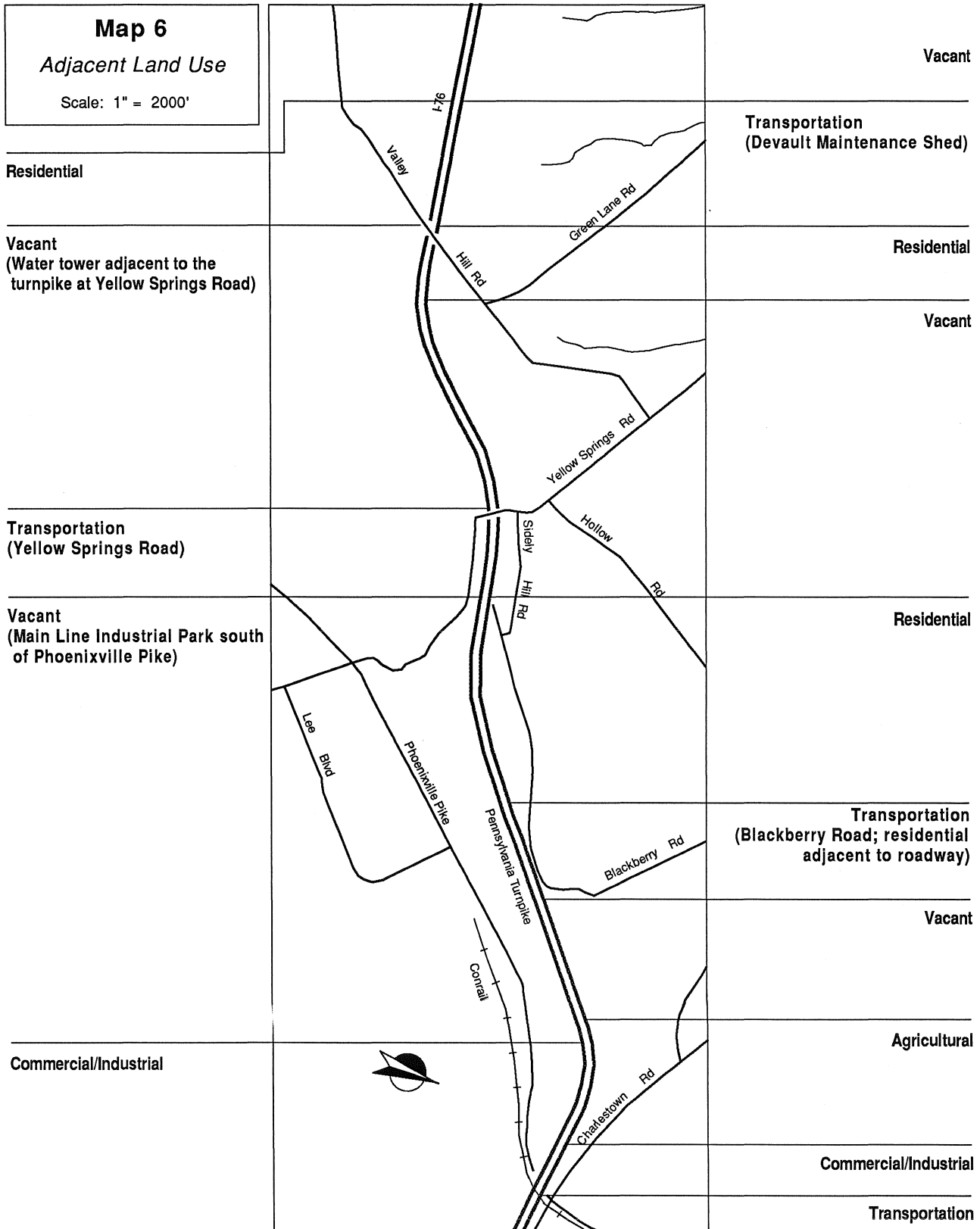
Residential

Vacant



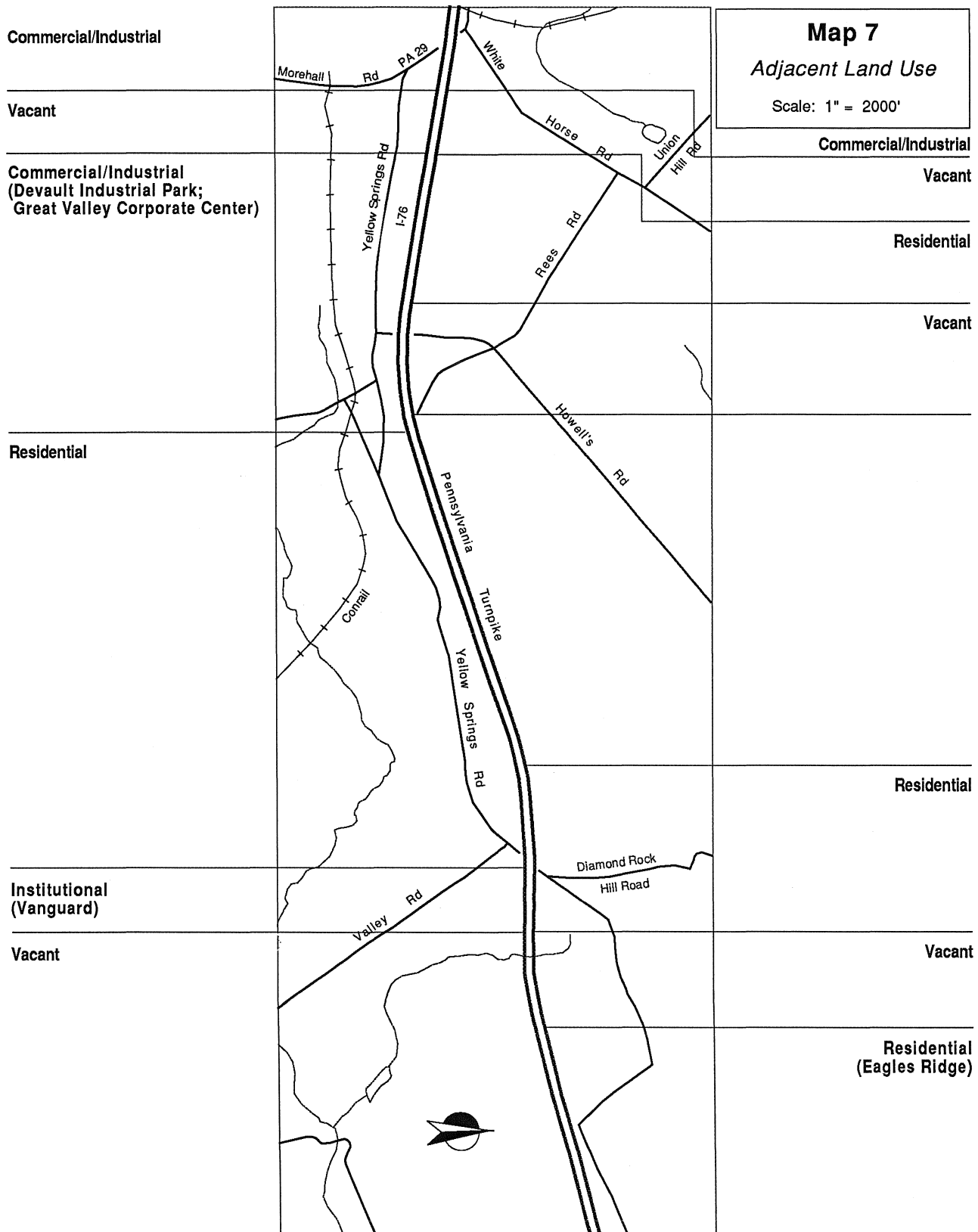


THE PENNSYLVANIA TURNPIKE





THE PENNSYLVANIA TURNPIKE





THE PENNSYLVANIA TURNPIKE

Map 8

Adjacent Land Use

Scale: 1" = 2000'

Residential
(Chesterbrook)

Residential

Vacant

Parkland
(Valley Forge National
Historic Park)

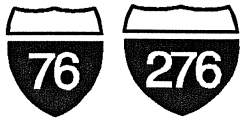
Commercial/Industrial
(Turnpike Rest Area)

Vacant
(residential south of Red Coat
Lane; Brookmeade Farms; Glen
Hardie)

Residential

Residential





THE PENNSYLVANIA TURNPIKE

Residential

Commercial/Industrial
(Golf course)

Residential
(Glen Hardie Apartments)

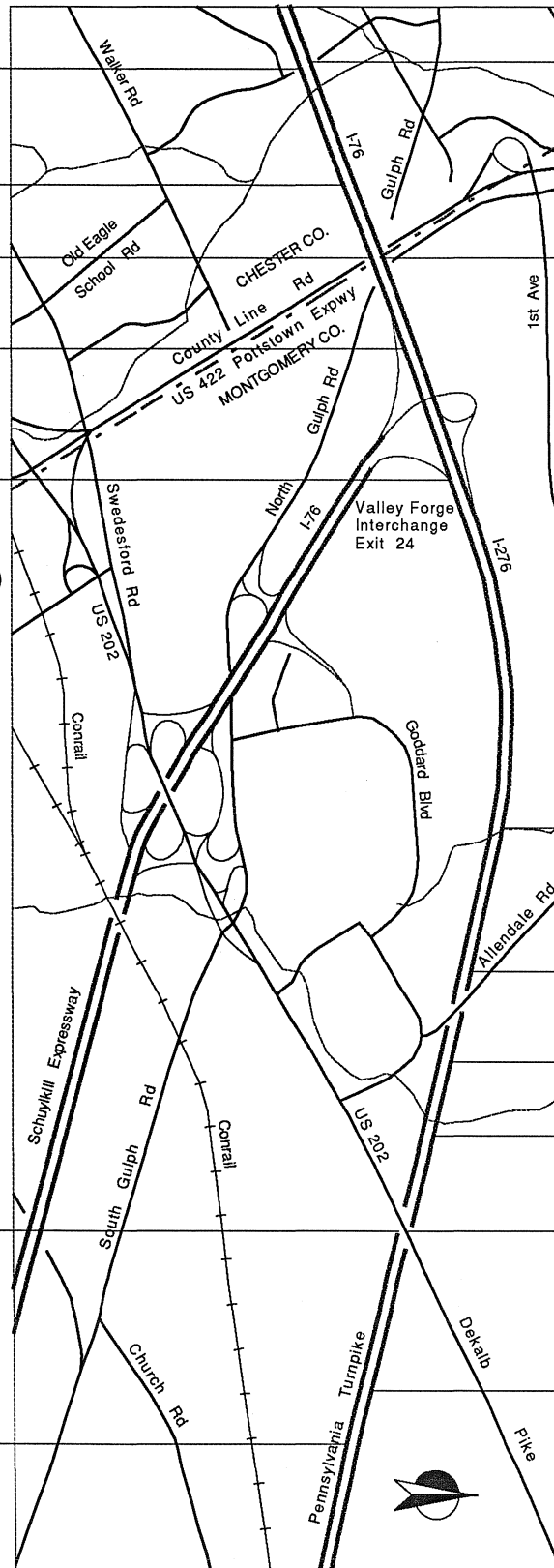
Commercial/Industrial

Transportation
(Schuylkill Expressway;
North Gulph Road)

Commercial/Industrial
(Including General Electric
Space Center; King of Prussia
Plaza; Court at King of Prussia)

Residential
(Valley Forge Homes)

Commercial/Industrial
(Transmission lines)



Map 9

Adjacent Land Use

Scale: 1" = 2000'

Residential

Commercial/Industrial
(Including Valley Forge Plaza
and Convention Center;
Freedom Business Center;
King of Prussia Industrial
Park)

Institutional
(Mother of Divine Providence
Church and School)

Commercial/Industrial

Parkland

Residential

Commercial/Industrial
(Turnpike Rest Area)

Residential
(Valley View Apartments)





THE PENNSYLVANIA TURNPIKE

Map 10
Adjacent Land Use
Scale: 1" = 2000'

Commercial/Industrial

Residential
(Valley View Apartments)

Commercial/Industrial

Transportation
(Conrail; commercial/industrial
or vacant adjacent to railroad
tracks)

Residential

Water

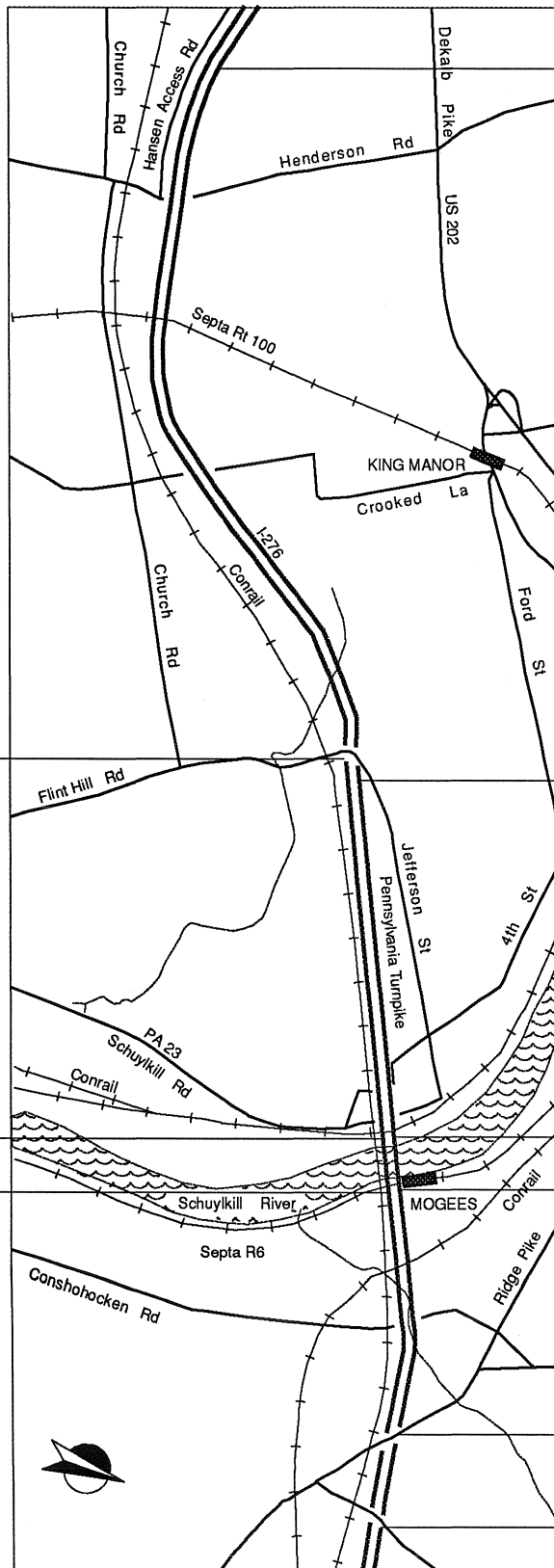
Water

Transportation
(Conrail; commercial/industrial
adjacent to railroad
tracks)

Commercial/Industrial

Residential

Commercial/Industrial





THE PENNSYLVANIA TURNPIKE

Transportation
(Conrail)

Vacant

Transportation
(Gravers Road; commercial/indus-
trial adjacent to Gravers Road)

Commercial/Industrial

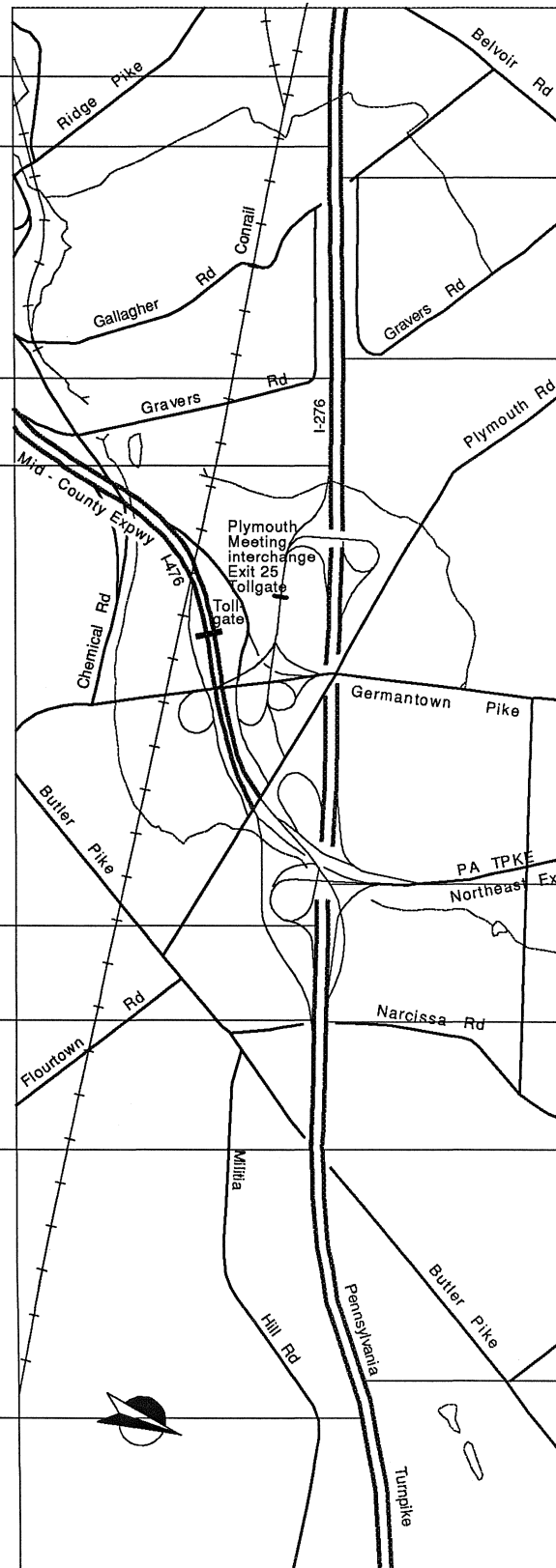
Transportation
(Blue Route interchange;
Northeast Extension interchange)

Vacant

Residential

Commercial/Industrial
(Sunnybrook Golf Course
south of Militia Hill Road)

Vacant



Map 11

Adjacent Land Use

Scale: 1" = 2000'

Transportation
(Gravers Road)

Commercial/Industrial
(Plymouth Woods Office Park;
Plymouth Meeting Shopping
Mall 1/2 mile north on German-
town Pike)

Vacant

Residential

Commercial/Industrial

Cemetery
(George Washington
Memorial Park)





THE PENNSYLVANIA TURNPIKE

Map 12

Adjacent Land Use

Scale: 1" = 2000'

Residential
(Fort Washington State Park
adjacent to Militia Hill Road)

Cemetery
(George Washington Memorial Park)

Vacant

Vacant

Residential

Parkland
(Fort Washington State Park)

Parkland

Residential

Residential

Parkland
(Fort Washington State Park)

Parkland

Commercial/Industrial

Residential
(Fort Hill Apartments)

Vacant

Vacant

Transportation
(Conrail; residential adjacent to
railroad tracks)

Commercial/Industrial

Vacant
(Fort Washington Inter-
change 26)





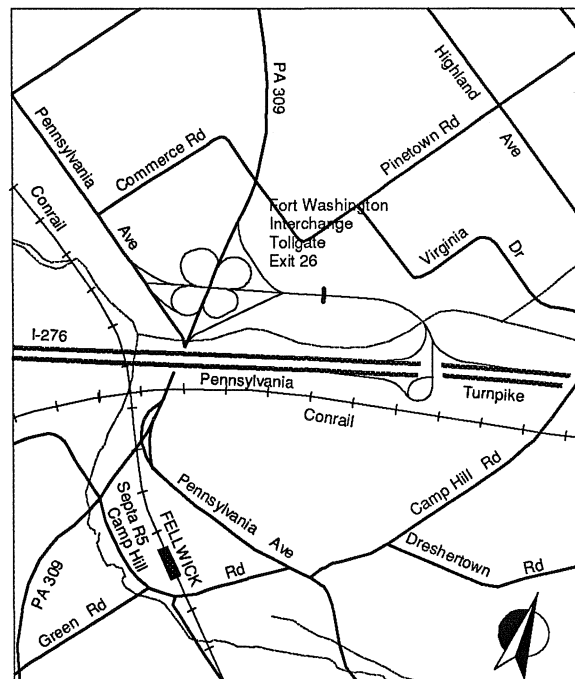
THE PENNSYLVANIA TURNPIKE

Map 12A

Adjacent Land Use

Scale: 1" = 2000'

(Note: See Maps 12 and 13)





THE PENNSYLVANIA TURNPIKE

Map 13

Adjacent Land Use

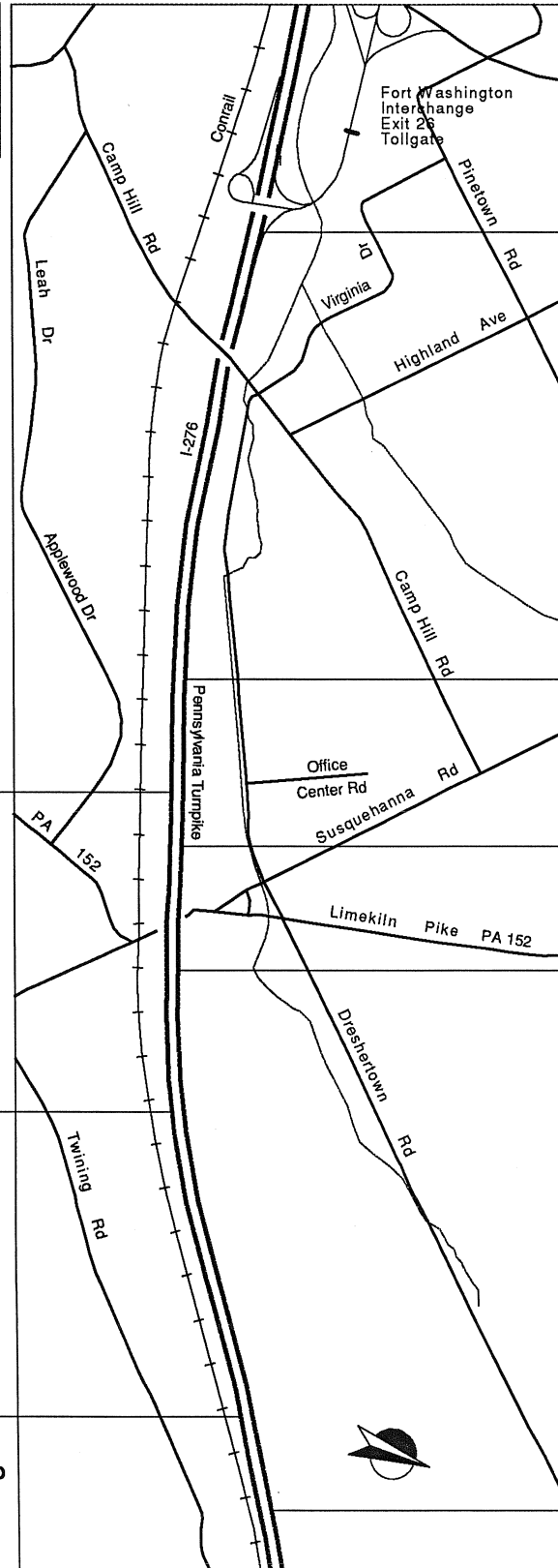
Scale: 1" = 2000'

Transportation
(Conrail; vacant to the railroad tracks; residential adjacent to railroad tracks)

Transportation
(Conrail; commercial/industrial adjacent to railroad tracks)

Transportation
(Conrail; Twining Valley Country Club adjacent to railroad tracks)

Transportation
(Conrail; residential adjacent to railroad tracks)



Vacant

Commercial/Industrial
(Fort Washington Office Center)

Vacant

Commercial/Industrial

Residential
(Dublin Hunt II)

Commercial/Industrial





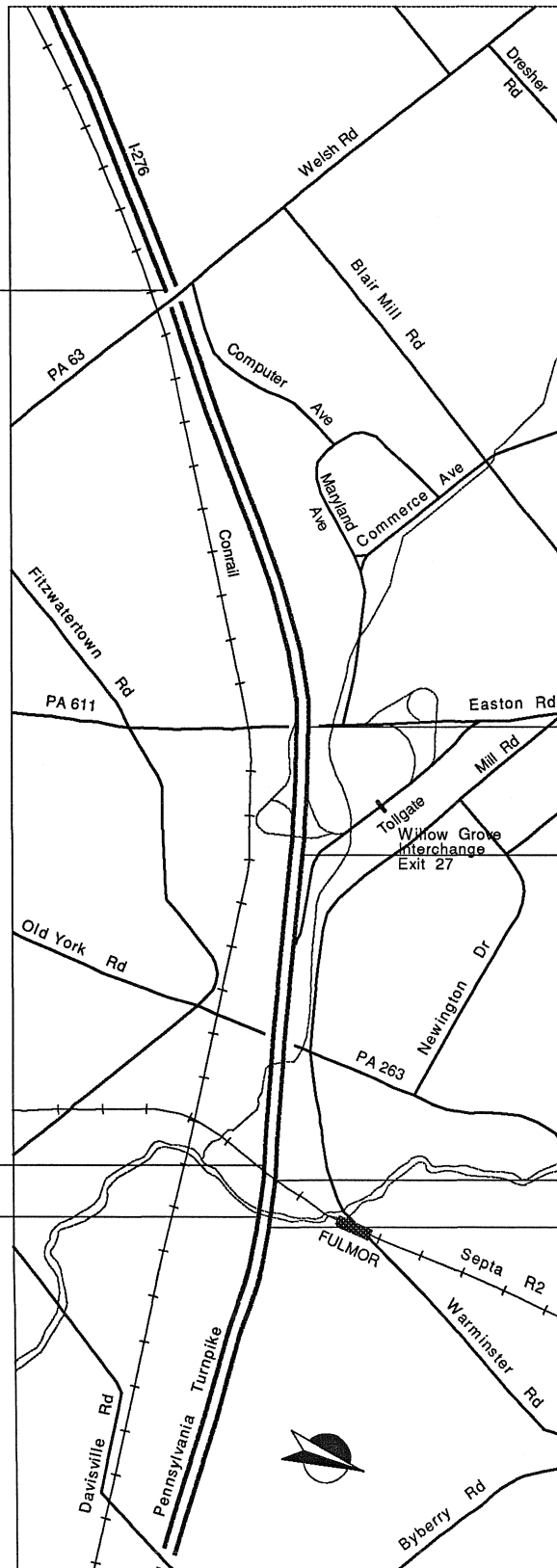
THE PENNSYLVANIA TURNPIKE

Transportation
(Conrail; residential adjacent to
railroad tracks)

Commercial/Industrial

Water
(Pennypack Creek)

Commercial/Industrial



Map 14

Adjacent Land Use

Scale: 1" = 2000'

Commercial/Industrial
(Including Willow Grove
Industrial and Office Center)

Transportation
(Willow Grove Interchange 27)

Commercial/Industrial

Water

Residential
(Water Tower adjacent to
turnpike)





THE PENNSYLVANIA TURNPIKE

Map 15

Adjacent Land Use

Scale: 1" = 2000'

Commercial/Industrial
(Including Mason Mill
Business Park)

Residential
(Including Shoemaker Village)

Residential

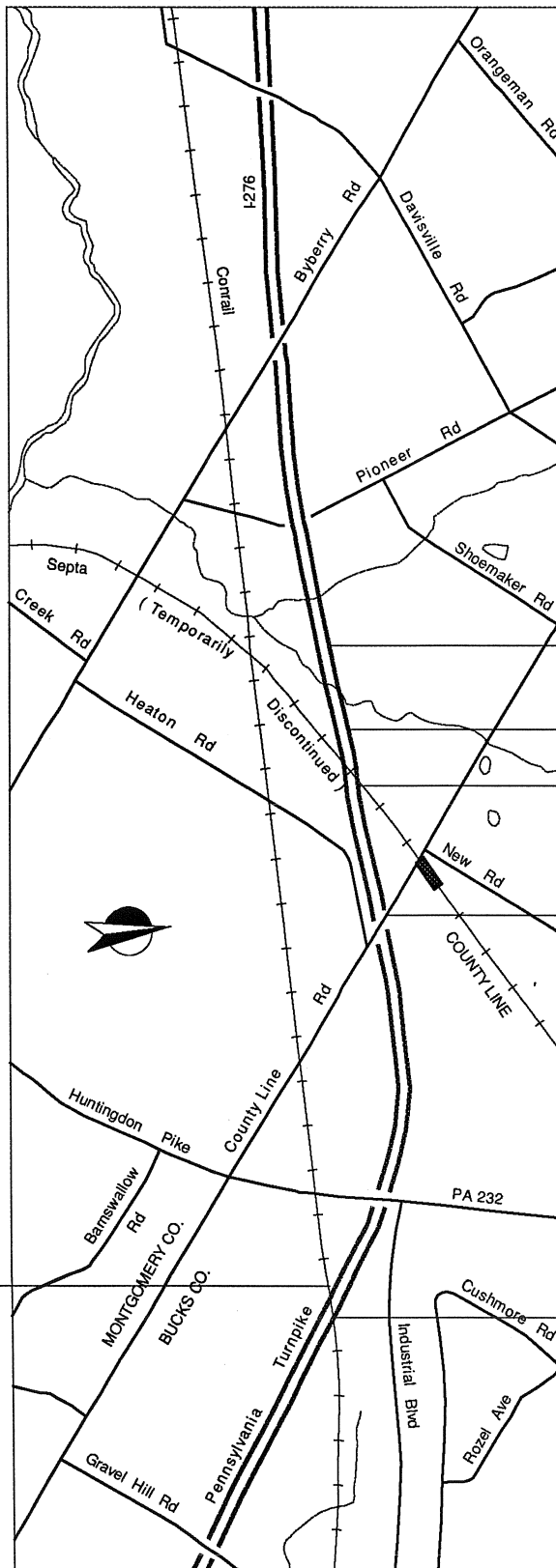
Vacant

Commercial/Industrial

Residential

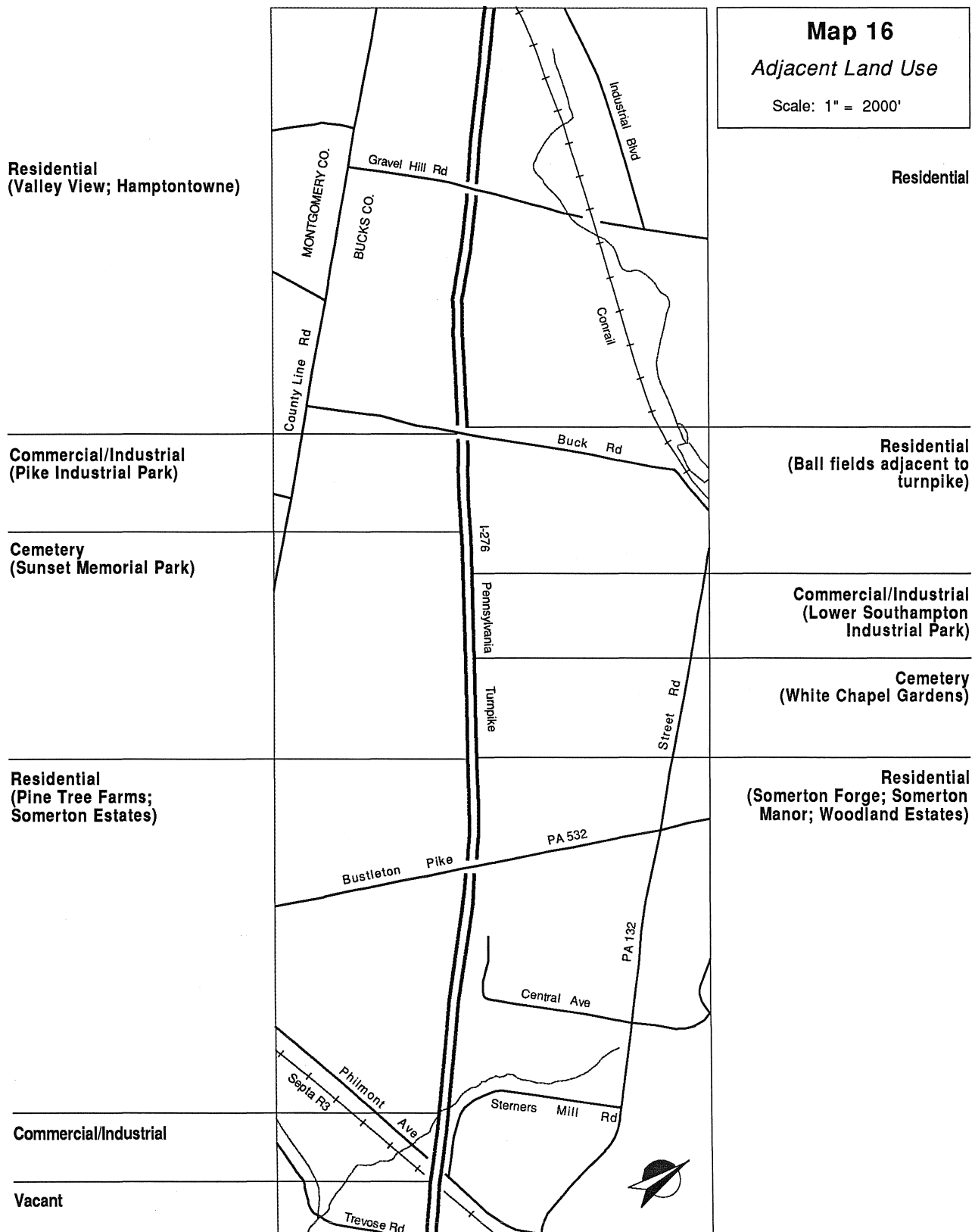
Commercial/Industrial
(County Line Industrial Park)

Residential





THE PENNSYLVANIA TURNPIKE





THE PENNSYLVANIA TURNPIKE

Map 17
Adjacent Land Use
Scale: 1" = 2000'

Vacant

Commercial/Industrial
(Metropolitan Industrial Park)

Residential
(Linconia Park)

Transportation
(Street Road/Route 1 interchange)

Commercial/Industrial
(Turnpike Rest Area)

Residential
(Hamilton Court; Country Commons)

Commercial/Industrial
(Philadelphia Park)

Commercial/Industrial
(Radio tower adjacent to turnpike)

Residential

Commercial/Industrial

Transportation

Commercial/Industrial

Institutional
(Eastern State Hospital;
Neshaminy Mall 1/2 mile
north on Route 1)

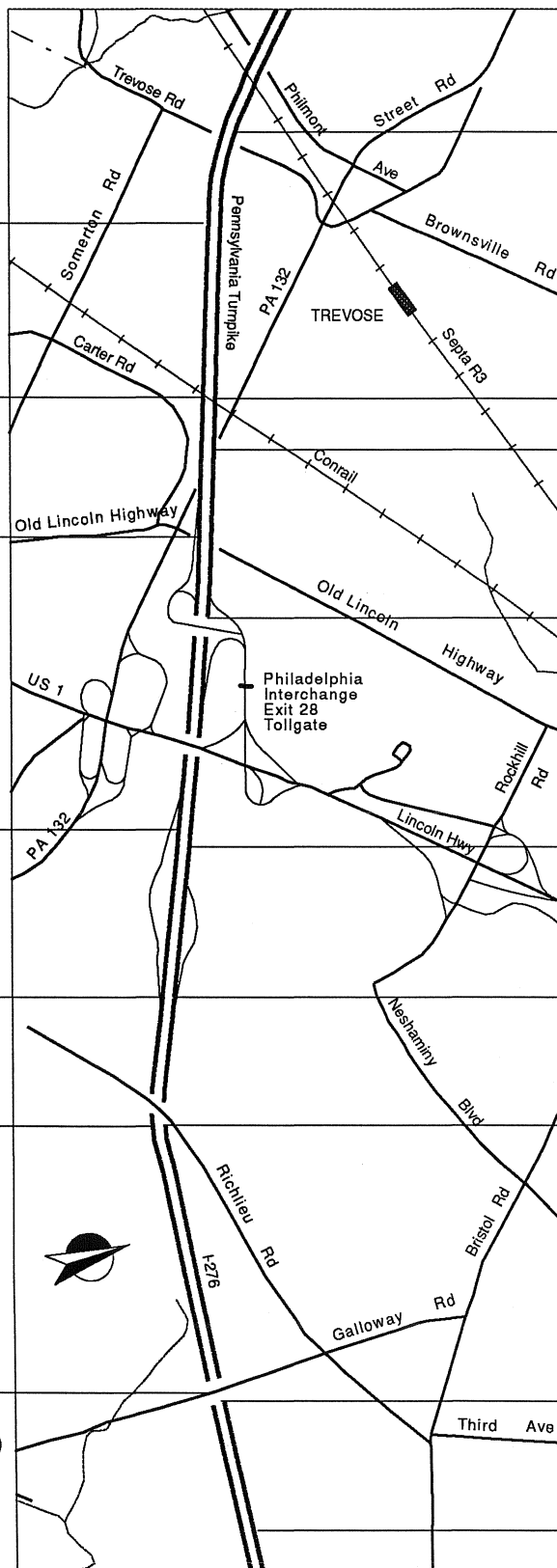
Commercial/Industrial
(Turnpike Rest Area)

Institutional
(Eastern State School)

Cemetery
(Rosedale Memorial Park)

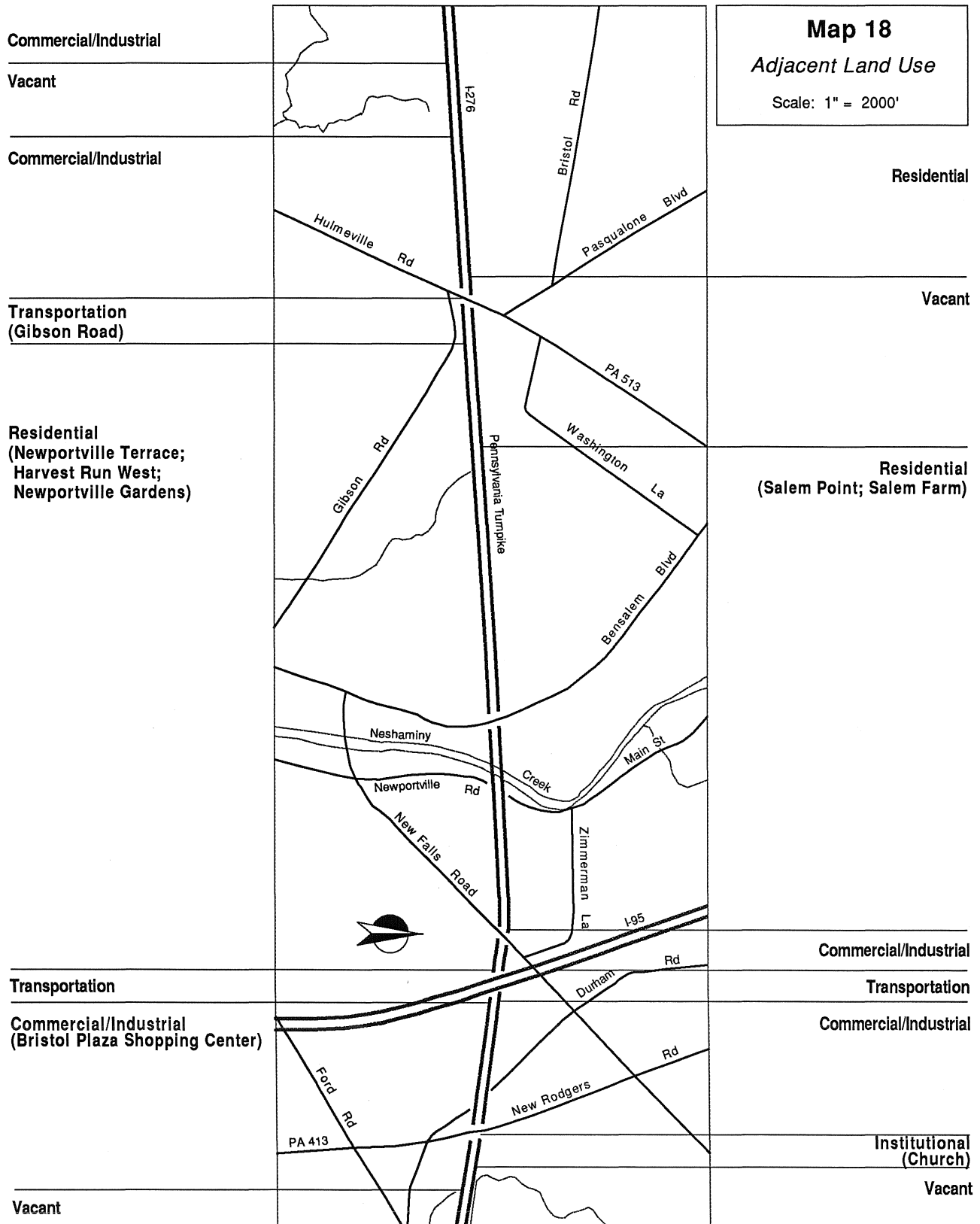
Vacant

Residential





THE PENNSYLVANIA TURNPIKE





THE PENNSYLVANIA TURNPIKE

Map 19
Adjacent Land Use
Scale: 1" = 2000'

Vacant

Residential

Vacant

Institutional
(Bristol Township Building)

Parkland
(Silver Lake County Park)

Commercial/Industrial

Vacant

Commercial/Industrial

Vacant

Commercial/Industrial

Residential

Water

Vacant
(Mill Creek adjacent to
turnpike)

Parkland
(Black Ditch Park)

Residential

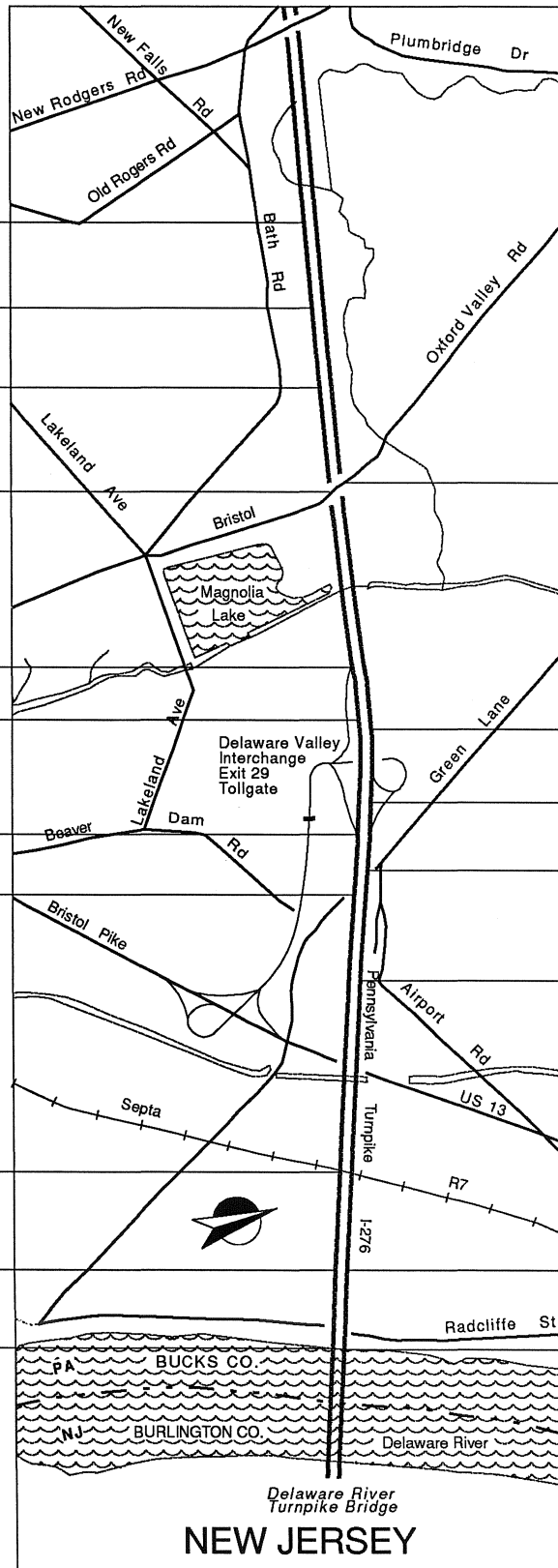
Vacant

Transportation

Commercial/Industrial

Residential

Water



Please

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PLANNED IMPROVEMENTS

This map series summarizes transportation improvements to the Pennsylvania Turnpike corridor which are either underway or programmed. Improvements are also shown for parallel and intersecting roadways. Projects which are not yet funded are not included in this listing.

Each project can be located on the map by referencing its TIP number, which is placed as closely as possible to actual location. When a project entails a corridor, it is referenced at one end only.

Two official program numbers, the Transportation Improvement Program (TIP; source: DVRPC) number and the Program Management System (PMS; source: Penn DOT) number are provided for reference purposes. In the few cases where a TIP number is not specified, federal funding is not involved and the project is therefore not on the TIP. Cost and estimated let and completion dates are subject to change.

Each project is catalogued according to the following format:

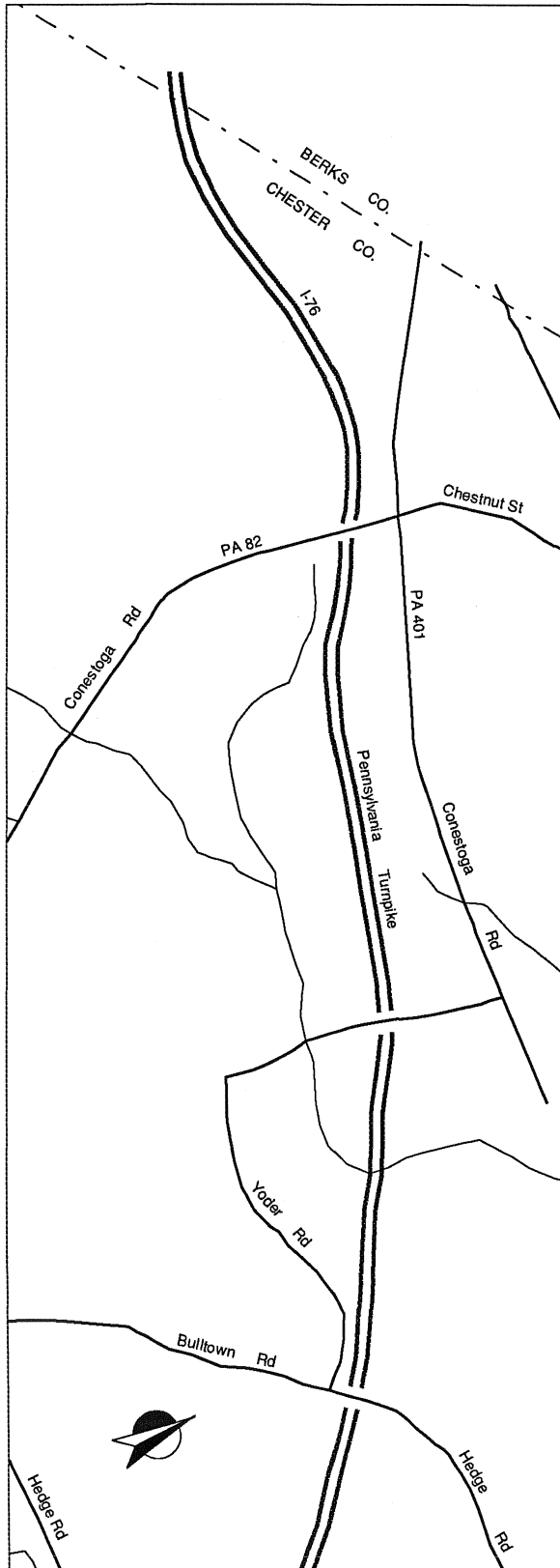
- Project Description
- Project Location
- Limits of Project (if necessary)
- Special Comments (related projects, funding, etc.)
- Transportation Improvement Program and Program Management System Numbers
- Current Estimated Cost
- Actual or Estimated Let Date
- Estimated Completion Date

The Pennsylvania Turnpike is a toll road, and projects are financed through the Turnpike Authority. The section between the Plymouth Meeting and Philadelphia interchanges was reconstructed and widened during the mid-1980's. There are no major projects currently scheduled for this corridor, however there are plans widen all bridges to a fifteen foot watertable in the next few years (no firm schedule has been established). There is one federally-funded project adjacent to the Turnpike at the Downingtown interchange. Project description and location can be found on Map 4.

Information was gathered for this section from DVRPC's Transportation Improvement Program, the City of Philadelphia Streets Department, and the PennDOT 12-year plan.



THE PENNSYLVANIA TURNPIKE



Map 1

Planned Improvements

Scale: 1" = 2000'

NO PROJECTS



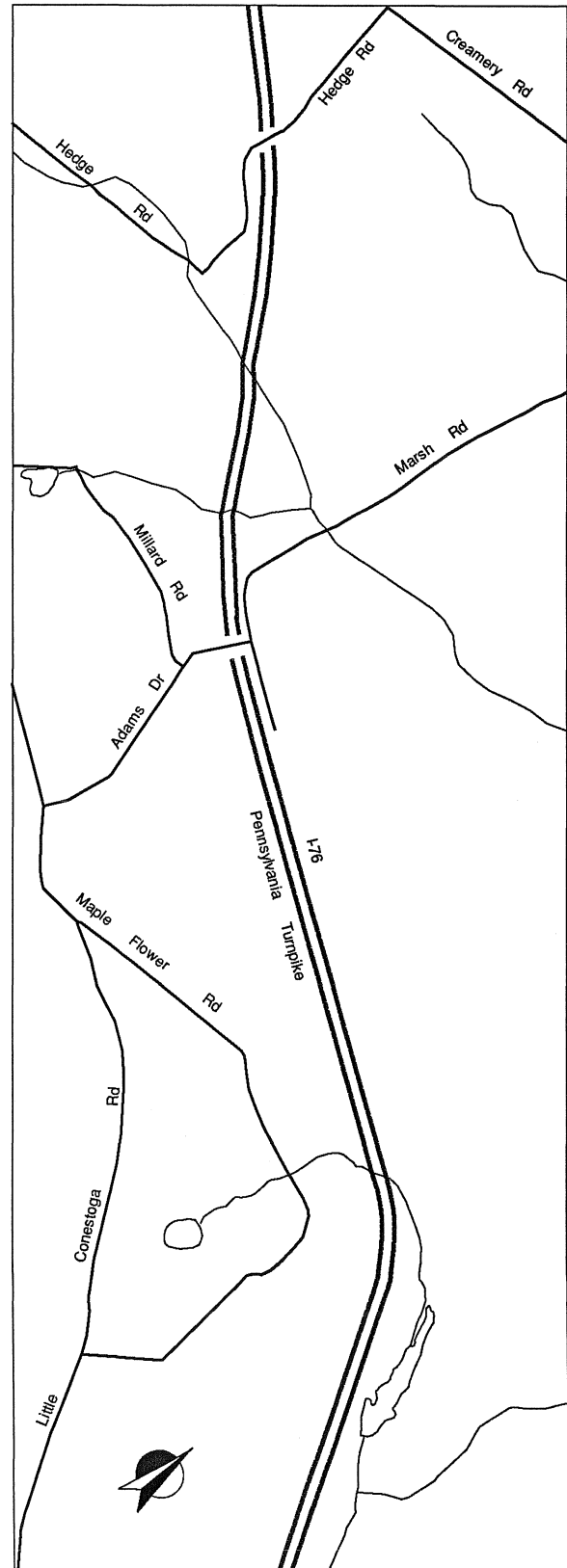


THE PENNSYLVANIA TURNPIKE

Map 2
Planned Improvements

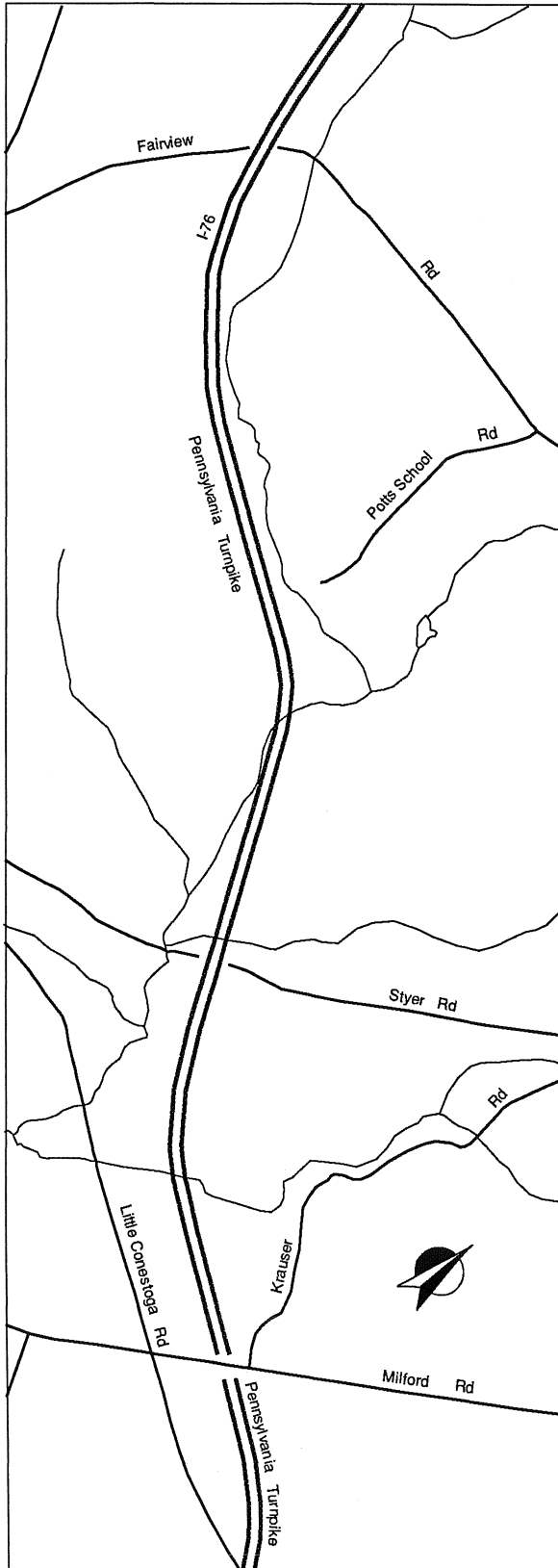
Scale: 1" = 2000'

NO PROJECTS





THE PENNSYLVANIA TURNPIKE



Map 3

Planned Improvements

Scale: 1" = 2000'

NO PROJECTS





THE PENNSYLVANIA TURNPIKE

Map 4

Planned Improvements

Scale: 1" = 2000'

Park & Ride Lot

PA 100, Pottstown Pike

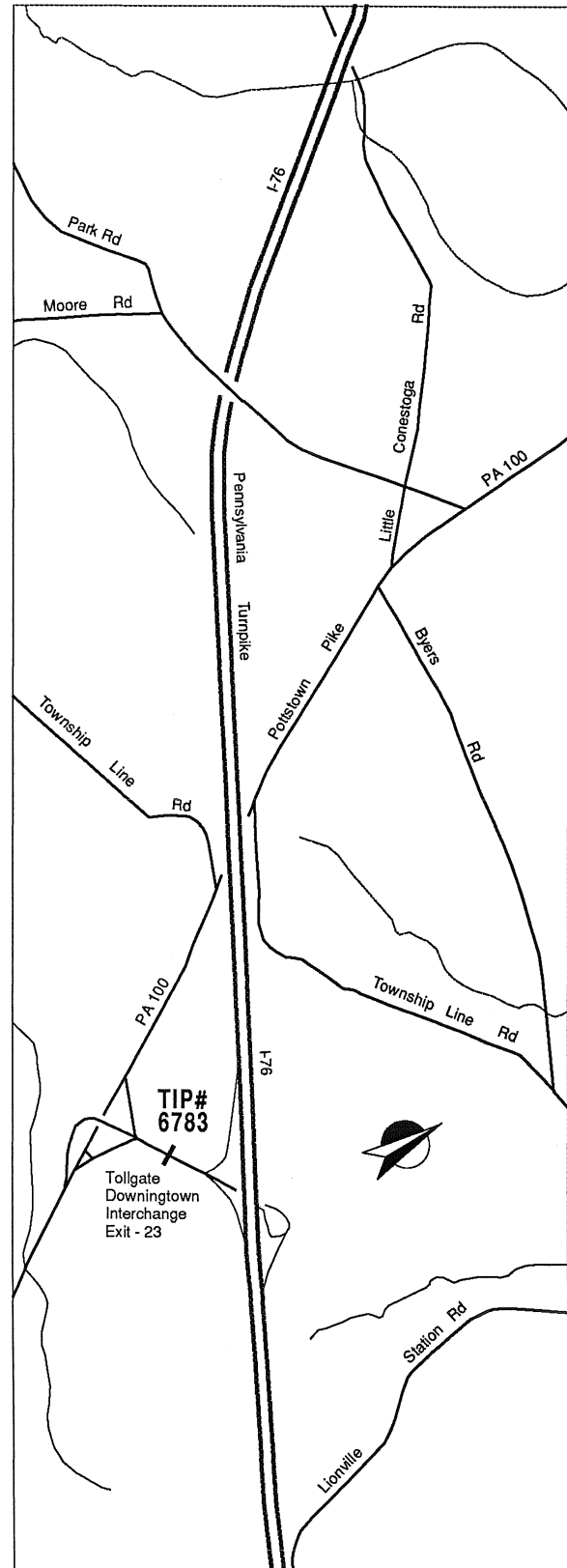
At PA Turnpike Downingtown Interchange

Uwchlan Township

TIP # 6783

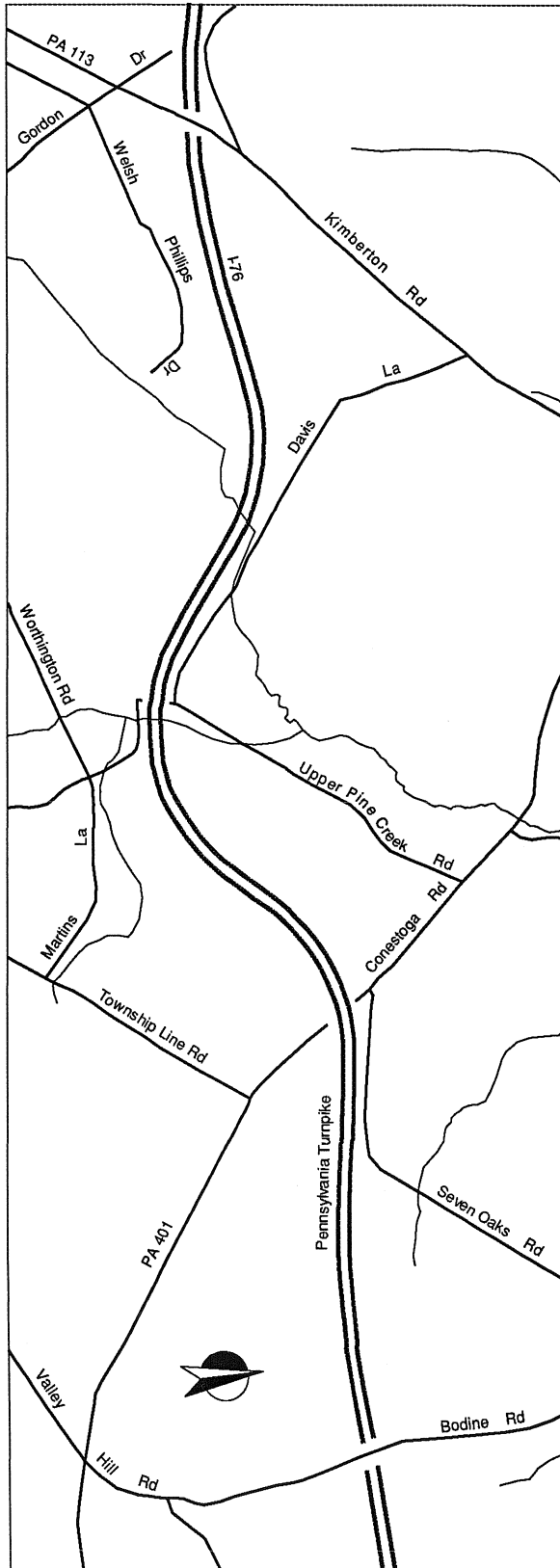
PMS # 062000

\$4.0 M





THE PENNSYLVANIA TURNPIKE



Map 5

Planned Improvements

Scale: 1" = 2000'

NO PROJECTS





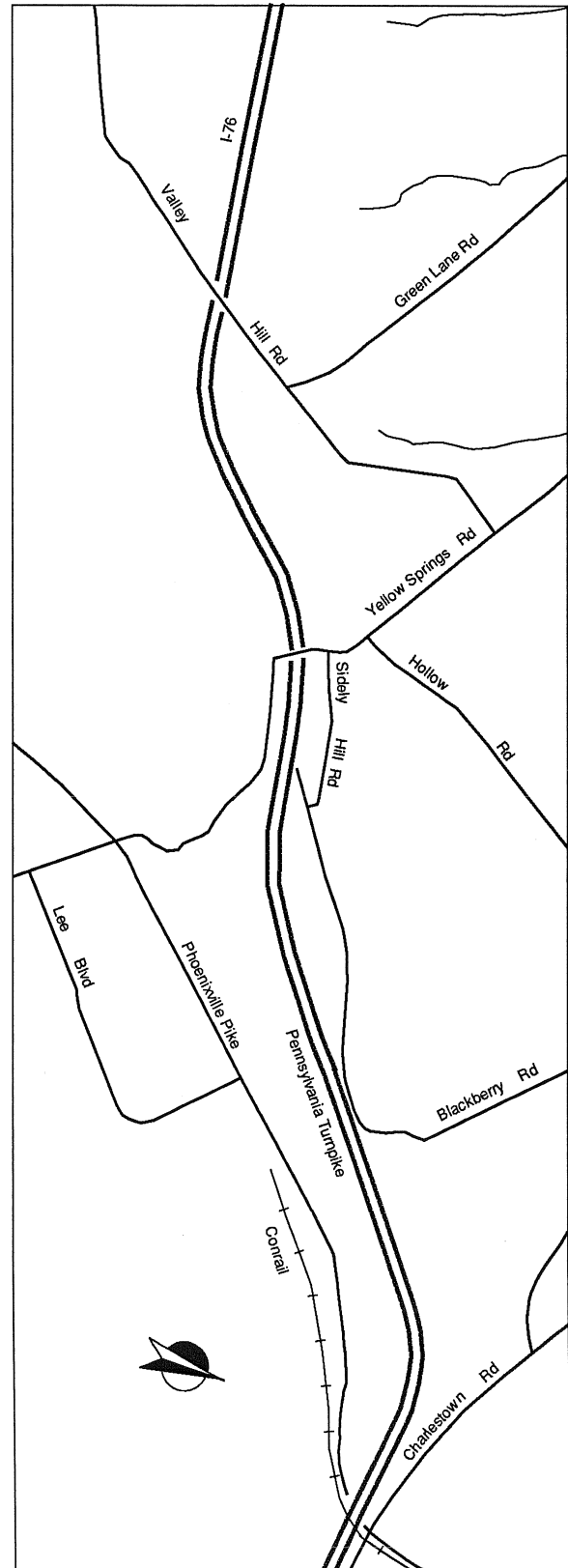
THE PENNSYLVANIA TURNPIKE

Map 6

Planned Improvements

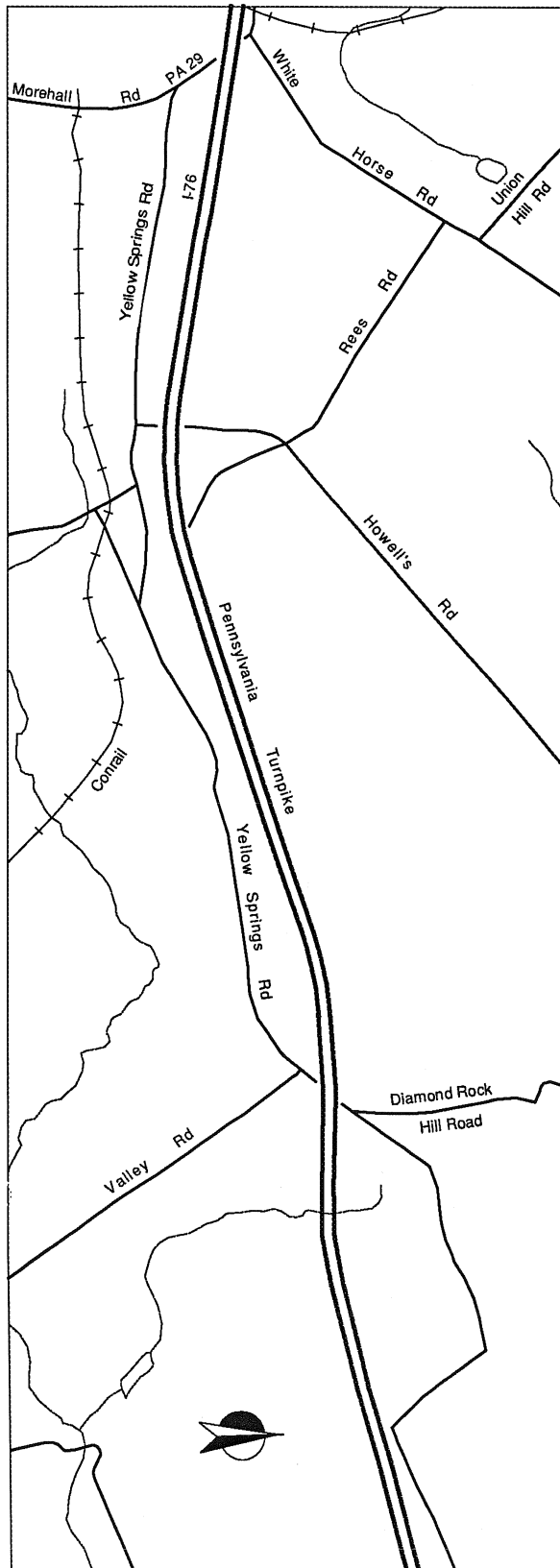
Scale: 1" = 2000'

All bridges over streams in this section to be widened to 15' watertables beginning in 1993.





THE PENNSYLVANIA TURNPIKE



Map 7

Planned Improvements

Scale: 1" = 2000'

All bridges over streams in this section to be widened to 15' watertables beginning in 1993.





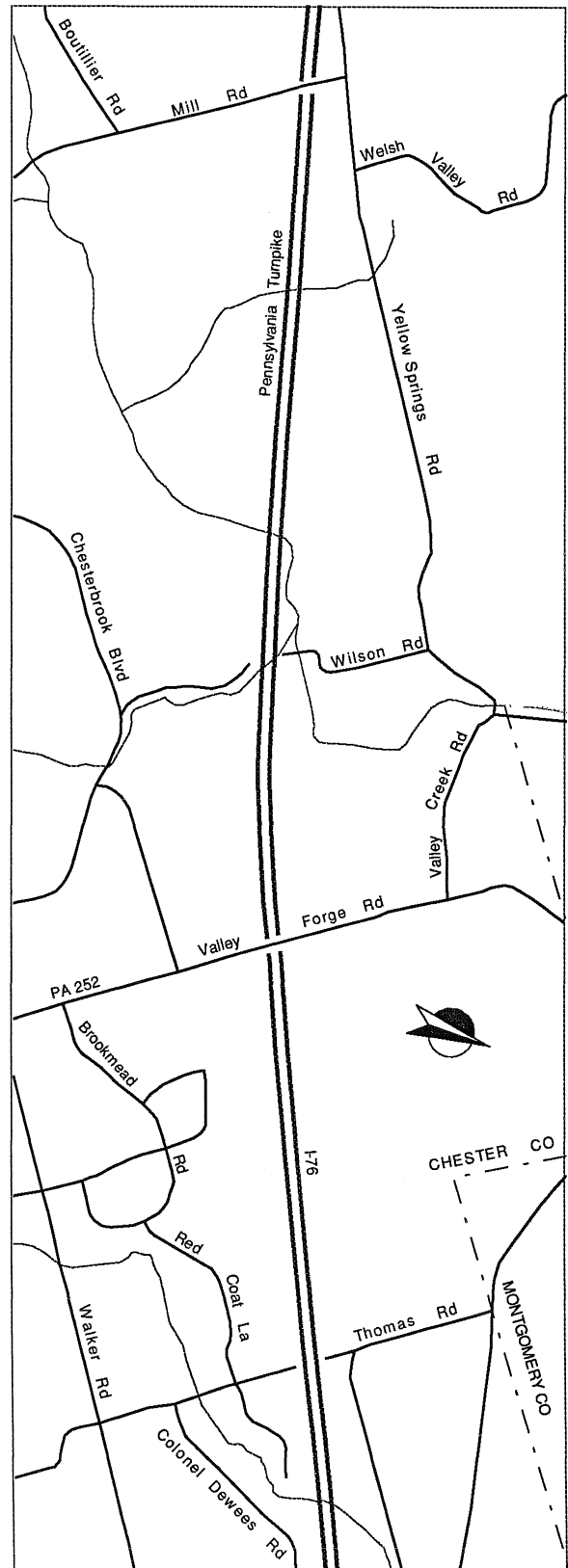
THE PENNSYLVANIA TURNPIKE

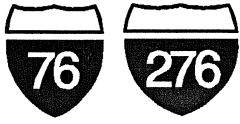
Map 8

Planned Improvements

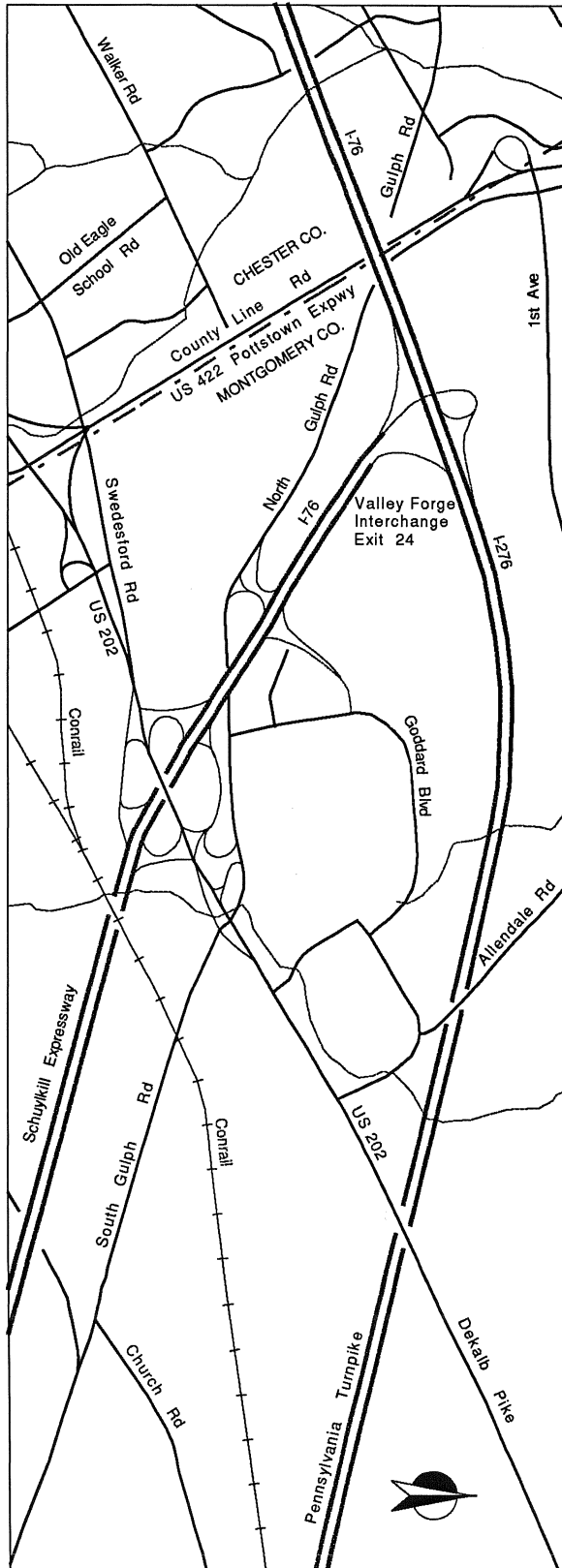
Scale: 1" = 2000'

All bridges over streams in this section to be widened to 15' watertables beginning in 1993.





THE PENNSYLVANIA TURNPIKE



Map 9

Planned Improvements

Scale: 1" = 2000'

All streams in this section west of 326.3 to be widened to 15' watertables beginning in 1993.





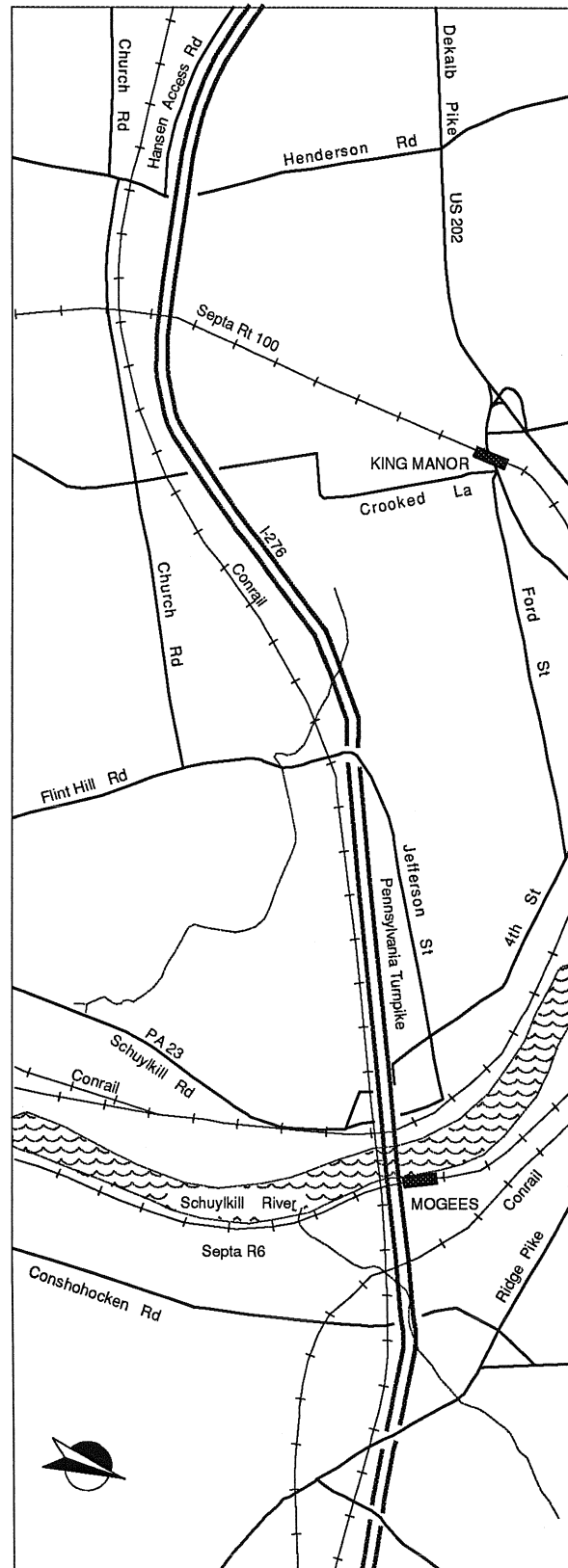
THE PENNSYLVANIA TURNPIKE

Map 10

Planned Improvements

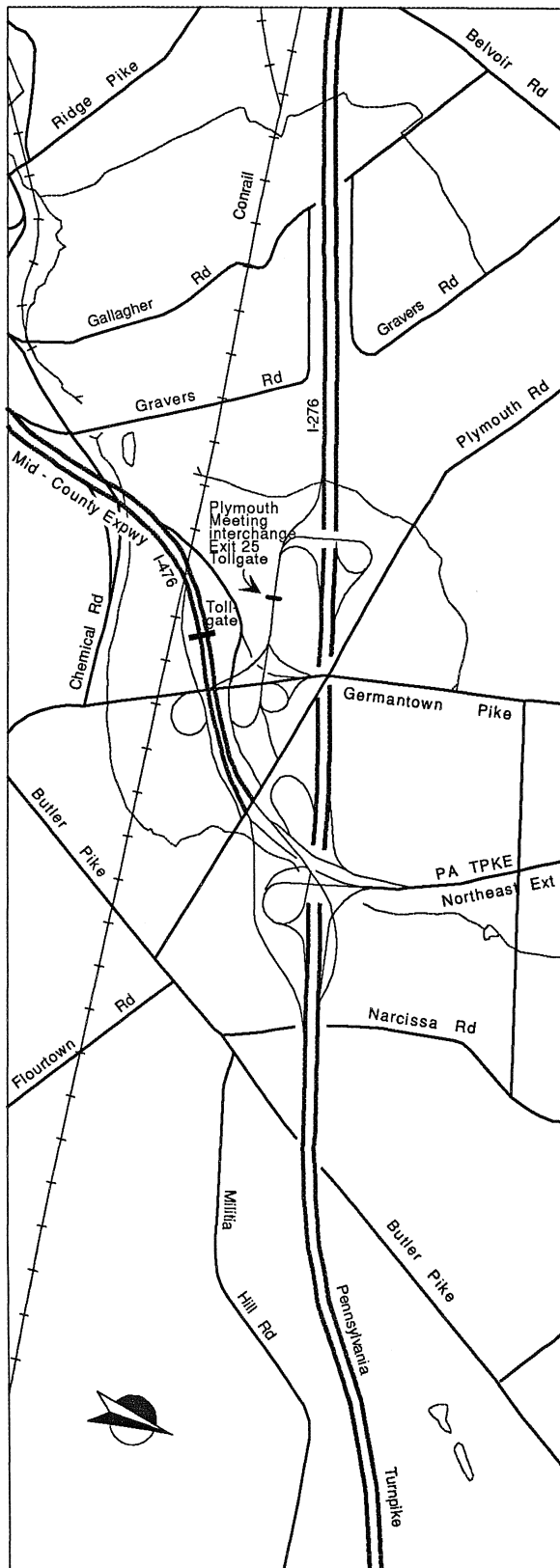
Scale: 1" = 2000'

NO PROJECTS





THE PENNSYLVANIA TURNPIKE



Map 11

Planned Improvements

Scale: 1" = 2000'

NO PROJECTS

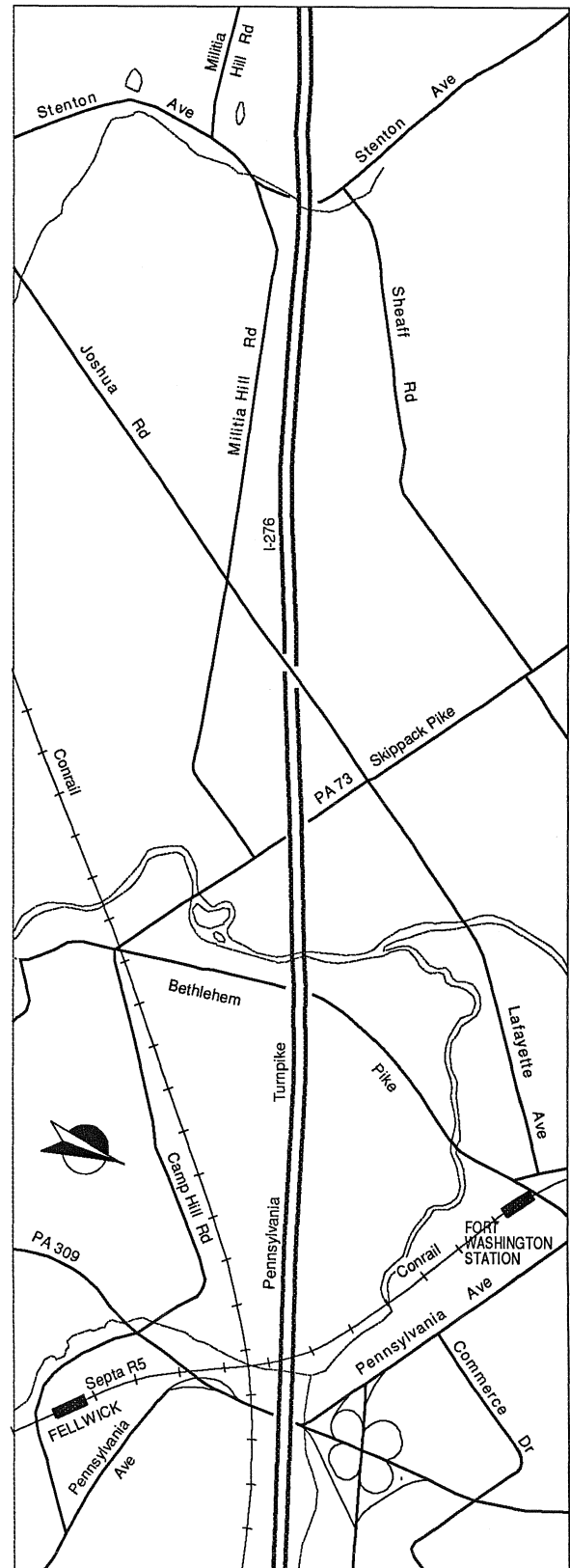




THE PENNSYLVANIA TURNPIKE

Map 12
Planned Improvements
Scale: 1" = 2000'

NO PROJECTS



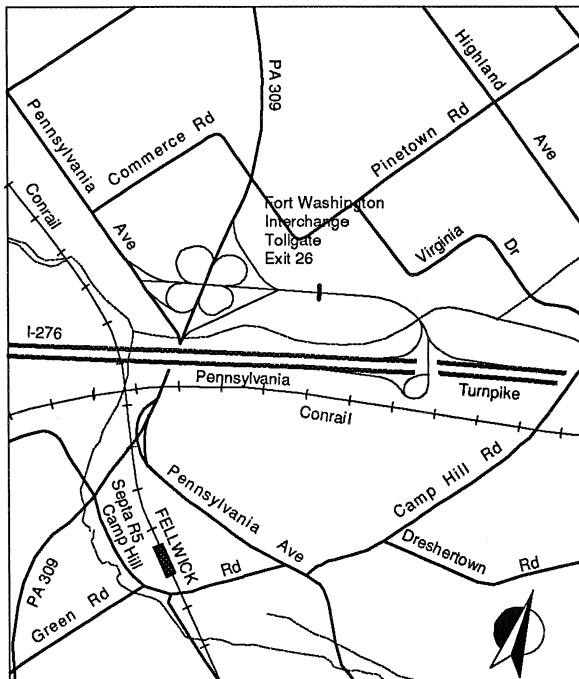


THE PENNSYLVANIA TURNPIKE

Map 12A

Planned Improvements

Scale: 1" = 2000'



NO PROJECTS





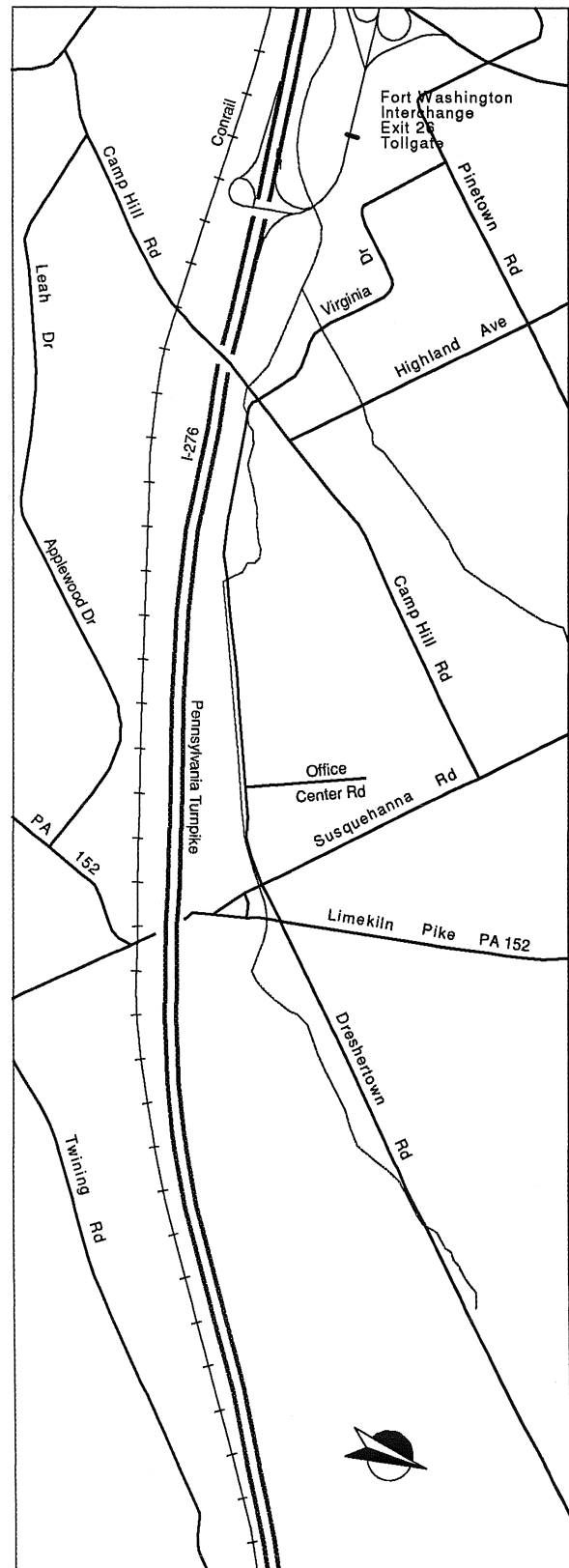
THE PENNSYLVANIA TURNPIKE

Map 13

Planned Improvements

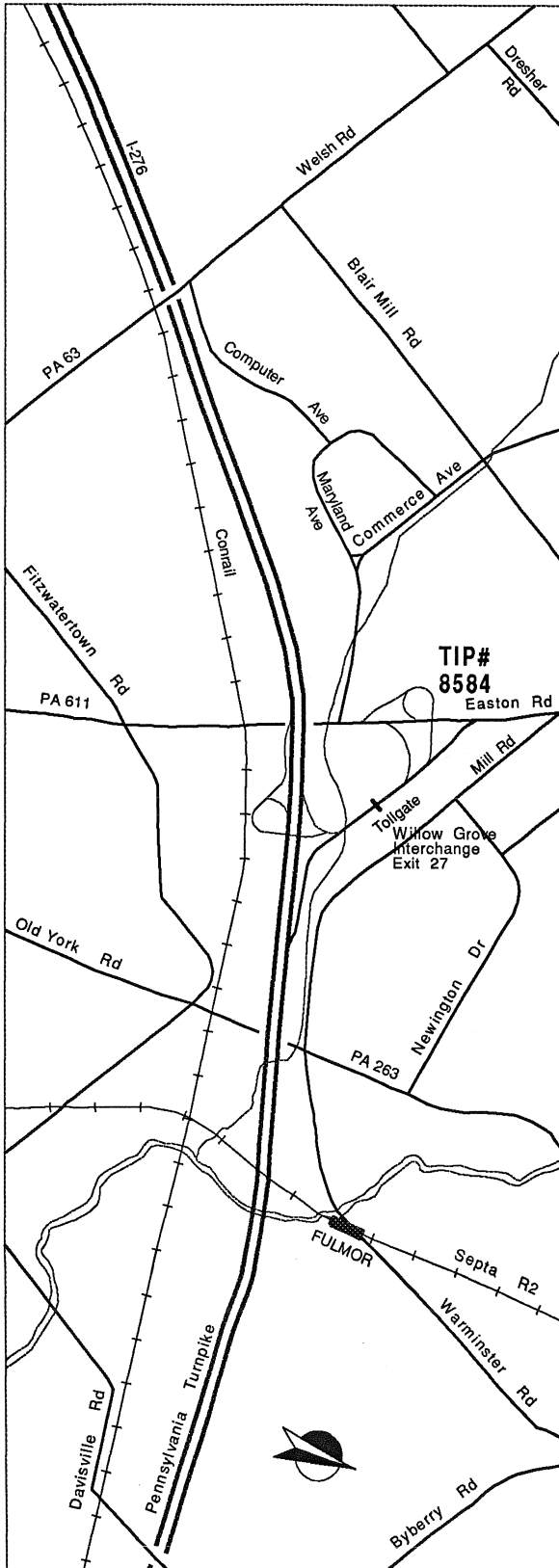
Scale: 1" = 2000'

NO PROJECTS





THE PENNSYLVANIA TURNPIKE



Map 14

Planned Improvements

Scale: 1" = 2000'

Widening to 6 Lanes, Intersection Improvements

PA 611, Easton Road

I-276 (PA Turnpike) to Blair Mill Road

TIP # 8584

PMS # 0640000

\$1,420,000





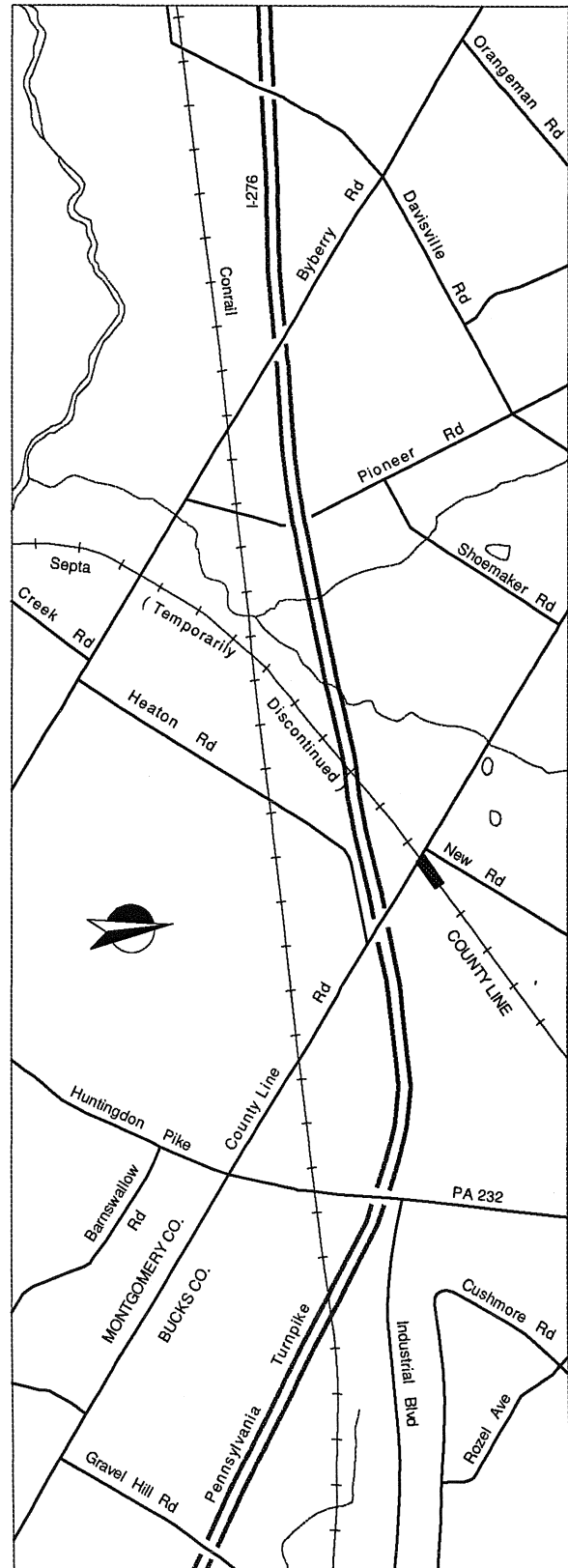
THE PENNSYLVANIA TURNPIKE

Map 15

Planned Improvements

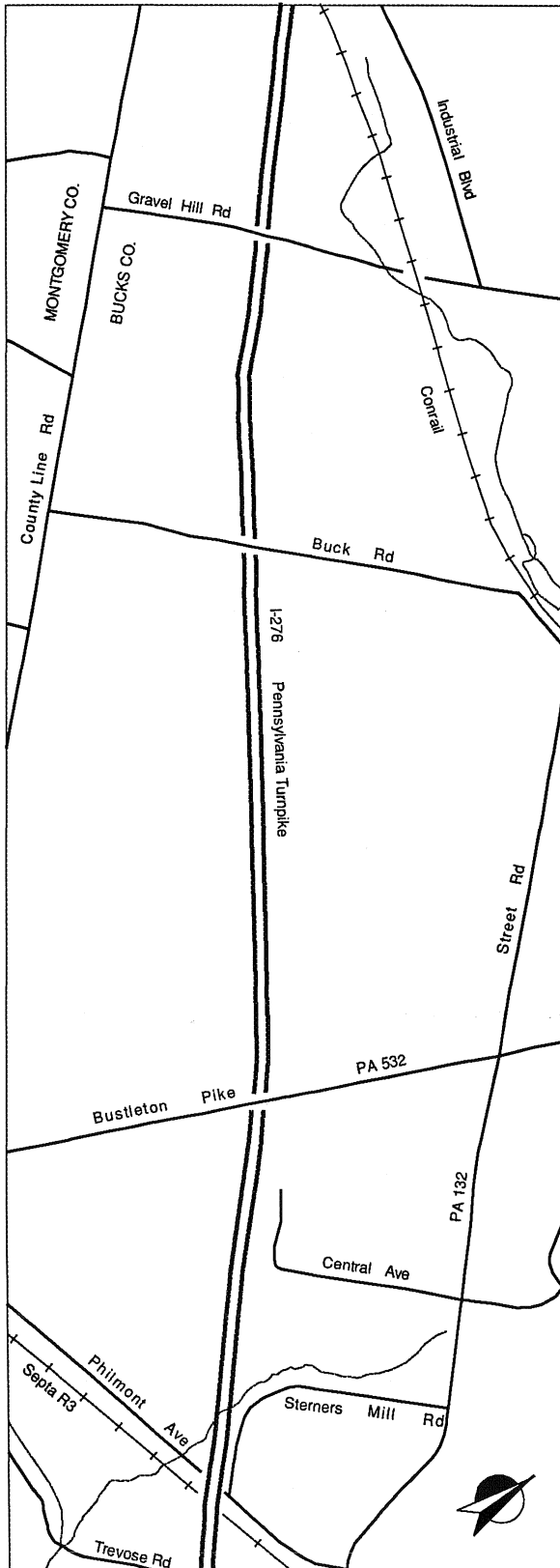
Scale: 1" = 2000'

NO PROJECTS





THE PENNSYLVANIA TURNPIKE



Map 16

Planned Improvements

Scale: 1" = 2000'

NO PROJECTS





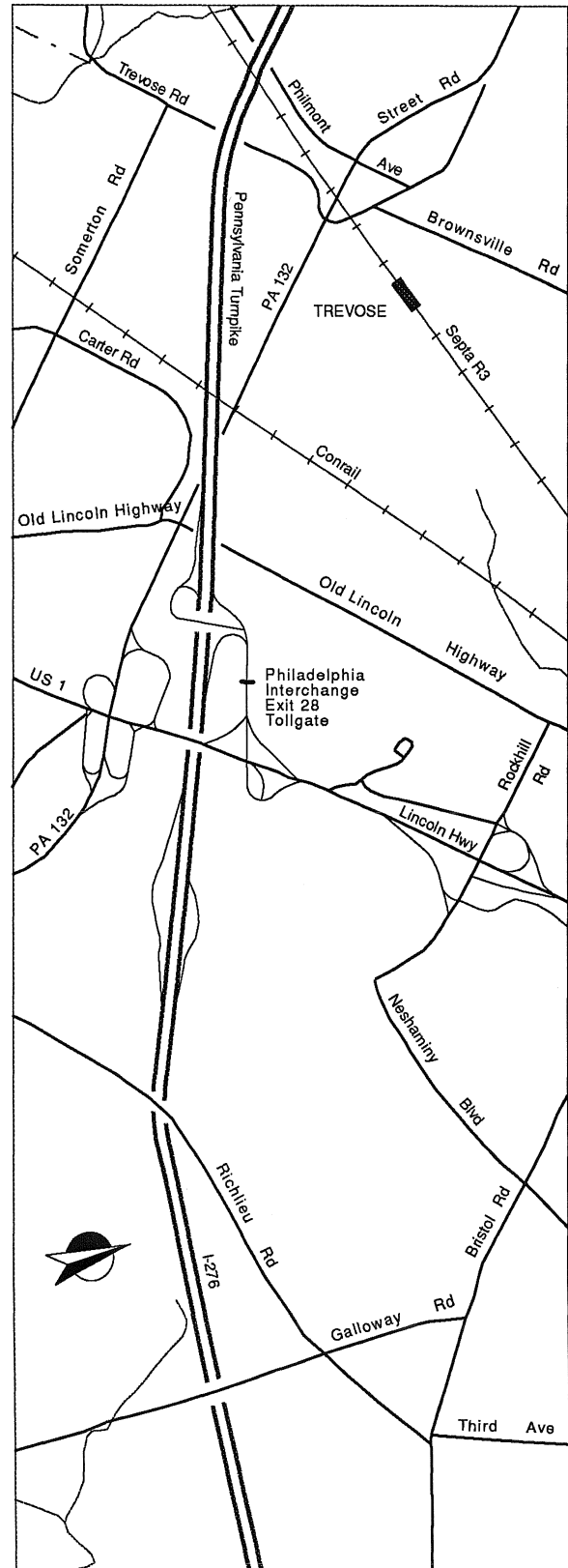
THE PENNSYLVANIA TURNPIKE

Map 17

Planned Improvements

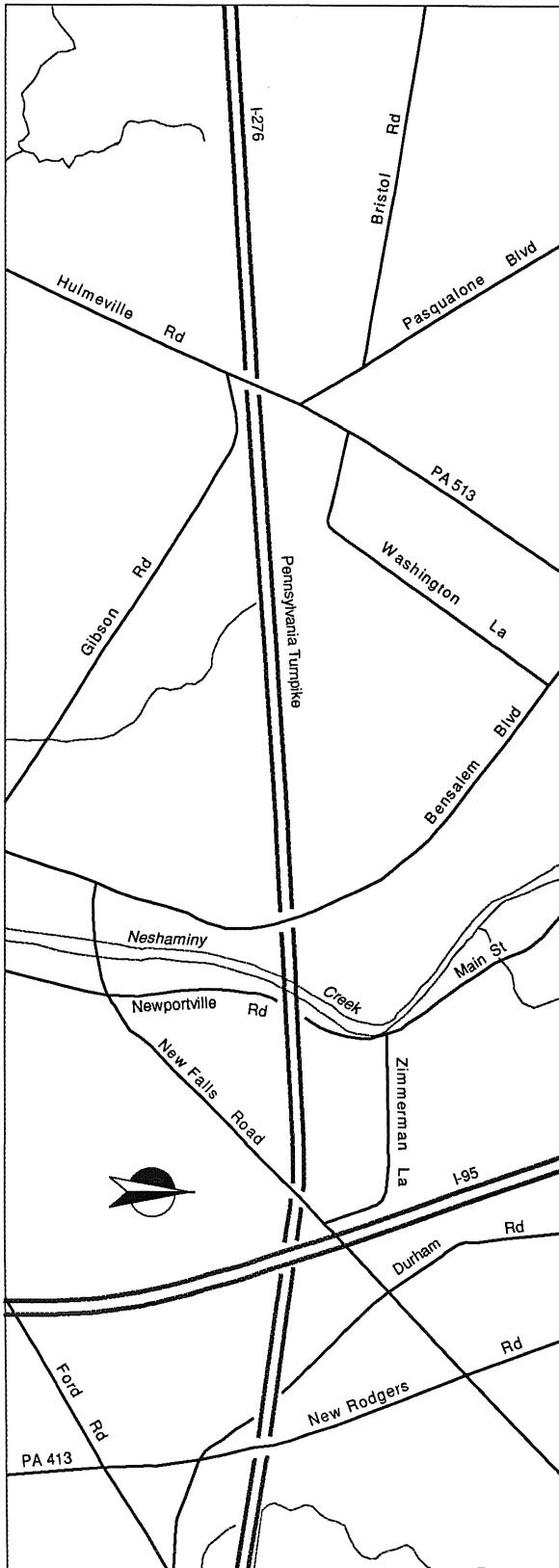
Scale: 1" = 2000'

NO PROJECTS





THE PENNSYLVANIA TURNPIKE



Map 18
Planned Improvements

Scale: 1" = 2000'

NO PROJECTS

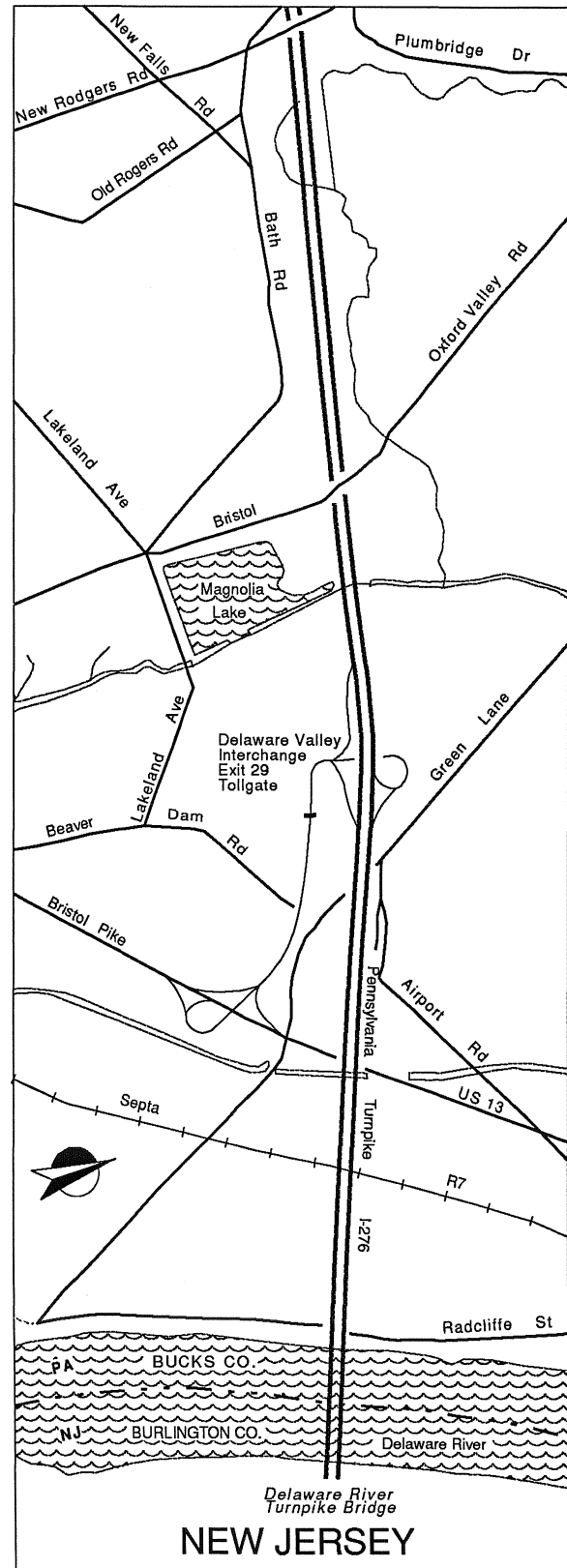




THE PENNSYLVANIA TURNPIKE

Map 19
Planned Improvements
Scale: 1" = 2000'

NO PROJECTS



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NATURAL AND CULTURAL ENVIRONMENT

The natural and cultural environment maps identify noteworthy features located within close proximity to the roadway. Natural features displayed on the map include wetlands and flood plains. Cultural features are primarily historic.

Most of the wetlands information, including location and general configuration, was derived from the National Wetlands Inventory (NWI) Maps, issued by the United States Department of the Interior. Wetlands are identified by a pattern of horizontal lines. No distinction is made between the various classifications of wetlands (such as estuarine, palustrine, riverine, marine, or lacustrine).

It should be noted that the NWI maps are compiled on a very large scale and are therefore very general. An indication of wetlands on an NWI map suggests that wetlands are probably present to some extent on that particular site. Conversely, an indication that wetlands are not present suggests that the site is probably free of wetlands. However, any specific site which is proposed for development must be surveyed individually before a final determination can be made as to whether or not wetlands are actually present.

The flood plains delineated on the map represent the 100-year flood plain boundaries as identified on the Flood Insurance Rate Maps distributed by the Federal Emergency Management Agency. Flood plain areas are indicated by a pattern of vertical lines. Areas where flood plains and wetlands overlap are indicated by intersecting vertical and horizontal lines. Wetlands which are actually bodies of standing water are already depicted on the map.

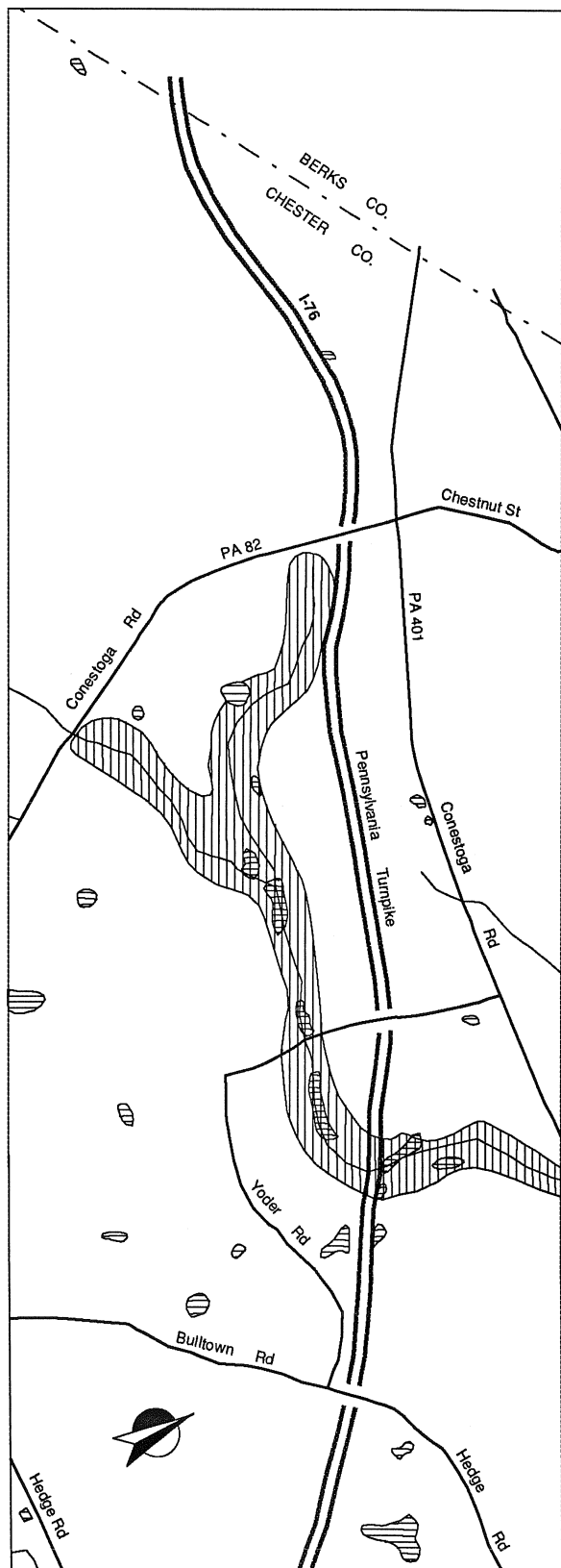
Historical features are also identified. These features include sites, structures, or districts which are significant in terms of American history, architecture, or culture. The general locations of the features are indicated on the map by large, upper-case letters. The site name, the year built, a general description and the type of ownership are provided in the margin text. Sites which are listed on the national Register of Historic Places are identified as such.

Other sites or buildings which have been determined to be eligible for the National Register are also identified. All public agencies are required to safeguard properties on the National Register as well as those which are or may be eligible for it.

Information for this section was gathered from DVRPC reports and field observations.



THE PENNSYLVANIA TURNPIKE



Map 1

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain





THE PENNSYLVANIA TURNPIKE

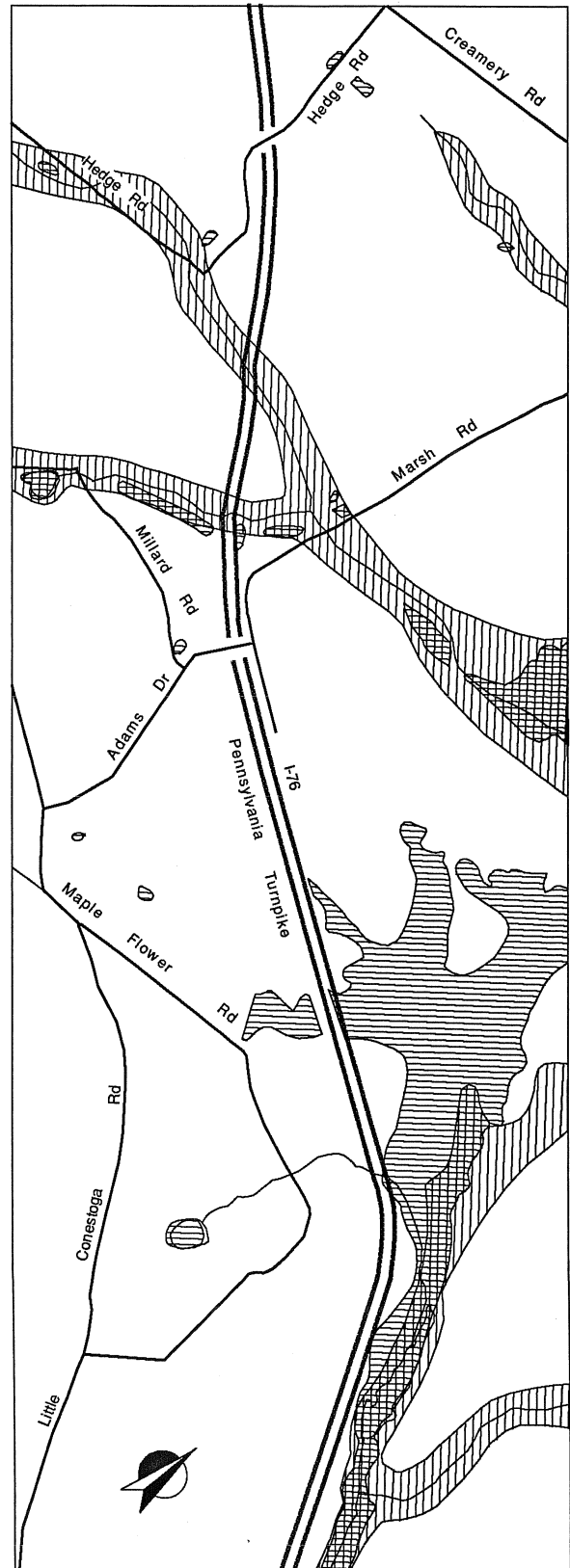
Map 2

Natural and Cultural Environment

Scale: 1" = 2000'

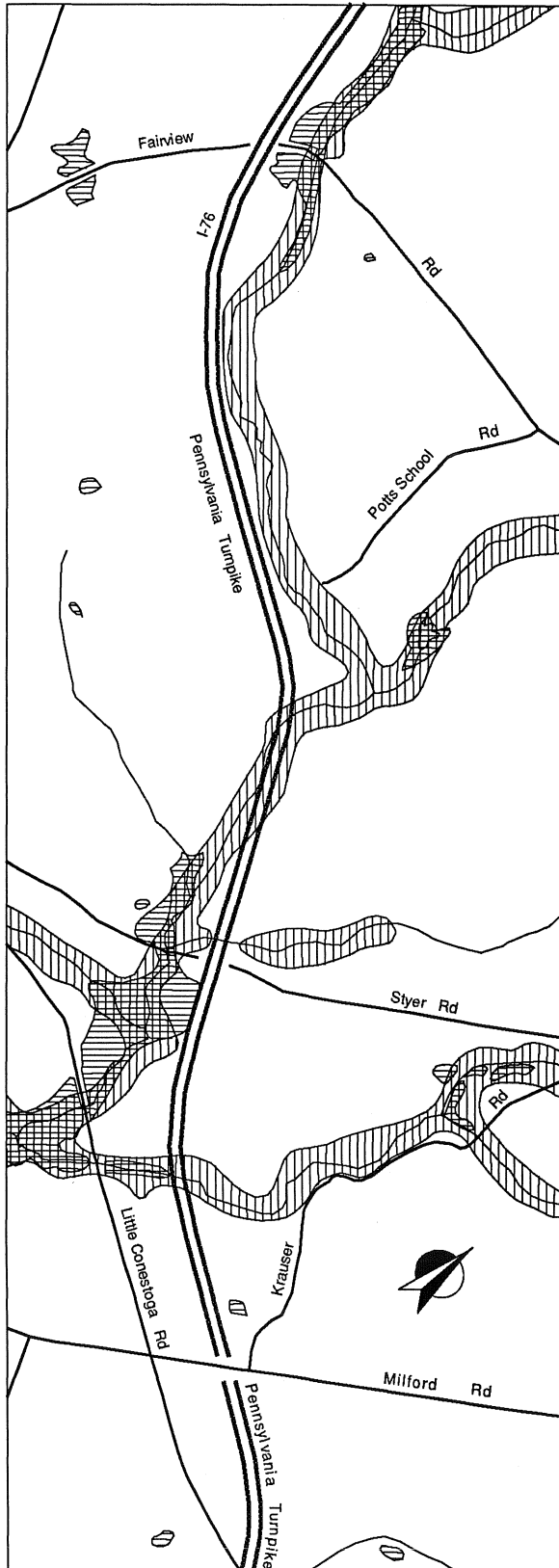
Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain





THE PENNSYLVANIA TURNPIKE



Map 3

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain





THE PENNSYLVANIA TURNPIKE

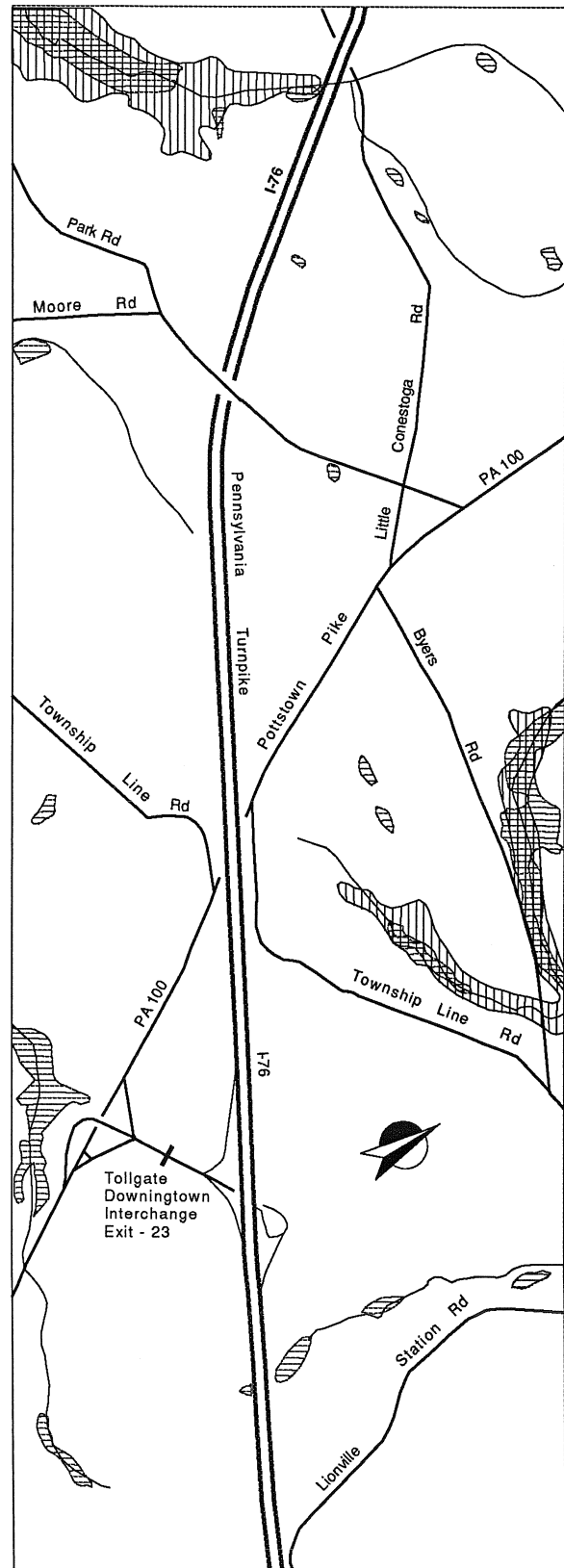
Map 4

Natural and Cultural Environment

Scale: 1" = 2000'

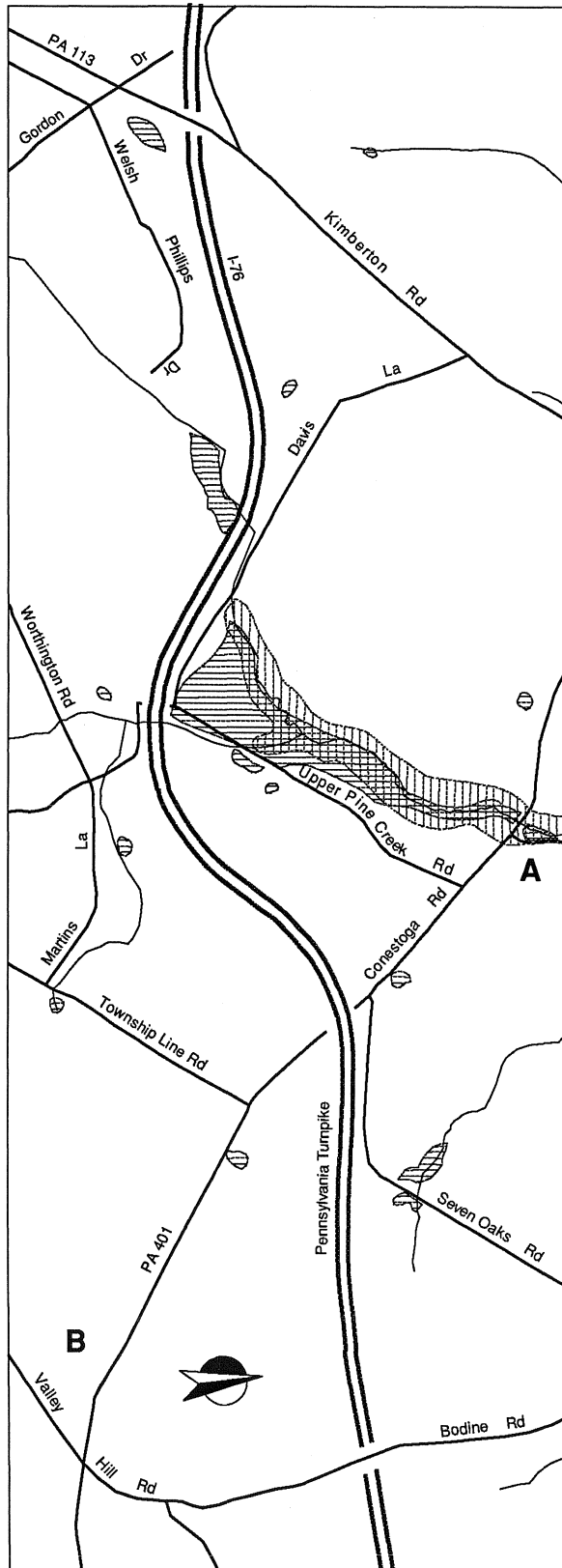
Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain





THE PENNSYLVANIA TURNPIKE



Map 5

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

- A Residence of D.A. Metz**
Pine Creek Road and PA 401.
Stone farmhouse; built 1780-1815.
Private.
- B Abigail Rice Home (Marker)**
One-quarter mile west of Conestoga
Road, north of Bacton. Built
circa 1761. Private.





THE PENNSYLVANIA TURNPIKE

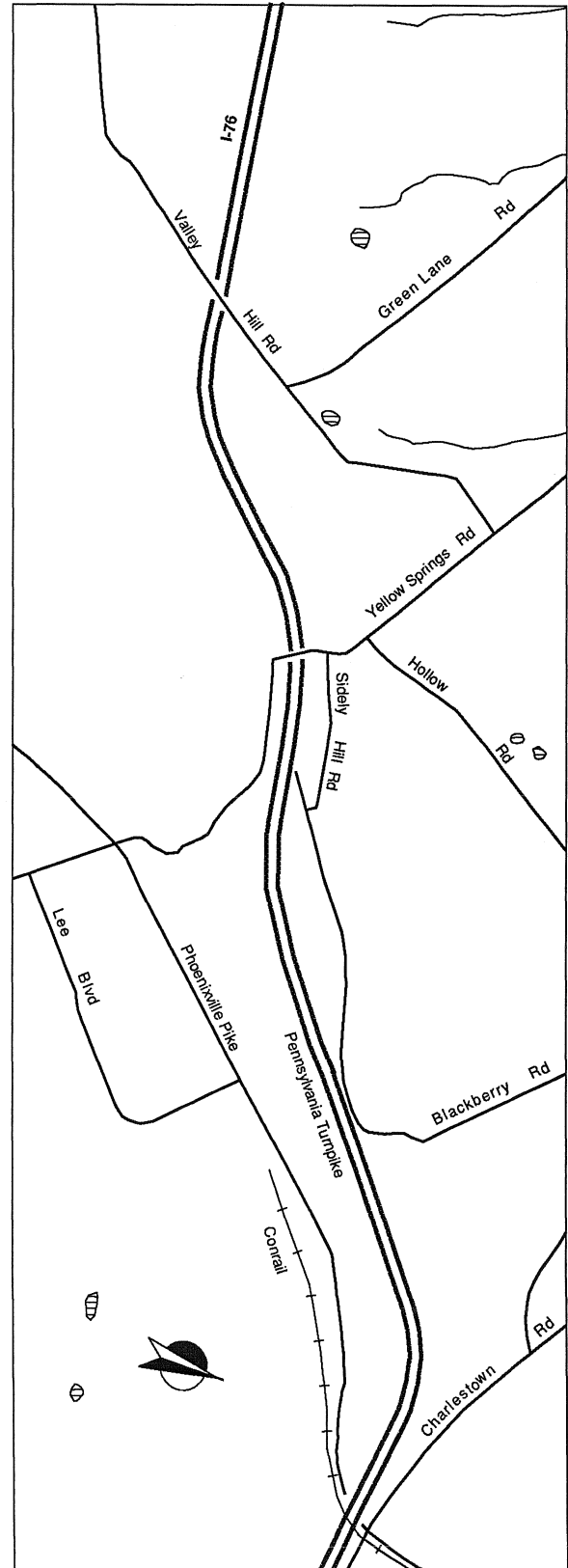
Map 6

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain





THE PENNSYLVANIA TURNPIKE

Map 7

Natural and Cultural Environment

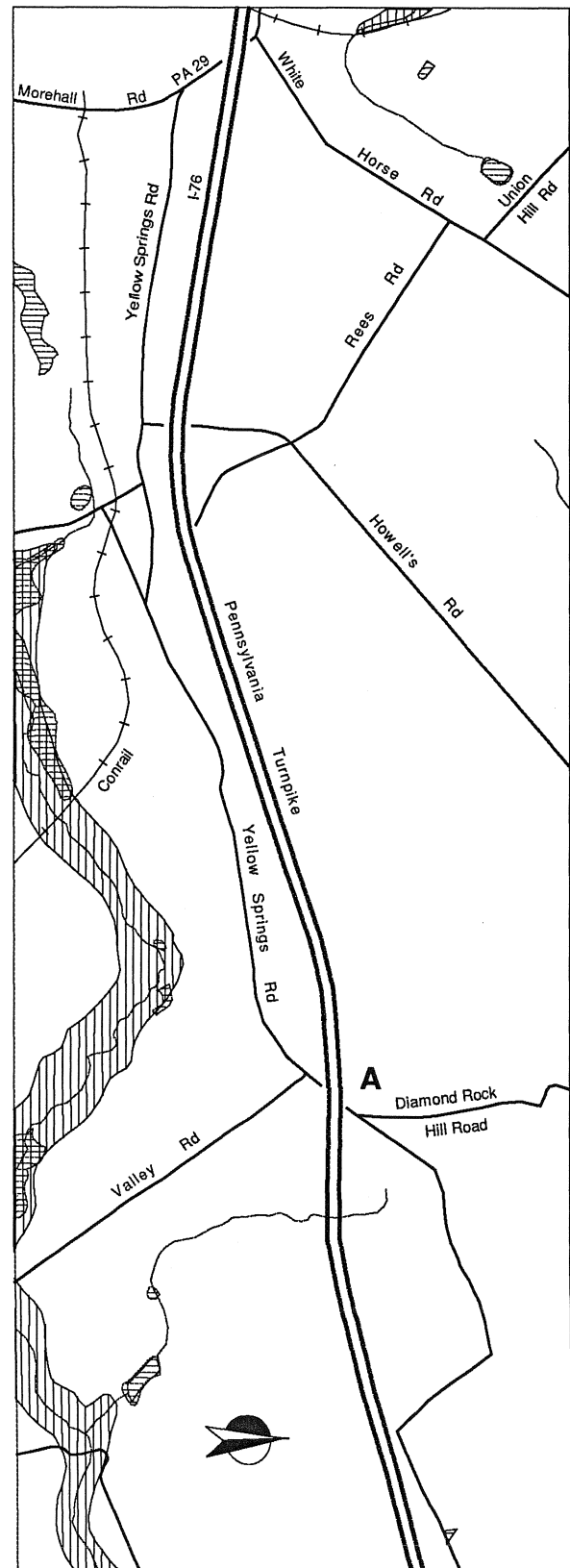
Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

A Octagonal School House

Yellow Springs and Diamond Rock Hill Roads.
Built 1818. Old Pupils Association.





THE PENNSYLVANIA TURNPIKE

Map 8

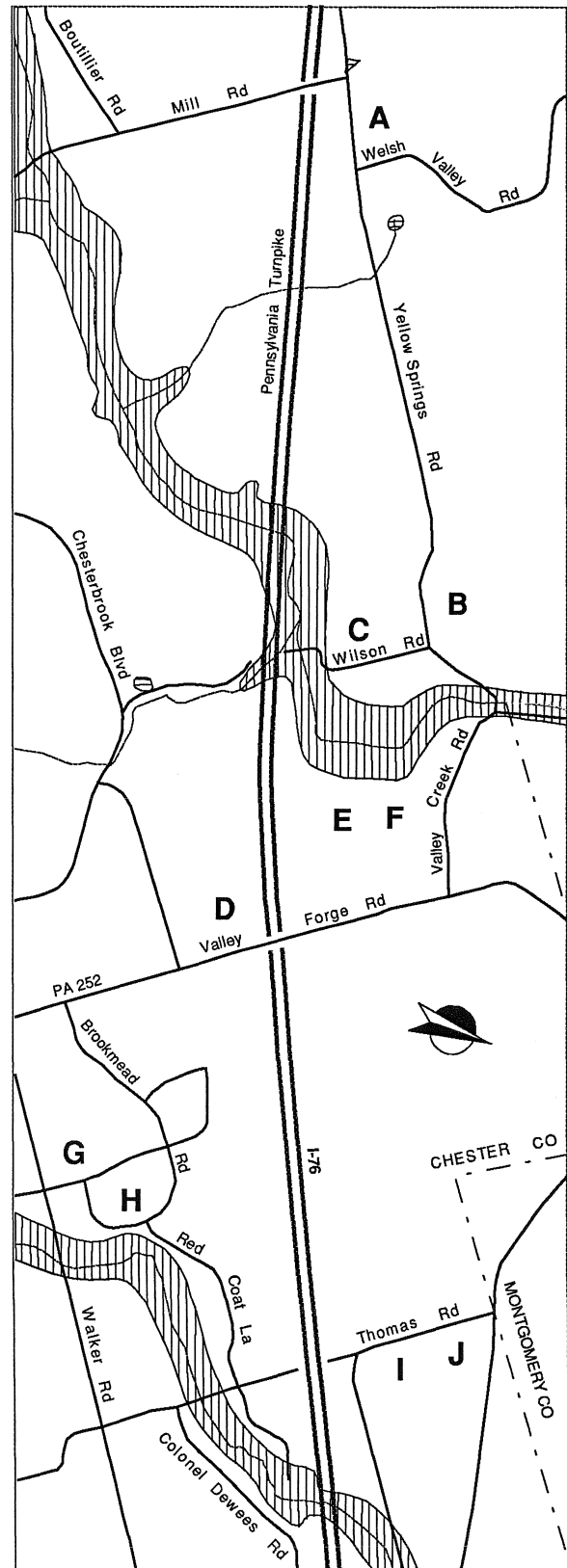
Natural and Cultural Environment

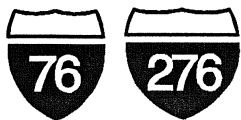
Scale: 1" = 2000'

Horizontal pattern: wetlands

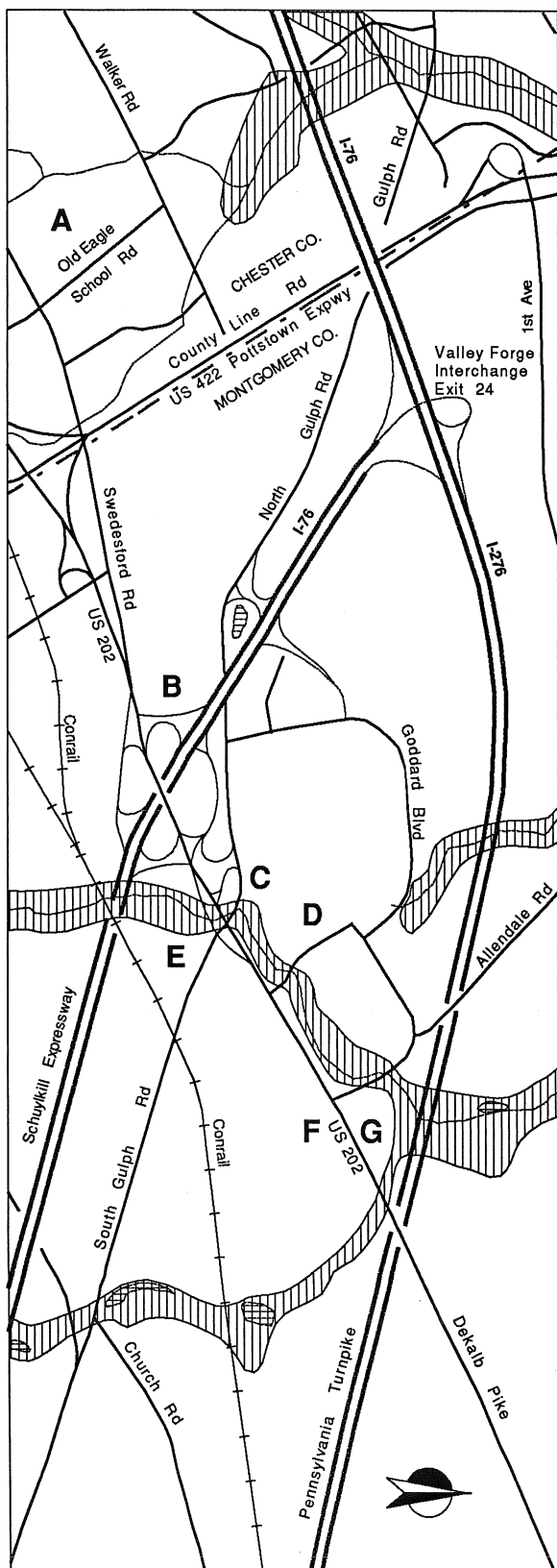
Vertical pattern: 100 - year flood plain

- A Hopper Log House (John Jacobs Log House)**
East of Parson Curry Road. Built circa 1758; 19th century additions. Private.
- B Stirling's Quarters**
Yellow Springs Road. Built 1769. Private.
- C Lafayette's Quarters (Brookside Farm)**
Wilson Road. Built 1763. Lafayette's quarters 1777-1778. Private.
- D DuPortail's Quarters (Chesterbrook Farm)**
West side of Route 252. Part of 1777 Valley Forge encampment. University of Pennsylvania.
- E Knox's Quarters (Valley Forge Farm)**
Route 252 and Valley Creek at Yellow Springs Road. Valley Forge Park Commission.
- F Maxwell's Quarters (Valley Forge Farm)**
Route 252 and Valley Creek at Yellow Springs Road. Built 1774. Valley Forge Park Commission.
- G Wayne's Quarters (Many Springs Farm)**
Route 252 and Anthony Wayne Drive. Built 1757. Valley Forge Park Commission.
- H Pulaski's Quarters (Brookmead Farm)**
North side of Walker Road. Built 1767. Private.
- I Potters Quarters**
1444 Thomas Road. Built 1715; part of Valley Forge encampment. Private.
- J J Poor's Quarters (Little Place Farm)**
1406 Thomas Road. Built circa 1736; part of Valley Forge encampment. Private.





THE PENNSYLVANIA TURNPIKE



Map 9

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

- A Greene's Quarters (Rehoboth Farm)**
Swedesford Road near Old Eagle School Road.
Built 1696; 18th century additions.
Part of 1777 Valley Forge encampment. Private.
- B Valley Forge Club House**
Gulph Road. Built 1728.
Private.
- C King of Prussia Inn**
Built 1719. Located on "island" in
Route 202 at Gulph Road.
Commonwealth of Pennsylvania.
- D King of Prussia Store**
Gulph Road and Route 202. Built circa 1800.
Private.
- E Peacock Gardens**
Gulph Road and Route 202. Built circa 1724.
Private.
- F Stewart Fund Hall**
Swedesford (Route 202) and Allendale Roads.
Built 1878.
Upper Merion Township's first significant public
building.
- G Union School Masters Quarters and Addition**
Swedesford and Allendale Roads.
Built circa 1810.
Used as station on the Underground Railroad,
1830-1865. Private.





THE PENNSYLVANIA TURNPIKE

Map 10

Natural and Cultural Environment

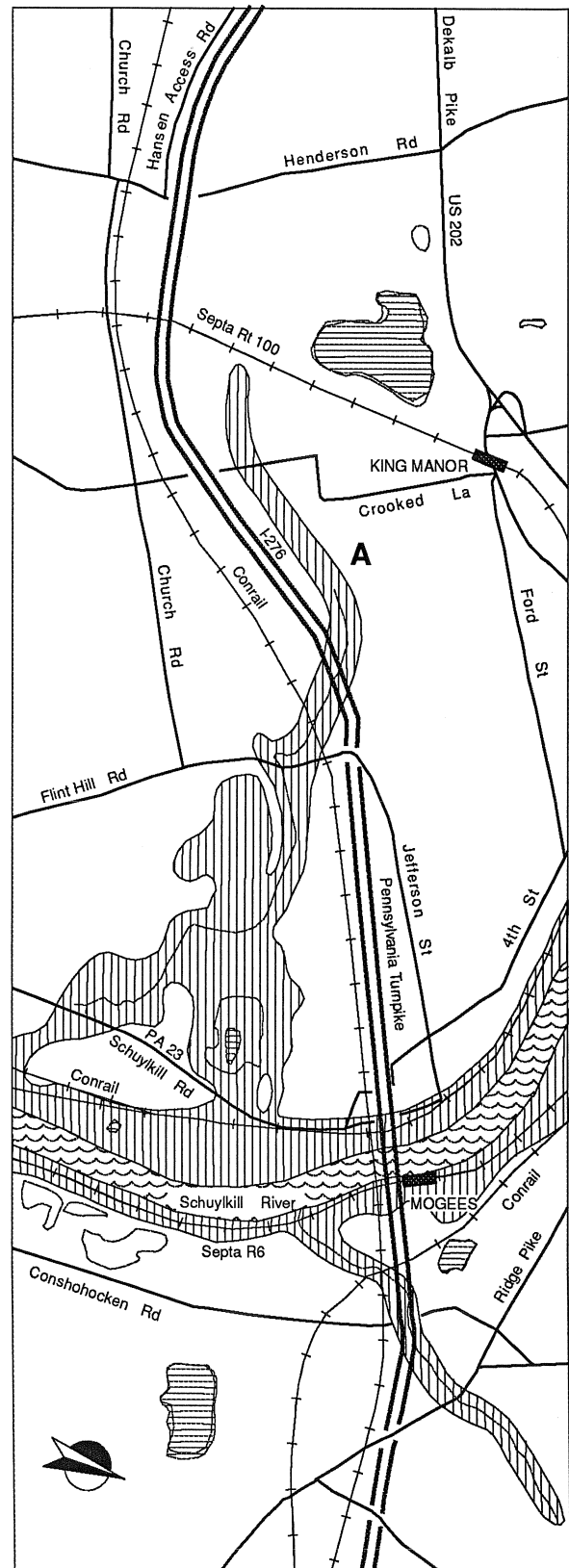
Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

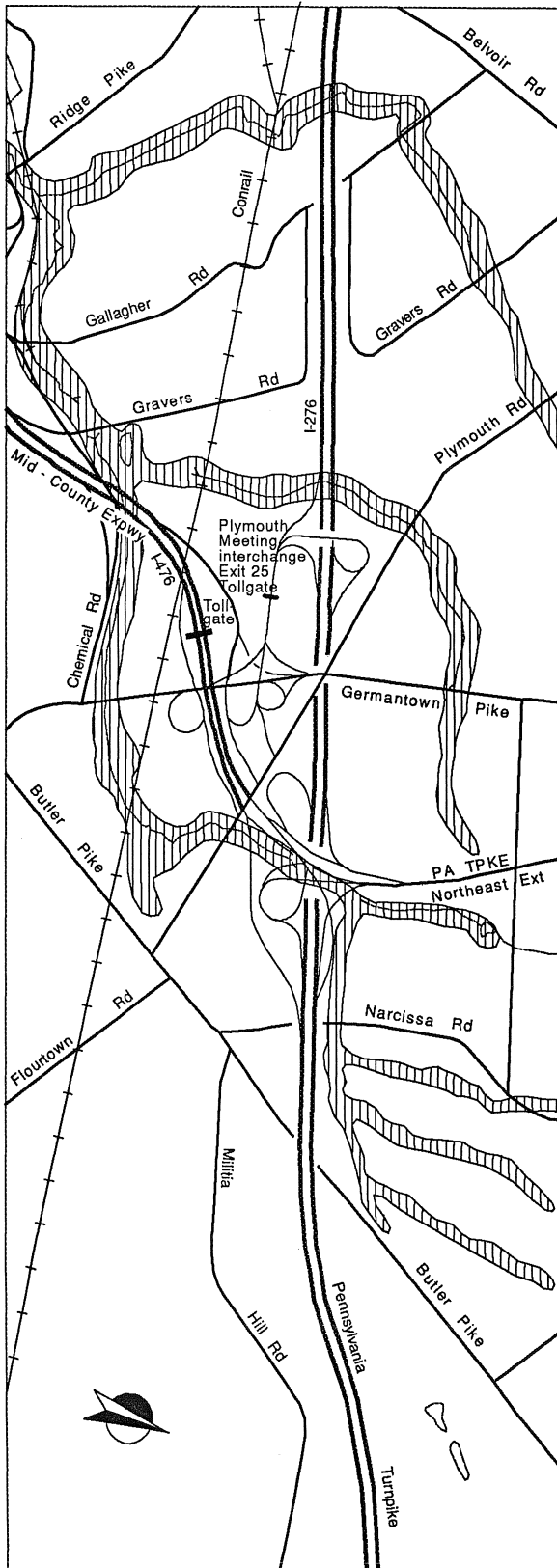
A Coates Dwelling

Coates and Church Streets.
Built circa 1775. Private.





THE PENNSYLVANIA TURNPIKE



Map 11

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain





THE PENNSYLVANIA TURNPIKE

Map 12

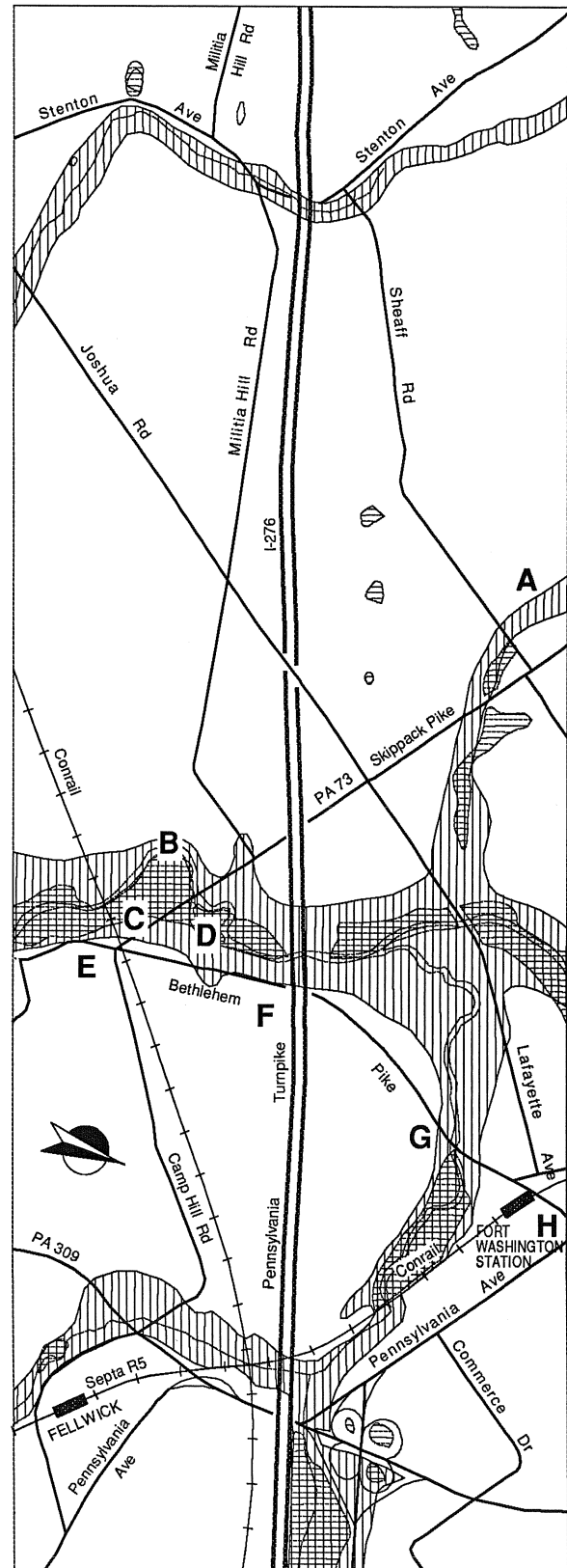
Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

- A Highlands**
Sheaff Road off Skippack Pike. Country manor house built circa 1788. Pennsylvania Historical Museum Commission.
- B Militia Hill**
On Skippack Pike near Bethlehem Pike. Historic site named by Pennsylvania militiamen during 1777 Whitemarsh encampment. Commonwealth of Pennsylvania.
- C Fort Washington State Park and Fort Hill**
Bethlehem and Skippack Pikes. Site of fortification built during 1777 encampment; portions of encampment preserved in park. Pennsylvania Department of Forests and Waters.
- D Farmer's Grist Mill (Mathers Mill)**
On Wissahickon Creek between Bethlehem and Skippack Pikes. Built 1710; supplied flour to Continental army in 1777. Pennsylvania Historical Museum Commission.
- E St. Thomas Episcopal Church**
Bethlehem and Skippack Pikes. Built circa 1690; rebuilt 1710. Church-owned.
- F Hope Lodge**
Bethlehem Pike north of Skippack Pike. Built circa 1723. Private.
- G Samuel Morris School**
Bethlehem Pike. Built 1773; 19th century additions. Private.
- H Clifton House**
473 Bethlehem Pike. Built circa 1770. Historical Society of Fort Washington.





THE PENNSYLVANIA TURNPIKE

Map 12A

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

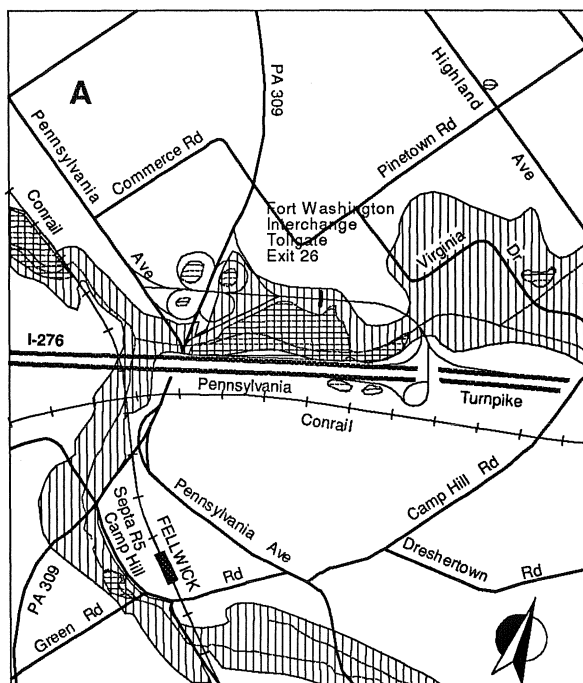
Vertical pattern: 100 - year flood plain

A Emlen House

Pennsylvania Avenue, Camp Hill.

Built 1720;

Washington's headquarters just prior to Valley Forge encampment. Private.





THE PENNSYLVANIA TURNPIKE

Map 13

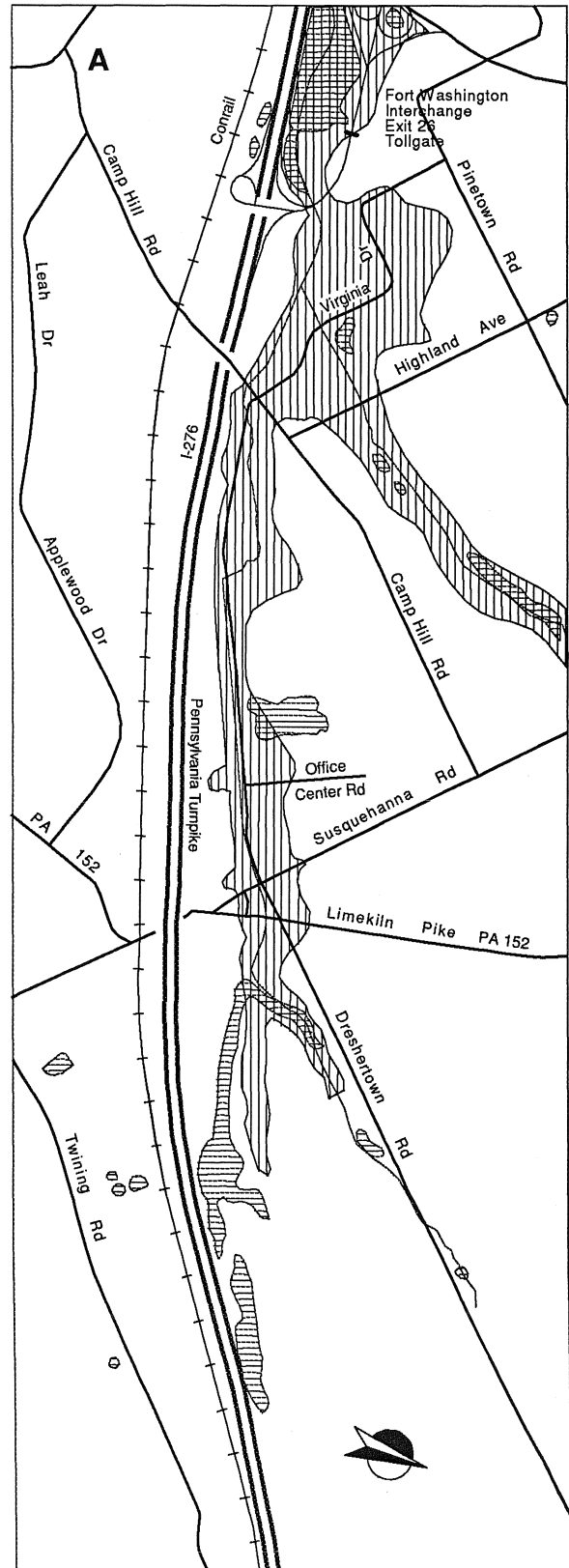
Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

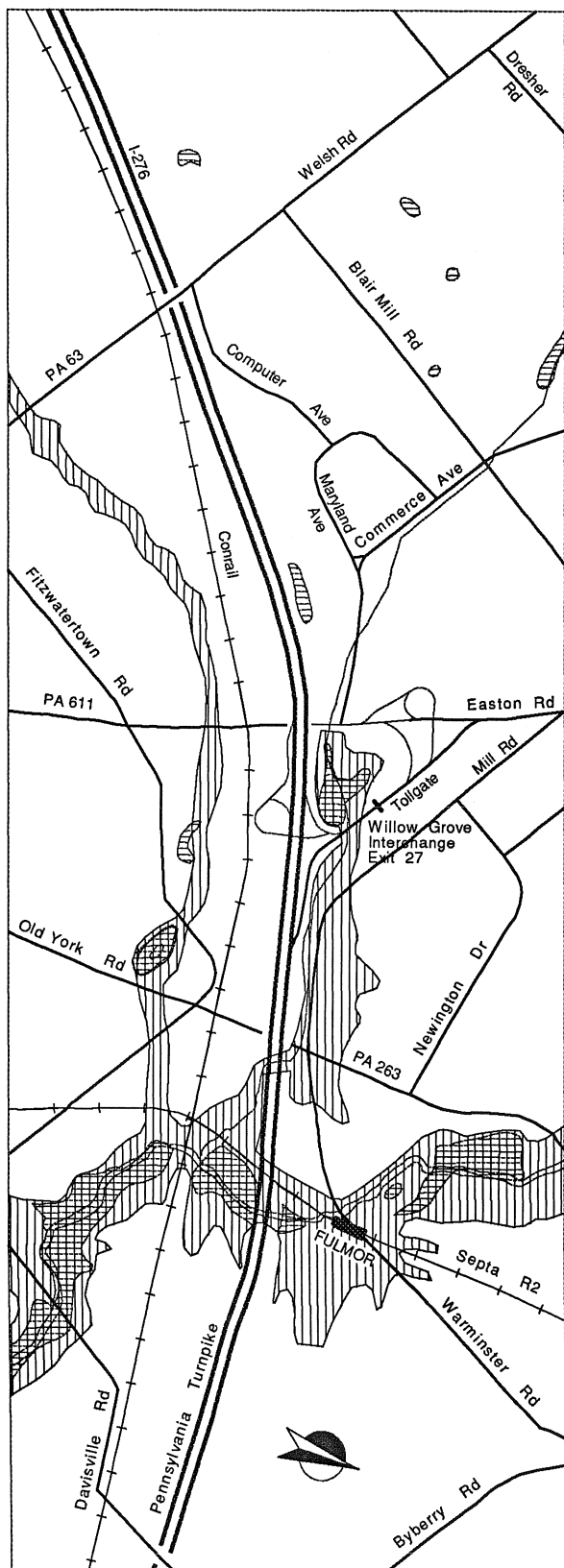
Vertical pattern: 100 - year flood plain

- A Emlen House**
Pennsylvania Avenue, Camp Hill.
Built 1720;
Washington's headquarters just prior to
Valley Forge encampment. Private.





THE PENNSYLVANIA TURNPIKE



Map 14

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain





THE PENNSYLVANIA TURNPIKE

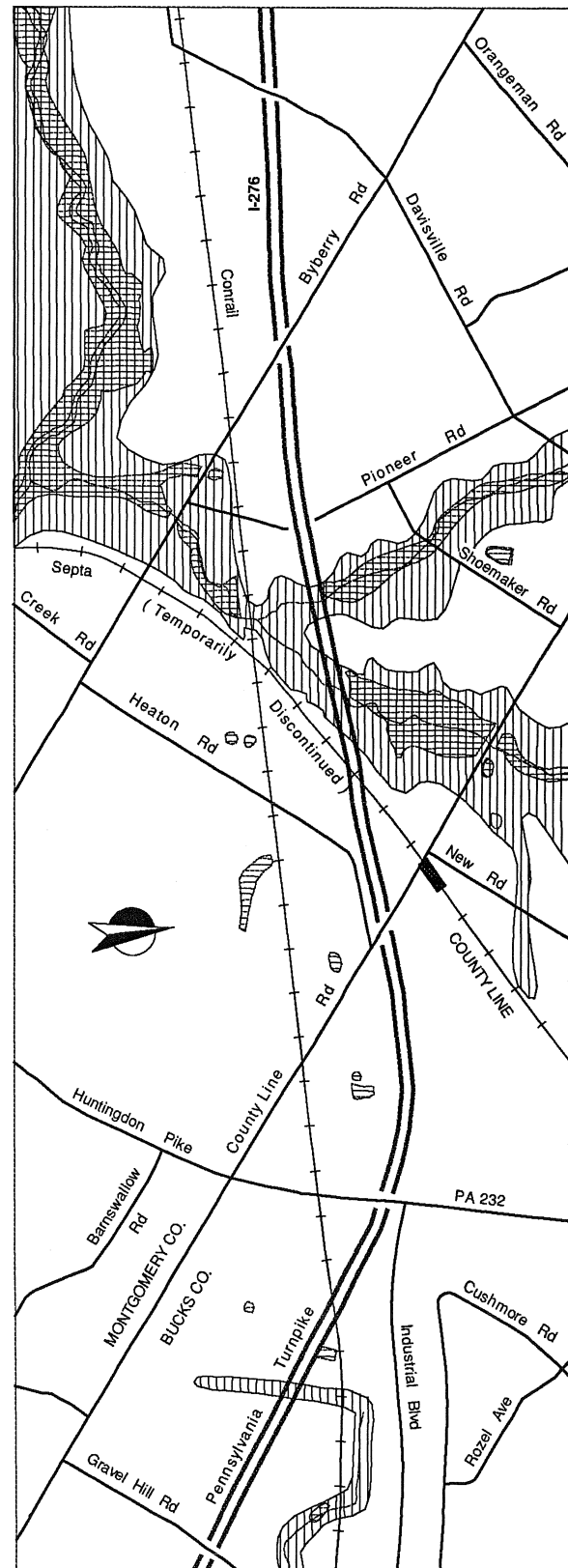
Map 15

Natural and Cultural Environment

Scale: 1" = 2000'

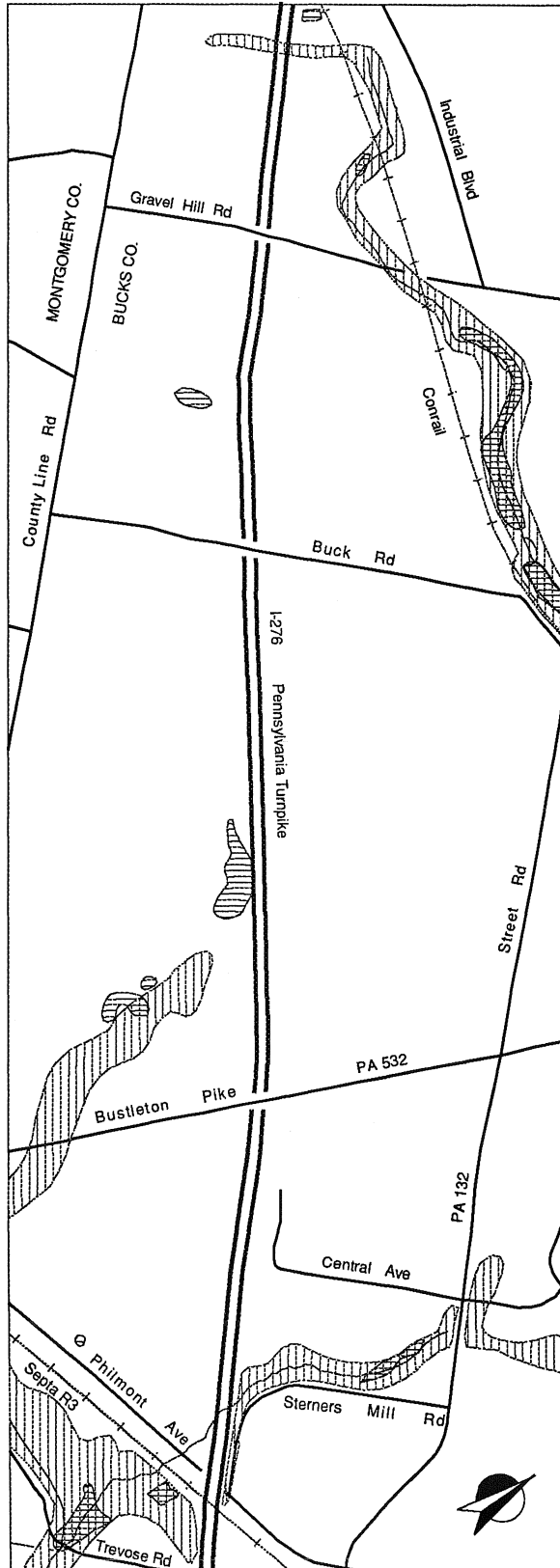
Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain





THE PENNSYLVANIA TURNPIKE



Map 16

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain





THE PENNSYLVANIA TURNPIKE

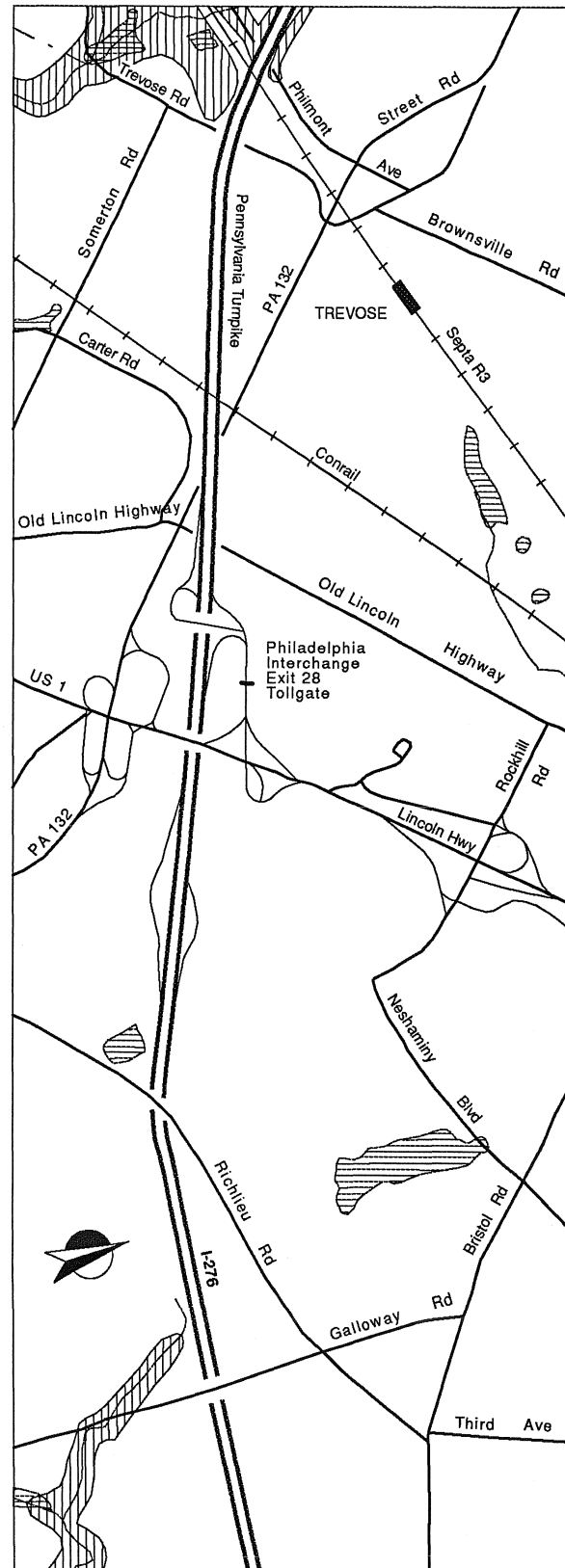
Map 17

Natural and Cultural Environment

Scale: 1" = 2000'

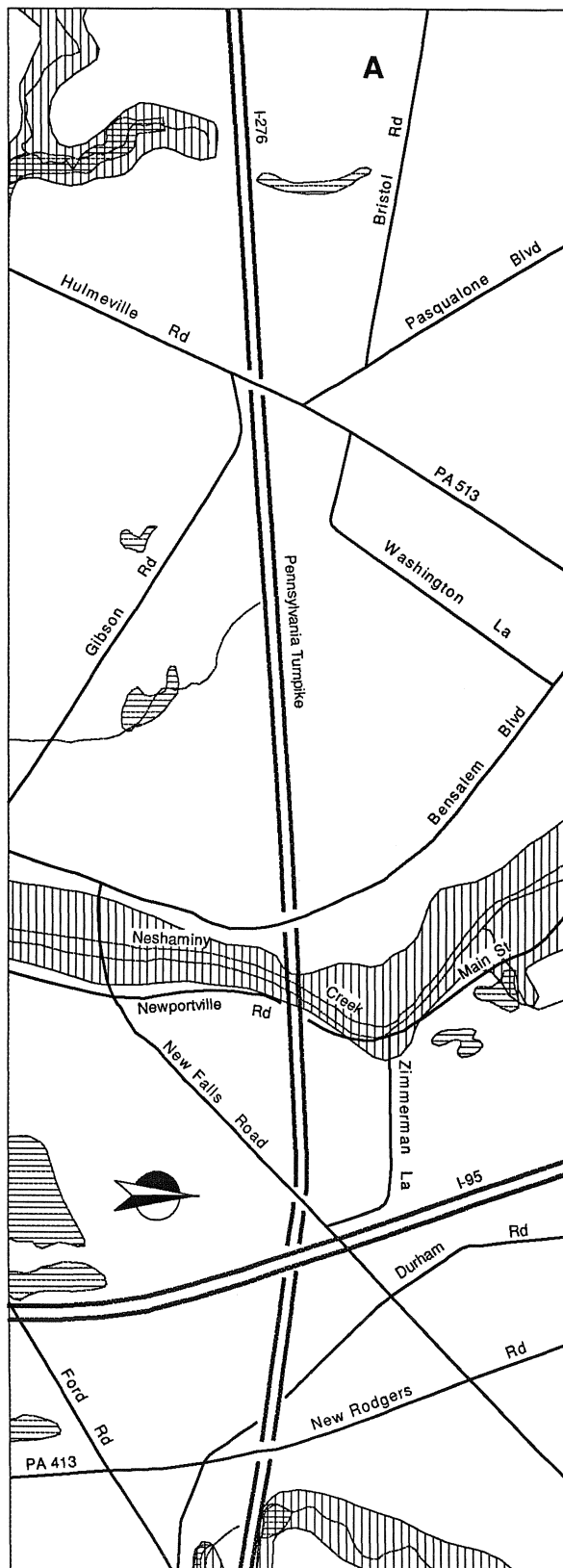
Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain





THE PENNSYLVANIA TURNPIKE



Map 18

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

- A** **Bensalem Presbyterian Church**
Bristol Road between Routes 1 and 513.
Established 1705. Churchowned.





THE PENNSYLVANIA TURNPIKE

Map 19

Natural and Cultural Environment

Scale: 1" = 2000'

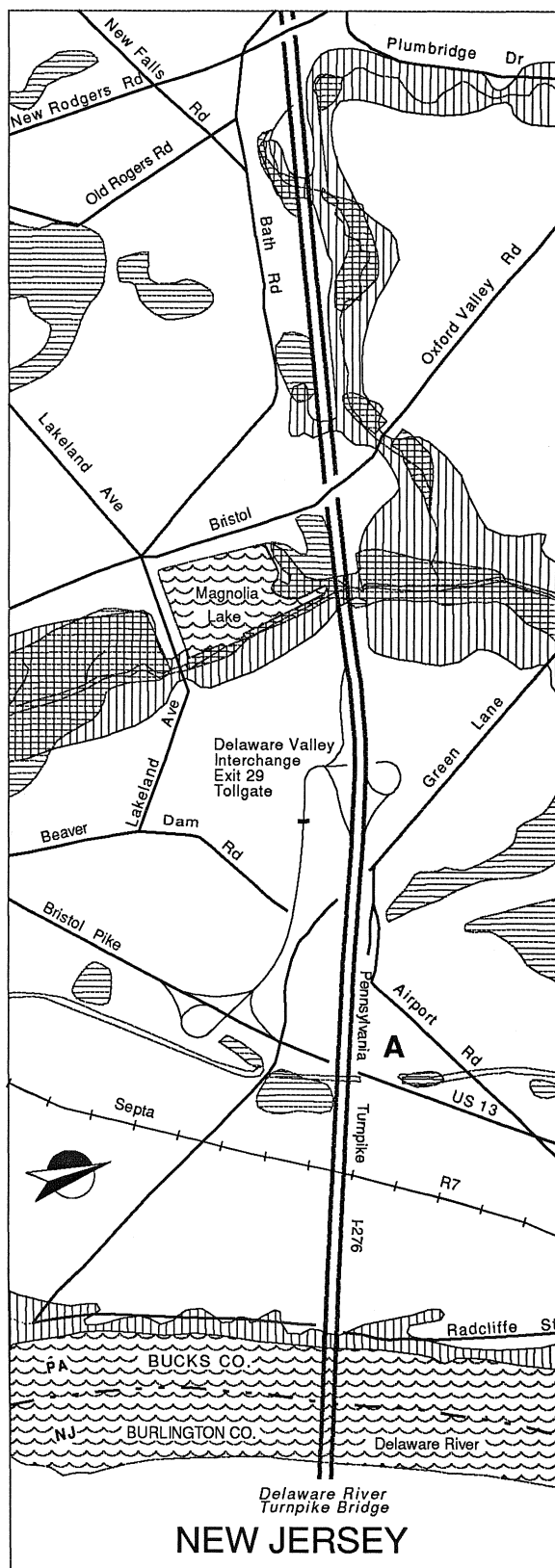
Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

A Pennsylvania Canal

Canal and locks; completed 1837.

Pennsylvania Bureau of State Parks.



Please
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IVORY

Paper

Thank You

TRANSIT AND TRAFFIC DATA

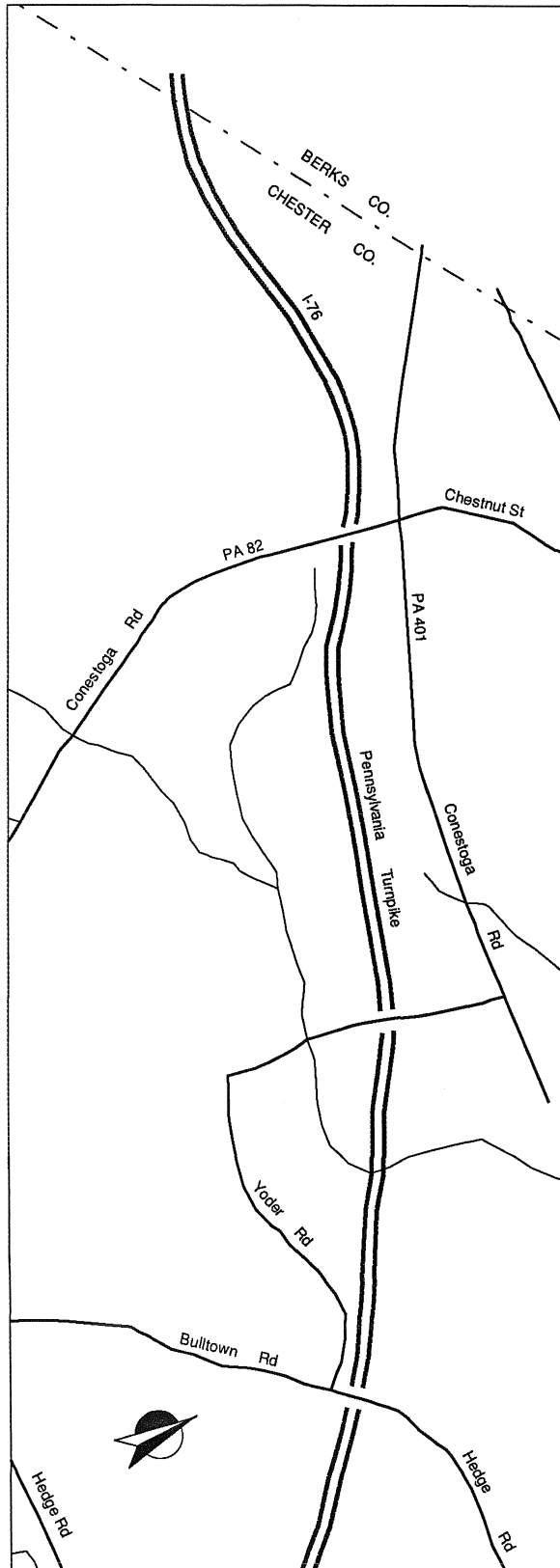
These maps illustrate those transit routes which either cross over or under the Pennsylvania Turnpike. Reference is made in the accompanying text to the route number, the origin and destination points, the road each route travels at the point of intersection, and whether the crossing is over or under the Turnpike. All points of intersection are shown. There are no local transit routes currently using the Turnpike. There are, however, long-distance carriers which operate scheduled service along the Turnpike.

Average Annual Daily Traffic (AADT) counts are shown on the individual maps.

Information for this section was provided by Southeastern Pennsylvania Transportation Authority.



THE PENNSYLVANIA TURNPIKE



Map 1

Transit and Traffic Data

Scale: 1" = 2000'

Note: 1990 AADT between Exits 22 and 23 = 30,059.

No Transit





THE PENNSYLVANIA TURNPIKE

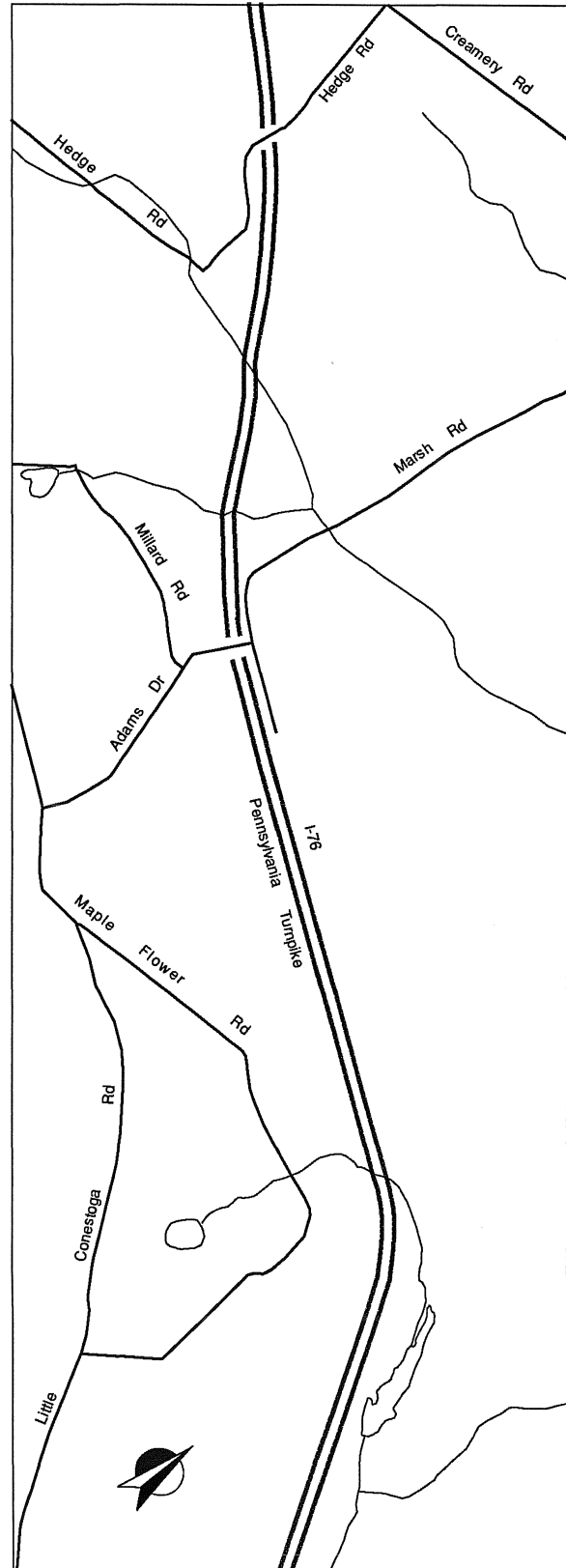
Map 2

Transit and Traffic Data

Scale: 1" = 2000'

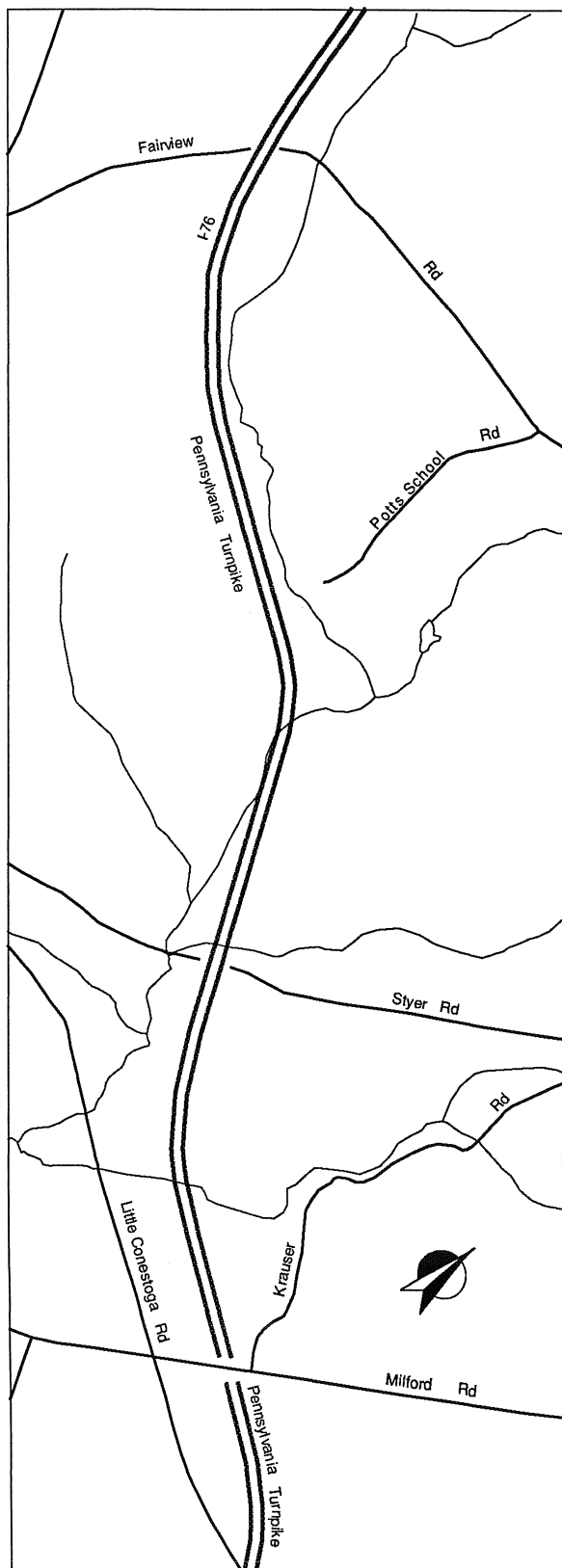
Note: 1990 AADT between Exits 22 and 23 = 30,059.

No Transit





THE PENNSYLVANIA TURNPIKE



Map 3

Transit and Traffic Data

Scale: 1" = 2000'

Note: 1990 AADT between Exits 22 and 23 = 30,059.

No Transit





THE PENNSYLVANIA TURNPIKE

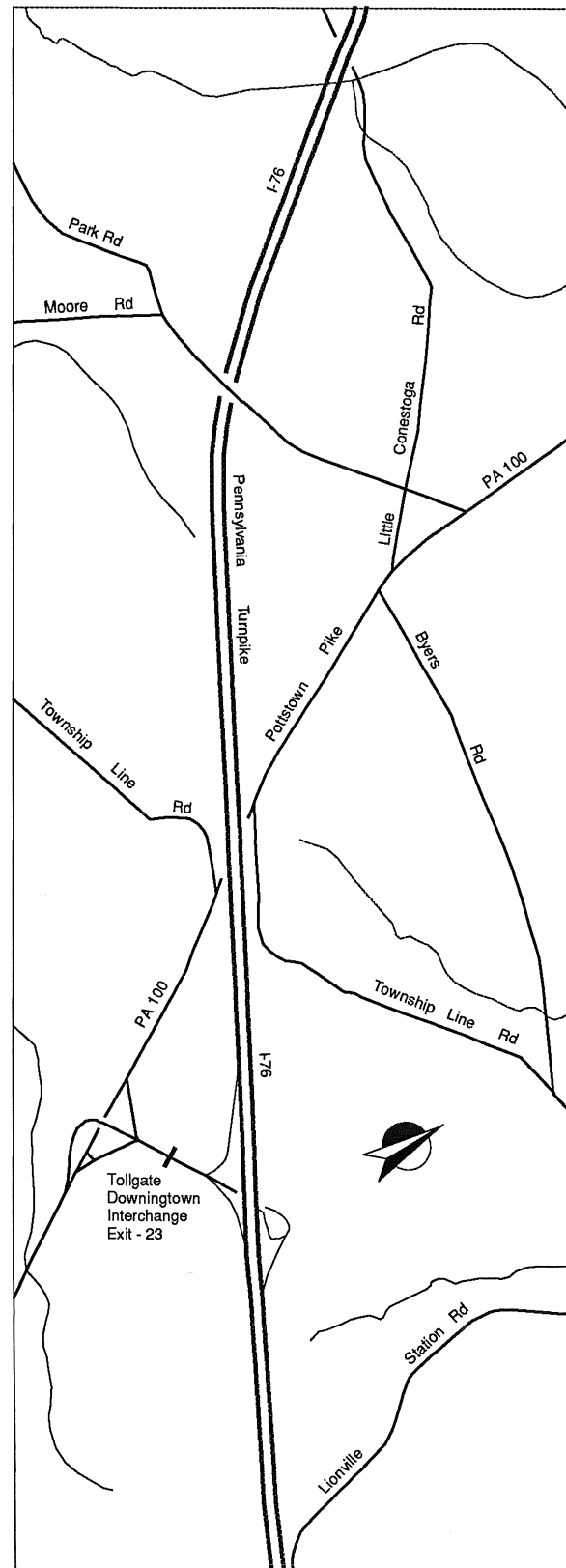
Map 4

Transit and Traffic Data

Scale: 1" = 2000'

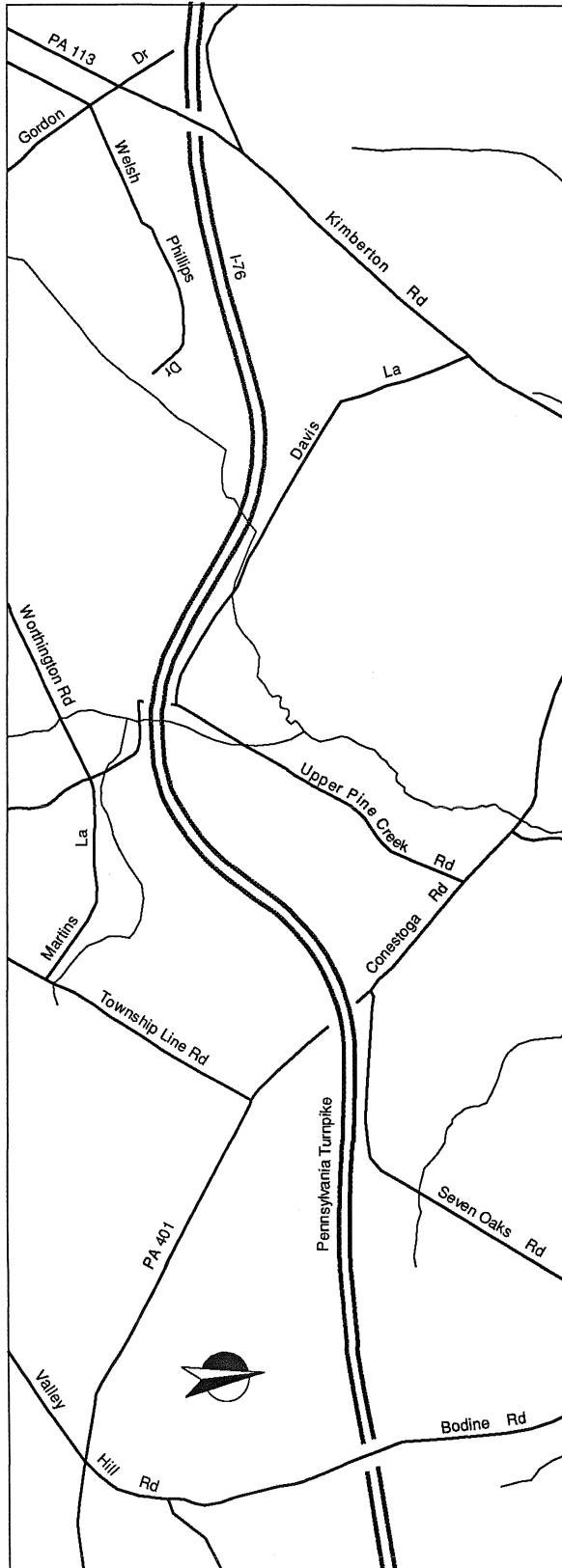
Note: 1990 AADT between Exits 22 and 23 = 30,059.
1990 AADT between Exits 23 and 24 = 31,462.

No Transit





THE PENNSYLVANIA TURNPIKE



Map 5

Transit and Traffic Data

Scale: 1" = 2000'

Note: 1990 AADT between Exits 23 and 24 = 31,462.

No Transit





THE PENNSYLVANIA TURNPIKE

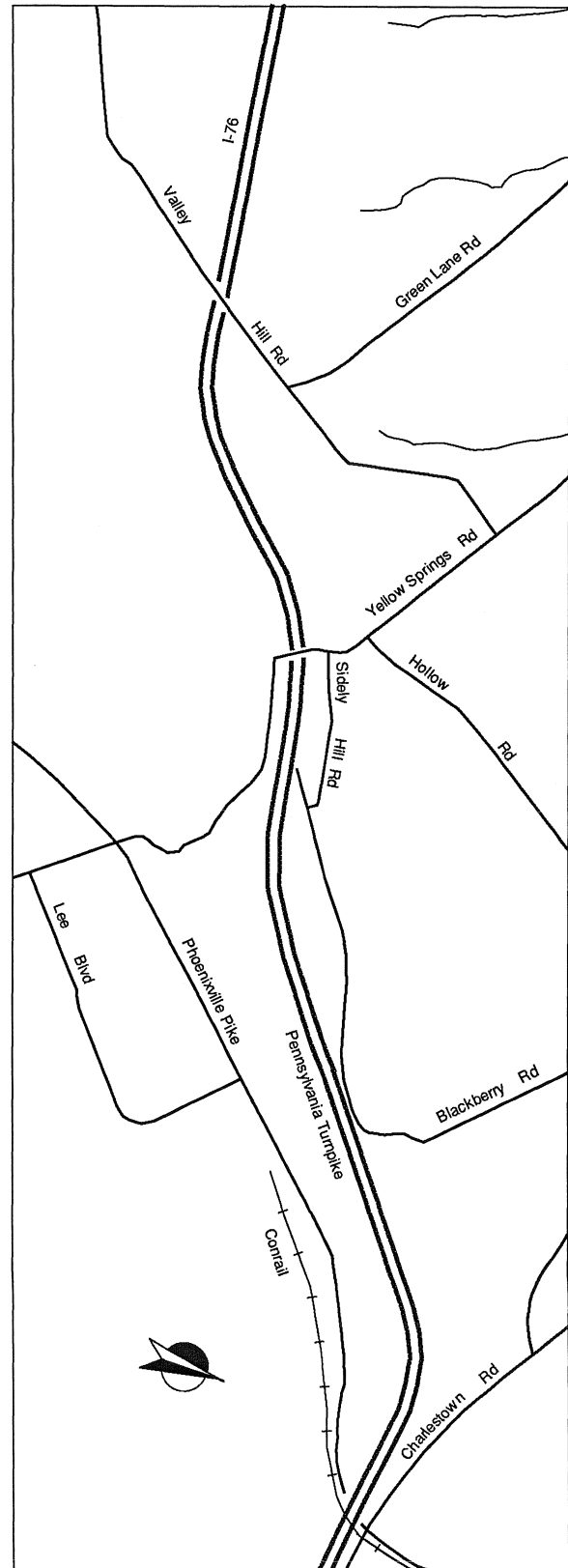
Map 6

Transit and Traffic Data

Scale: 1" = 2000'

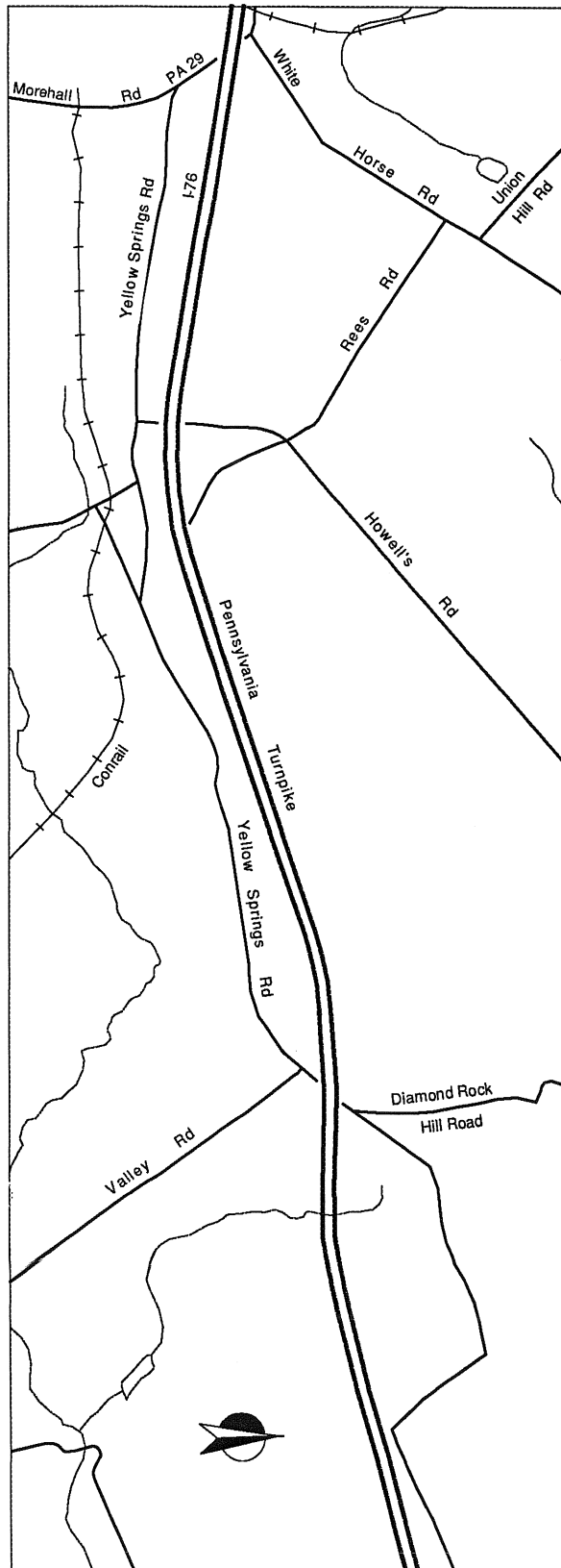
Note: 1990 AADT between Exits 23 and 24 = 31,462.

No Transit





THE PENNSYLVANIA TURNPIKE



Map 7

Transit and Traffic Data

Scale: 1" = 2000'

Note: 1990 AADT between Exits 23 and 24 = 31, 62.

No Transit





THE PENNSYLVANIA TURNPIKE

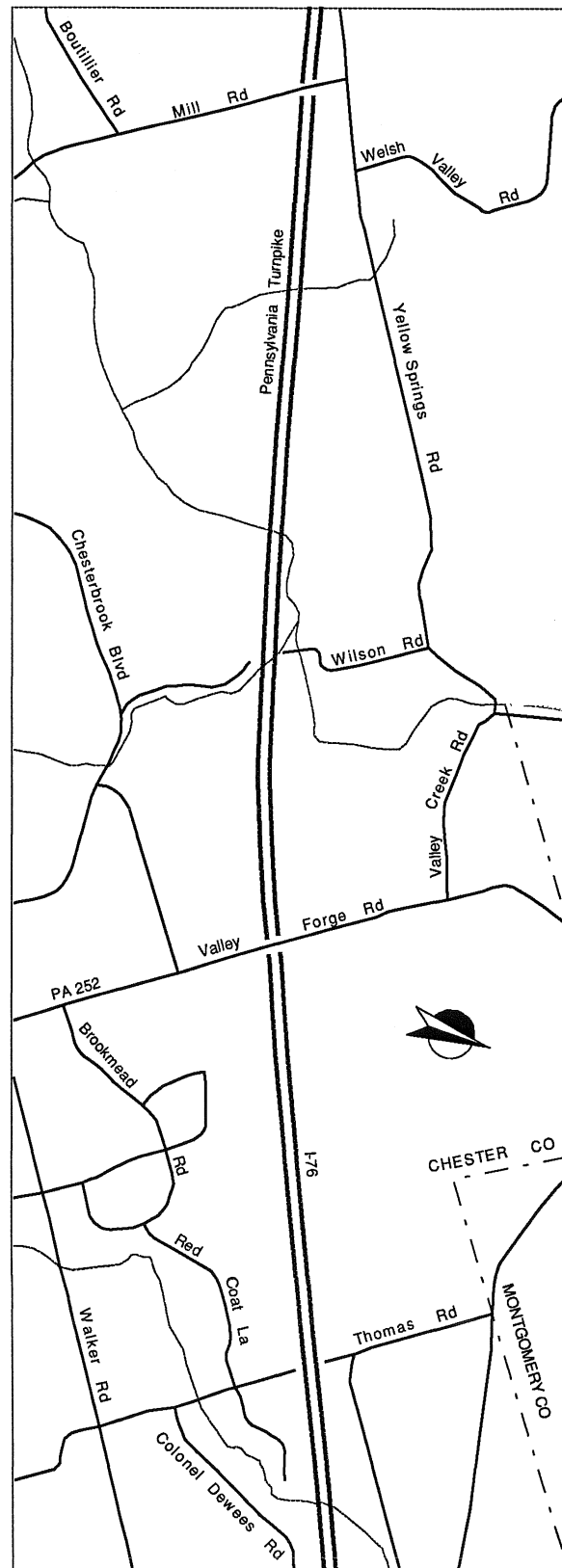
Map 8

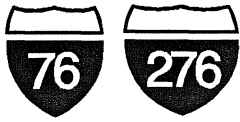
Transit and Traffic Data

Scale: 1" = 2000'

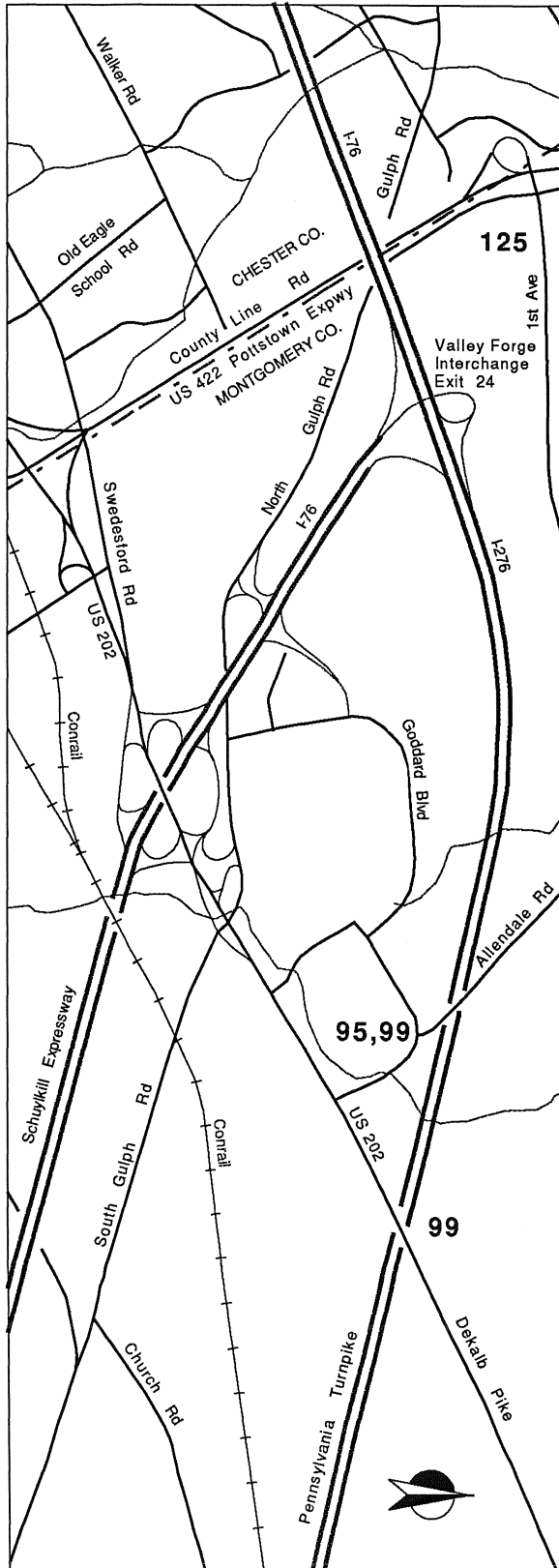
Note: 1990 AADT between Exits 23 and 24 = 31,62.

No Transit





THE PENNSYLVANIA TURNPIKE



Map 9

Transit and Traffic Data

Scale: 1" = 2000'

Note: 1990 AADT between Exits 23 and 24 = 31,462.
1990 AADT between Exits 24 and 25 = 57,361.

- 125** Center City to Chesterbrook
Crosses under I-76 at North Gulph Road
- 95** Plymouth Meeting Mall to King of Prussia
Crosses over I-276 at Allendale Road
- 99** Norristown to Royersford
Crosses over I-276 at Allendale Road and US 202





THE PENNSYLVANIA TURNPIKE

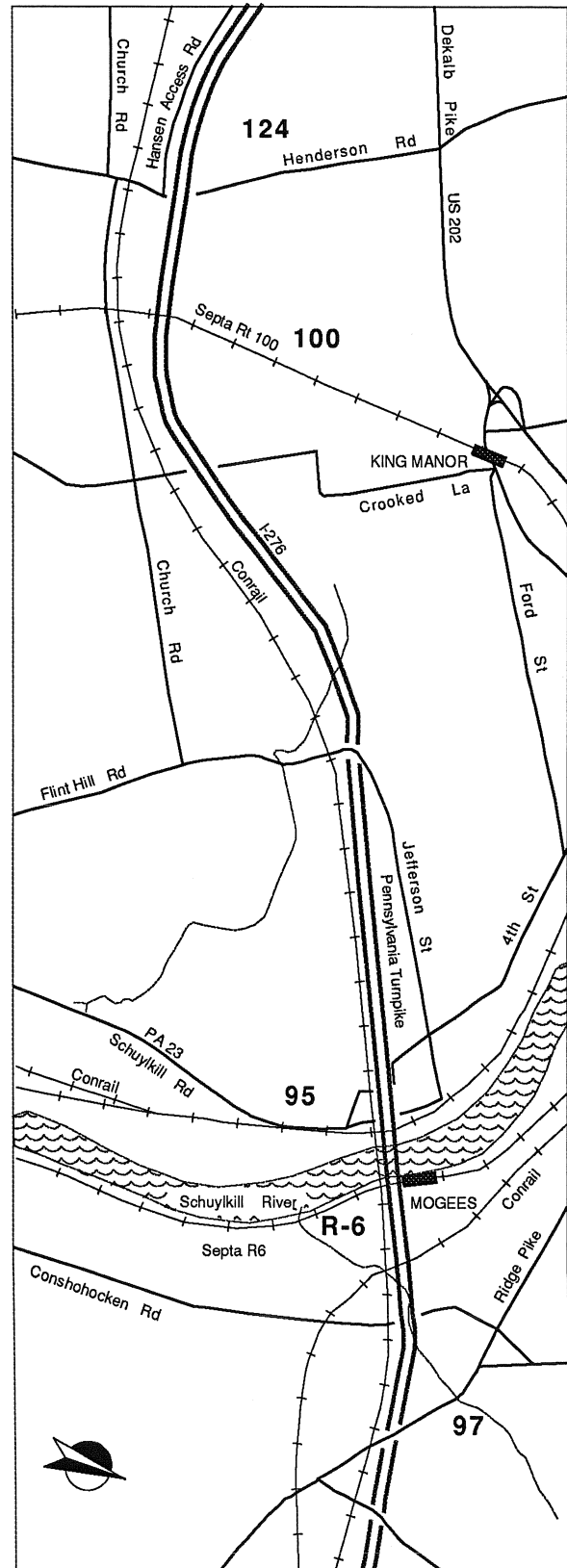
Map 10

Transit and Traffic Data

Scale: 1" = 2000'

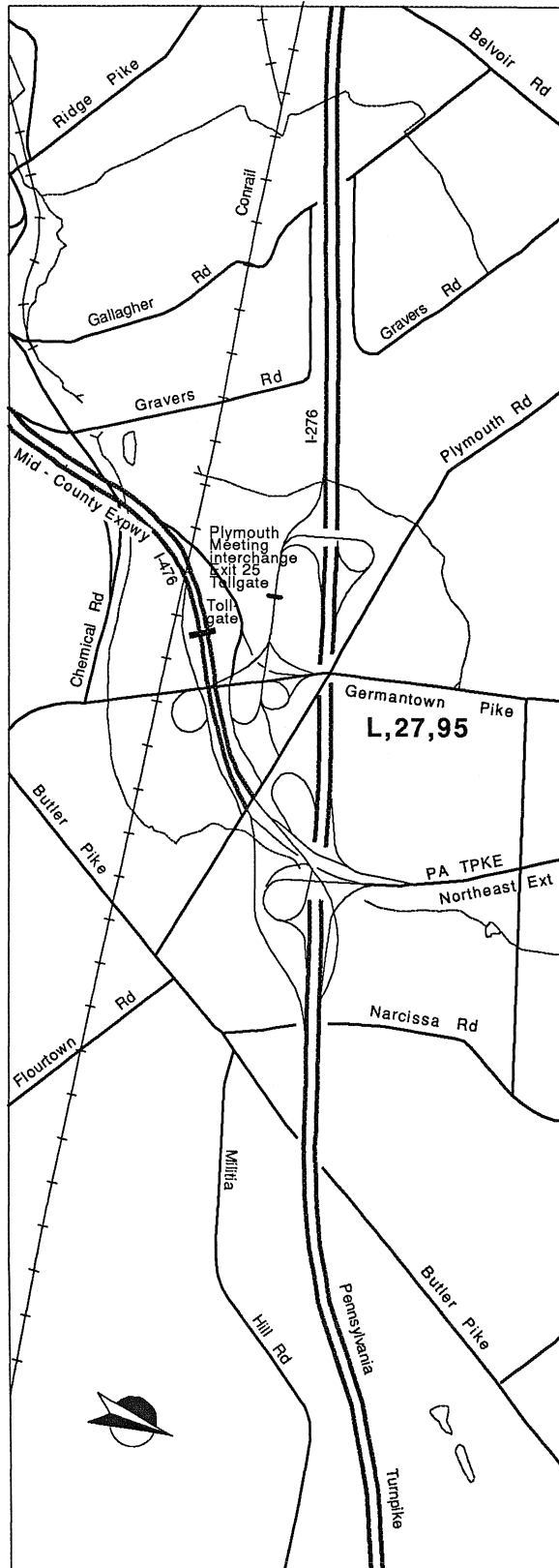
Note: 1990 AADT between Exits 24 and 25 = 57,361.

- 124** King of Prussia to Center City
Crosses under I-276 at Henderson Road
- 100** Light Rail from 69th Street to Norristown
Crosses under I-276 east of Henderson Road
- 95** Plymouth Meeting Mall to King of Prussia
Crosses under I-276 at River Road PA 23
- R-6** Regional Rail to Norristown
Crosses I-276 west of the Schuylkill River
- 97** Penn Square to Spring Mill
Crosses I-276 at Ridge Pike





THE PENNSYLVANIA TURNPIKE



Map 11

Transit and Traffic Data

Scale: 1" = 2000'

Note: 1990 AADT between Exits 24 and 25 = 57,361.
1990 AADT between Exits 25 and 33 = 77,732.
1990 AADT between Exits 33 and 26 = 68,286.

- L** Olney to Plymouth Meeting Mall
Crosses over I-276 at Germantown Pike
- 27** Center City to Plymouth Meeting Mall
Crosses over I-276 at Germantown Pike
- 95** Plymouth Meeting Mall to King of Prussia
Crosses over I-276 at Germantown Pike





THE PENNSYLVANIA TURNPIKE

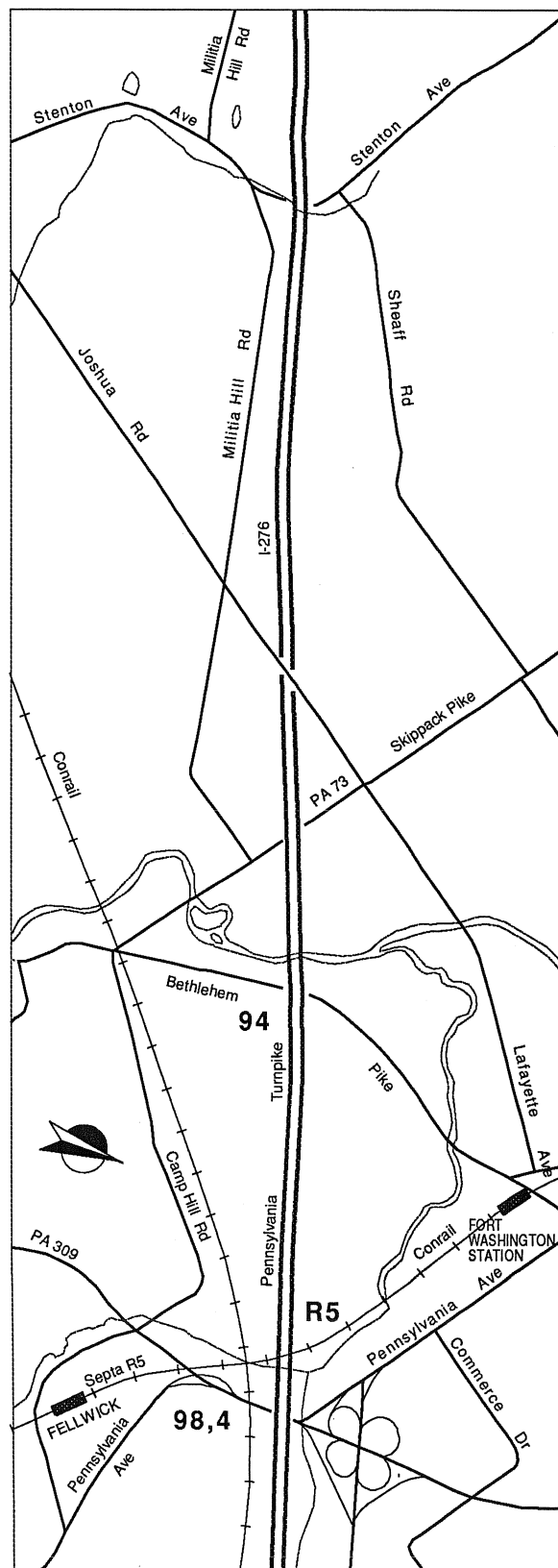
Map 12

Transit and Traffic Data

Scale: 1" = 2000'

Note: 1990 AADT between Exits 33 and 26 = 68,286.

- 94** Chestnut Hill to Montgomery Mall
Crosses under I-276 at Bethlehem Pike
- R-5** Regional Rail to Doylestown
Crosses under I-276 east of PA 309
- 98** Oaks to Willow Grove
Crosses under I-276 at PA 309
- 4** Olney to Fort Washington
Crosses under I-276 at PA 309



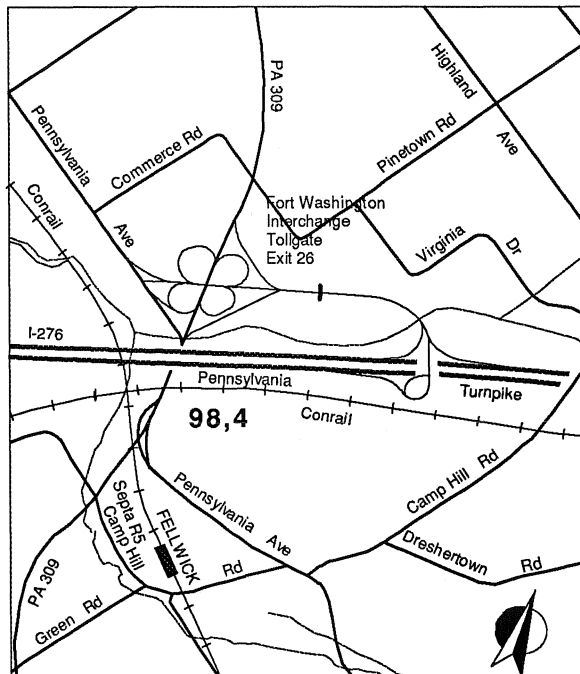


THE PENNSYLVANIA TURNPIKE

Map 12A

Transit and Traffic Data

Scale: 1" = 2000'



Note: 1990 AADT between Exits 33 and 26 = 68,286.
1990 AADT between Exits 26 and 27 = 69,522.

- 98** Oaks to Willow Grove
Crosses under I-276 at PA 309
- 4** Olney to Fort Washington
Crosses under I-276 at PA 309





THE PENNSYLVANIA TURNPIKE

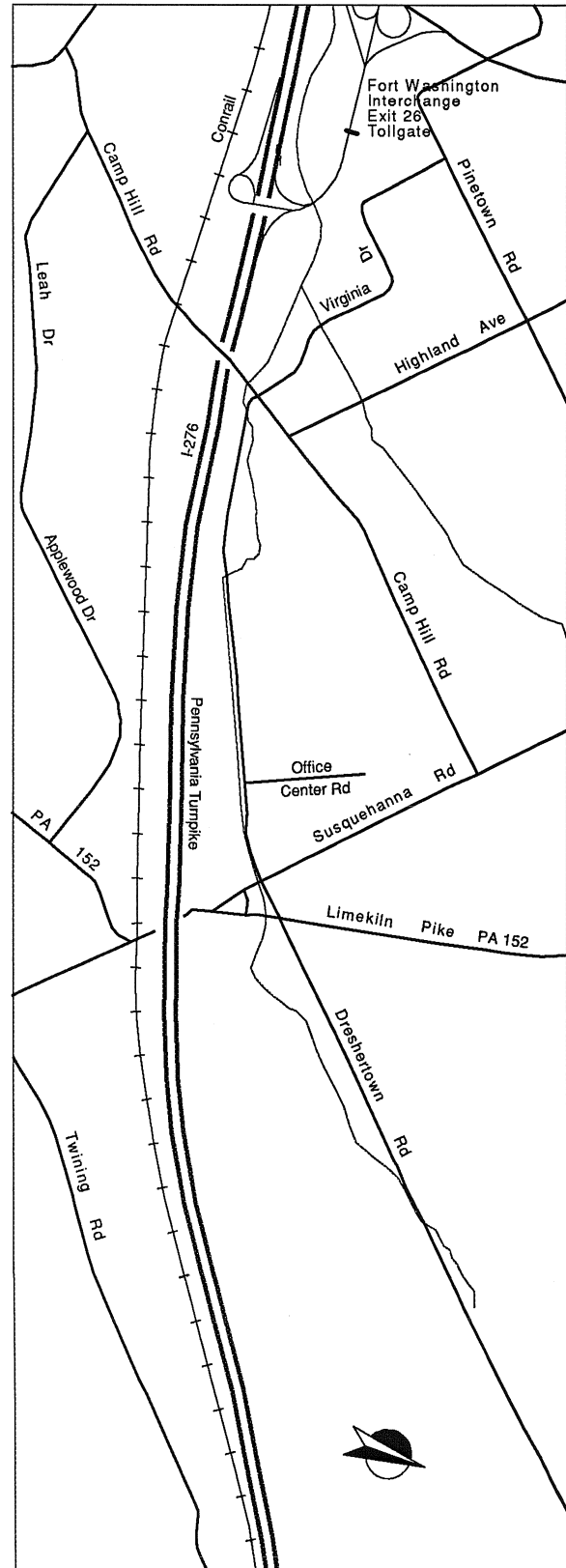
Map 13

Transit and Traffic Data

Scale: 1" = 2000'

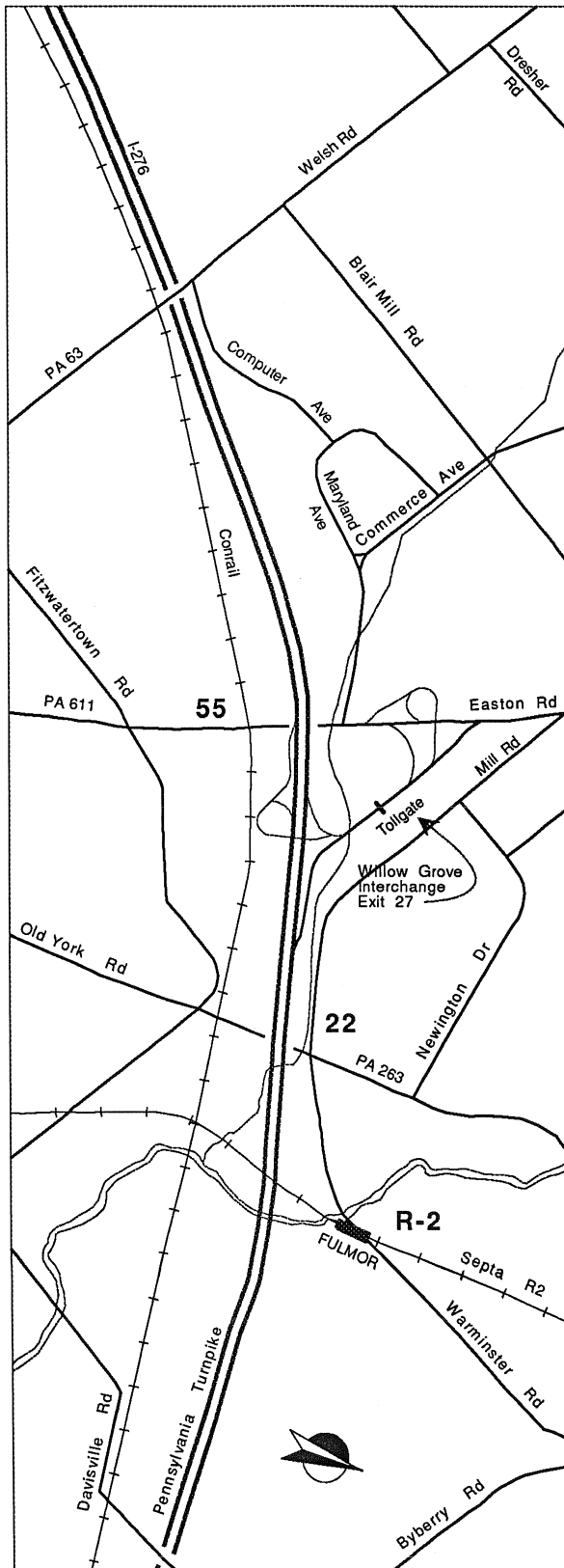
Note: 1990 AADT between Exits 26 and 27 = 69,522.

No Transit





THE PENNSYLVANIA TURNPIKE



Map 14

Transit and Traffic Data

Scale: 1" = 2000'

Note: 1990 AADT between Exits 26 and 27 = 69,522.
1990 AADT between Exits 27 and 28 = 69,522.

55 Olney to Doylestown
Crosses under I-276 at Easton Road PA 611

22 Olney to Warminster
Crosses under I-276 at Old York Road PA 263

R-2 Regional Rail to Warminster
Crosses under I-276 west of PA 263





THE PENNSYLVANIA TURNPIKE

Map 15

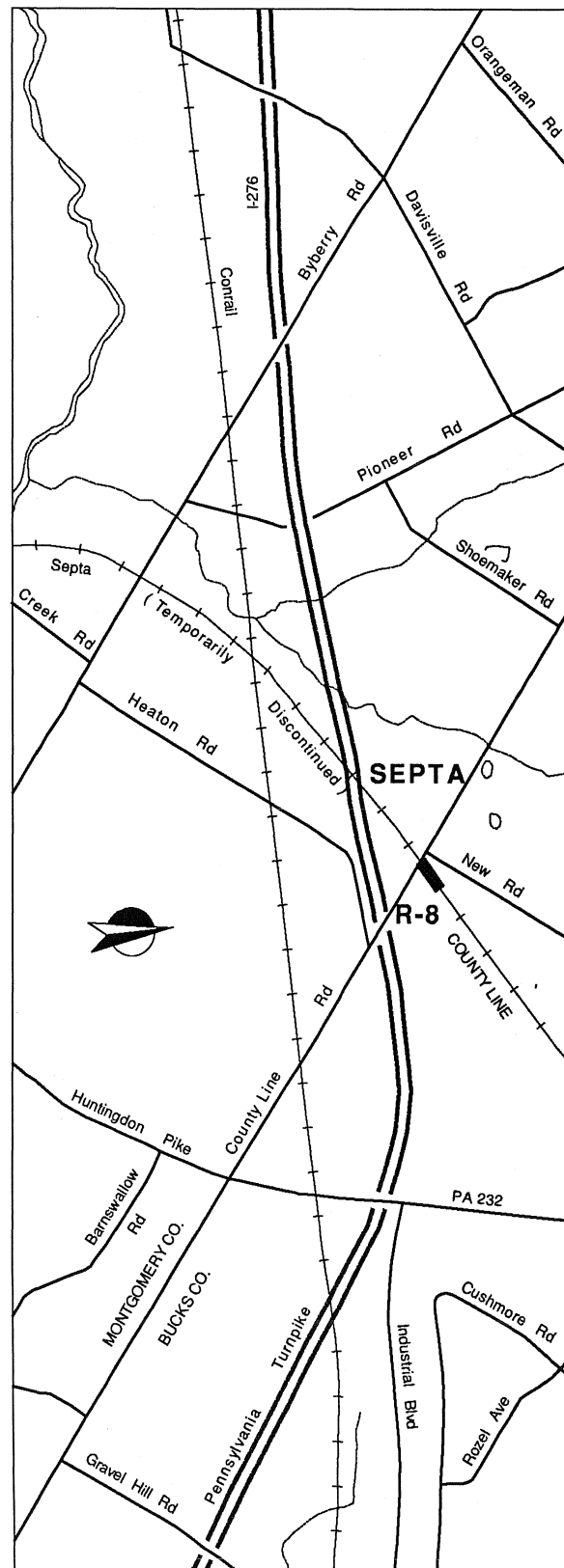
Transit and Traffic Data

Scale: 1" = 2000'

Note: 1990 AADT between exits 27 and 28 = 69,522.

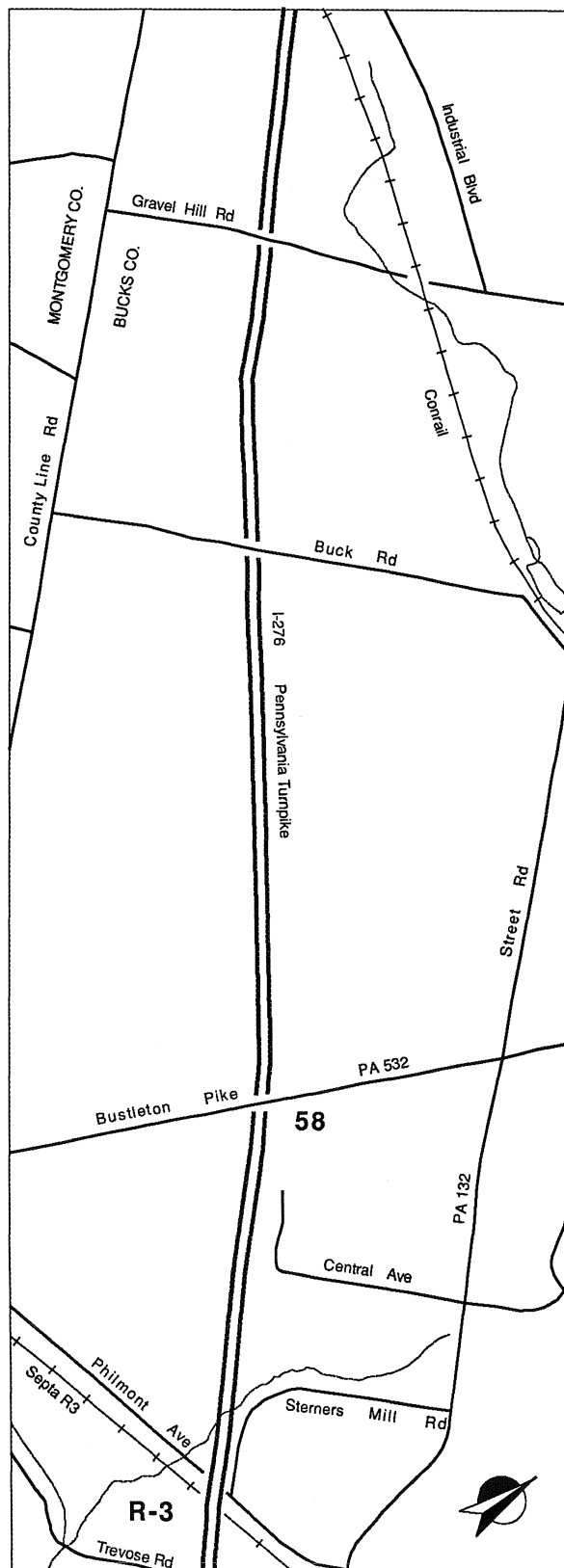
SEPTA Discontinued Regional Rail
Crosses under I-276 east of County Line Road

R-8 Bus from Fox Chase to Newtown
Crosses over I-276 at County Line Road





THE PENNSYLVANIA TURNPIKE



Map 16

Transit and Traffic Data

Scale: 1" = 2000'

Note: 1990 AADT between exits 27 and 28 = 69,522.

58 Frankford to Neshaminy Mall
Crosses over I-276 at Bustleton Pike

R-3 Regional Rail to West Trenton
Crosses under I-276 east of Trevose Road





THE PENNSYLVANIA TURNPIKE

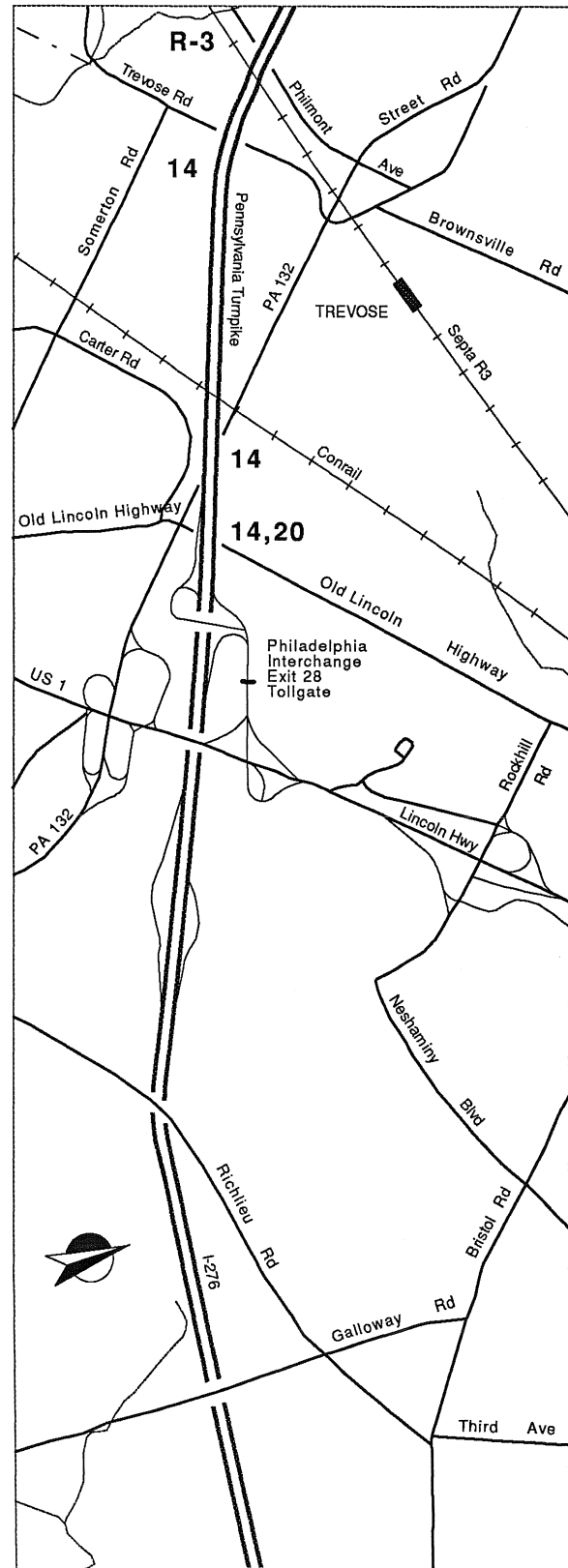
Map 17

Transit and Traffic Data

Scale: 1" = 2000'

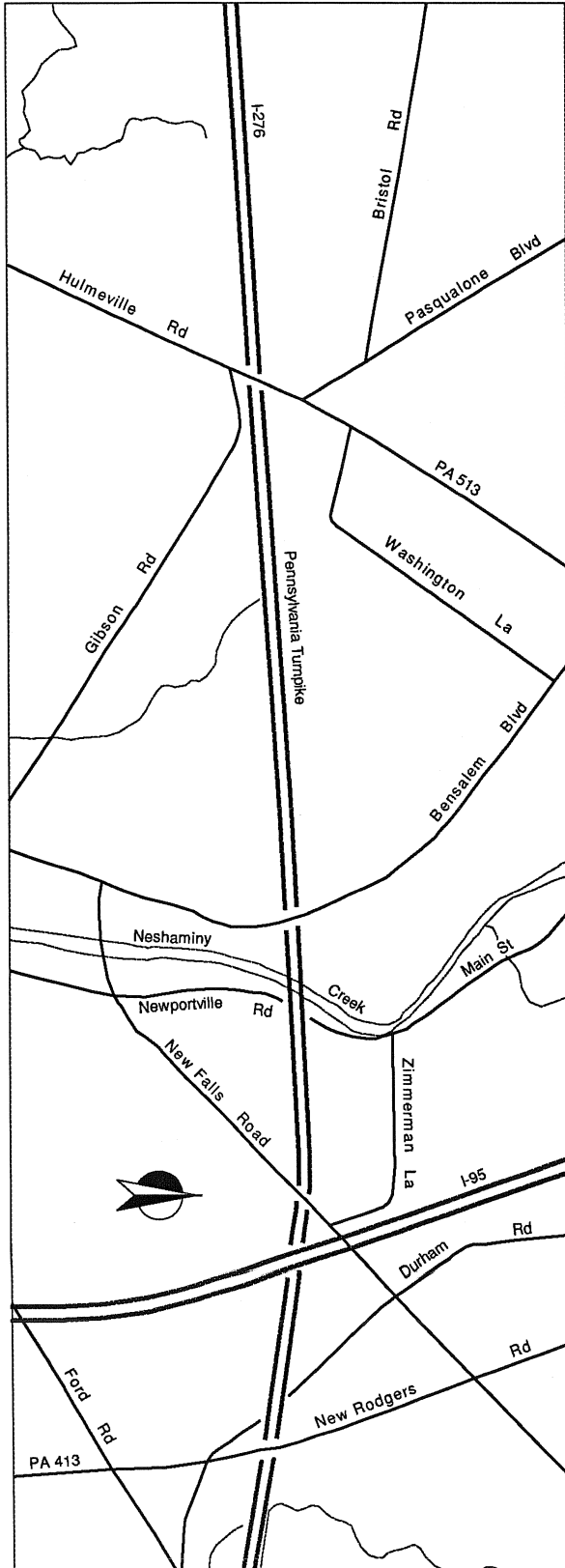
Note: 1990 AADT between exits 27 and 28 = 69,522.
1990 AADT between exits 28 and 29 = 34,556.

- R-3** Regional Rail to West Trenton
Crosses under I-276 east of Trevose Road
- 14** Frankford to Oxford Valley Mall
crosses under I-275 at Trevose Road and Street Road PA 132 and Old Lincoln Highway
- 20** Frankford to Neshaminy Mall
Crosses under I-276 at Old Lincoln Highway





THE PENNSYLVANIA TURNPIKE



Map 18

Transit and Traffic Data

Scale: 1" = 2000'

Note: 1990 AADT between Exits 28 and 29 = 34,556.

No Transit





THE PENNSYLVANIA TURNPIKE

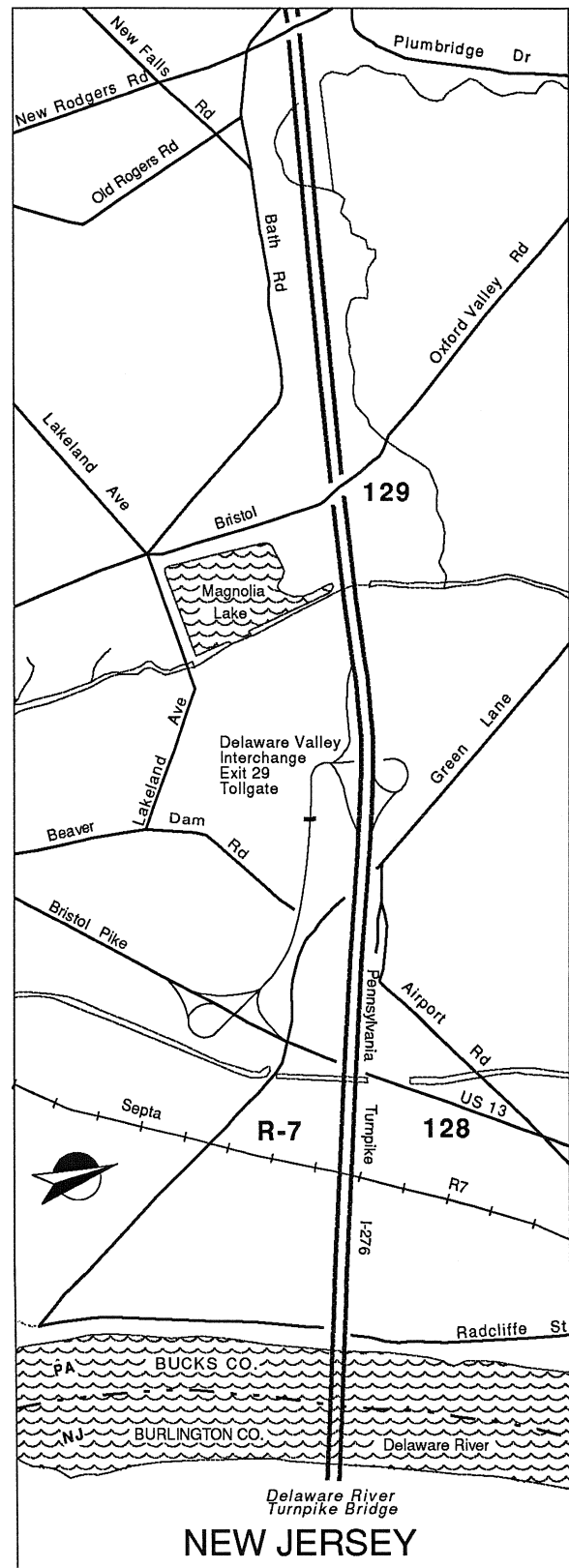
Map 19

Transit and Traffic Data

Scale: 1" = 2000'

Note: 1990 AADT between Exits 28 and 29 = 34,556.
1990 AADT between Exit 29 and Turnpike Bridge toll station = 33,146.

- 128** Bristol to Oxford Valley Mall
Crosses under I-276 at Bristol Pike US 13
- 129** Torresdale to Oxford Valley Mall
Crosses over I-276 at Bristol-Oxford Valley Road
- R-7** Regional Rail to Trenton
Crosses under I-276 west of Toll Plaza



SECTION FOUR

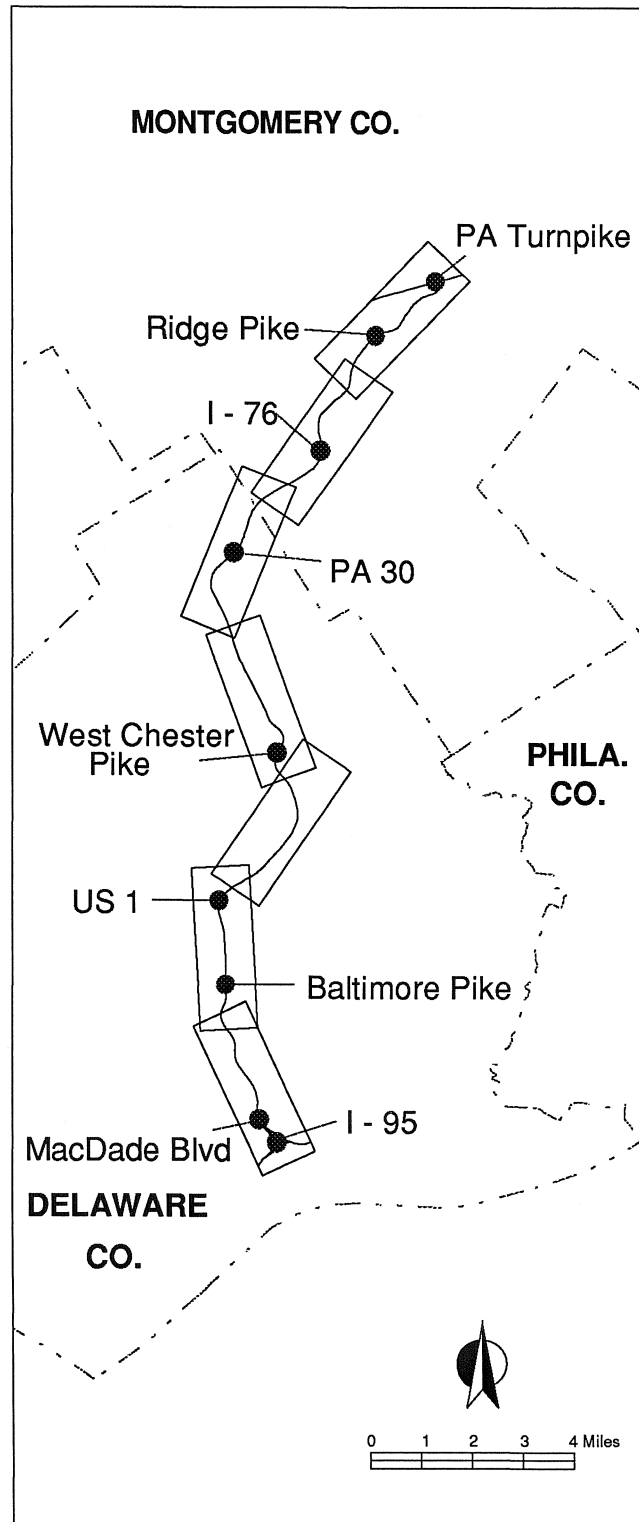


THE MID-COUNTY EXPRESSWAY



THE MID-COUNTY EXPRESSWAY

Figure 5



DESIGN FEATURES

This series of maps inventories the current physical attributes, including physical characteristics and structural information, of the Mid-County Expressway. It should be noted that the part of this roadway south of I-76 (The Schuylkill Expressway) was completed and opened to traffic in December of 1991. The roadway itself is two lanes each direction south of PA Route 3 (West Chester Pike), and three lanes by direction north of PA Route 3. Travel lanes are 12' in width, with median strip and wide shoulders the entire length of the road. Because of its recent opening, it is surrounded for most of its length by sound barrier walls.

Segment/offset markers on this highway run south to north, and are shown on these maps accordingly. General information about the section of road shown in each map is located underneath the title block. Segment/offset numbers posted on the maps correspond to locations of structures which are described beside each map.

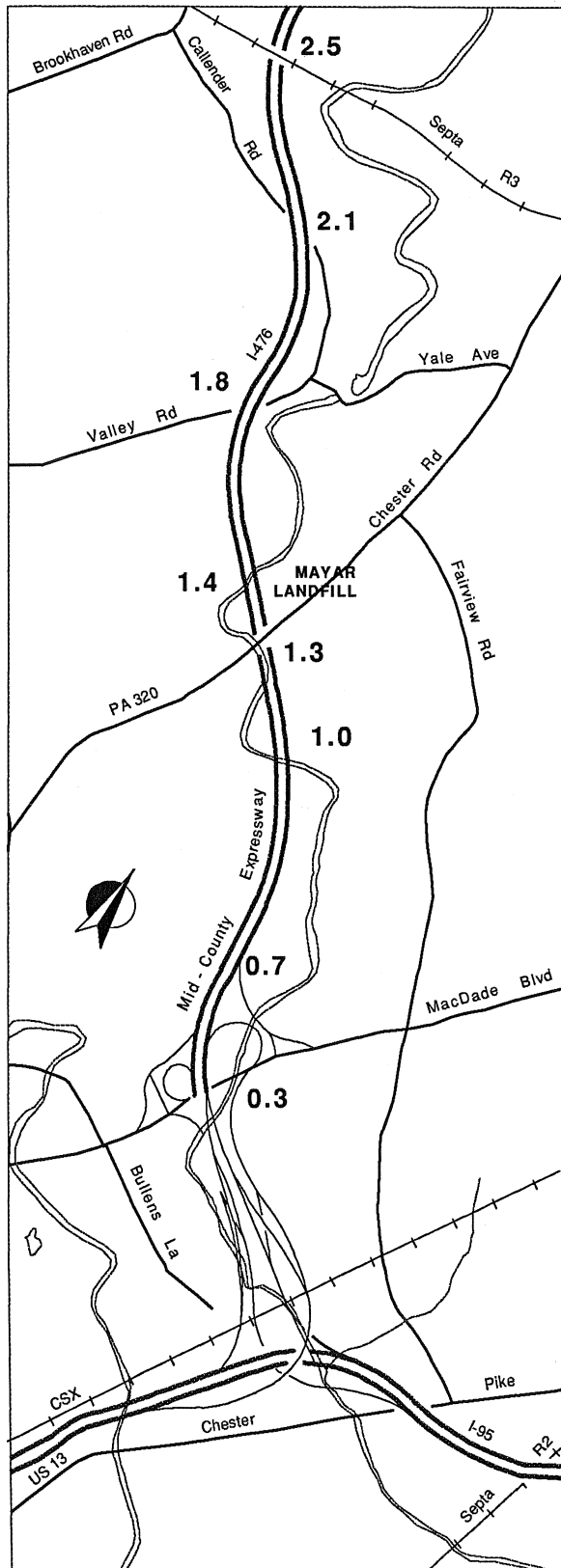
Specific information regarding structures is provided as necessary. Citations are made for either bridges or overpasses. For bridges, structure number, span length, and deck width are given, as well as information regarding the underneath facility. For structures overpassing the Mid-County Expressway, the distance between piers is given in exact measurements (where available) or is listed according the location of the piers. Structure number and description of facility carried are also given. Due to the interstate status of this highway, there are no sub-standard weight or clearance structures.

Information on these maps was gleaned from a combination of construction drawings, PennDOT Bridge Unit's structure lists, and field observations.





THE MID-COUNTY EXPRESSWAY



Map 1

Design Features

Scale: 1" = 2000'

Note: Two 12' travel lanes by direction; 12' shoulders.

0.3 Dual Bridge

Over MacDade Boulevard
S#: 10142, 10142B
Span Length: NBD 237'
SBD 237'
Deck Width: NBD 90.8'
SBD 89.2'

1.4 Dual Bridge

Over Mayar Landfill
(at ground level)
S#: 16595
Span Length: NB - 474',
SB - 474'

0.7 Overpass

Bullens Lane
SR 2008 LR 23035
(not shown)
S#: 16592
Distance Between Piers:
84'-3", 105'-8"

1.4 Dual Bridge

Over Crum Creek /
Avondale Road
SR 3003, LR 23013
S#: 16596
Span Length: 330'-0"
Deck Width: NB - 47'-6",
SB 46'-6"
Between 2'-6"

1.0 Dual Bridge

Over Crum Creek
S#: 16593
Span Length: 528'-0"
Deck Width: NB - 46'-7"
SB - 46'-7"
Between - 2'-6"

1.8 Dual Bridge

Over Rose Valley Road
SR 3019, LR 23070
S#: 16597
Span Length: NB - 135'-0"
SB - 119'-1"
Deck Width: NB - 46'-6"
SB - 53'-6"

1.3 Overpass

Chester Road PA 320
SR 0320 LR 225
S#: 16584
Distance Between Piers:
179', 138', 188', 178'-6"

2.1 Bridge

Over Avondale Road
SR 3019 LR 23070
Dicks Run S#:16598
Span Length: 700'-0"
Deck Width: NB - 53'-6"
SB - 47'-6"
Between - 2'-6"

2.5 Overpass

SEPTA R-3
S#:16599
Distance Between Piers:
136'-0"





THE MID-COUNTY EXPRESSWAY

Map 2

Design Features

Scale: 1" = 2000'

Note: Between 2.4 to 3.55, three NBD and two SBD travel lanes. North of 3.55, two travel lanes by direction. All lane widths 12'; shoulder widths 10' to 14' (variable).

2.5 Overpass

SEPTA R-3

S#:16599

Distance Between Piers:
136'-0"

2.7 Overpass

Rogers Lane

SR 2012 LR 23069

S#:16600

Distance Between Piers:
115', 182'-6", 195'

3.0 Overpass

Plush Mill Road

S#:16601

Distance Between Piers:
104', 122'

3.2 Dual Bridge

Over Baltimore Pike

SR 2016 LR 130

S#:16602

Span Length: 140'-0"
Deck Width: NB - 46'-6"
SB - 46'-6"
Between - 2'-6"

3.5 Bridge

Over Crum Creek

(Smedley Park Entrance)

S#: 16604

Span Length: 140'
Deck Width: 44'-6"

3.6 Dual Bridge

Over Crum Creek /
Paper Mill Road

S#:16529

Span Length: 1120'-0"
Deck Width: NB - 57'-9"
SB - 57'-9"

3.9 Dual Bridge

Over Crum Creek

S#:16530

Span Length: 540'-0"
Deck Width: NB - 57'-9"
SB - 57'-9"

4.3 Overpass

Beatty Road

SR 2018

LR 23018

S#:16531

Distance Between Piers:
100', 100'

4.75 Bridge

Over Old Marple Road

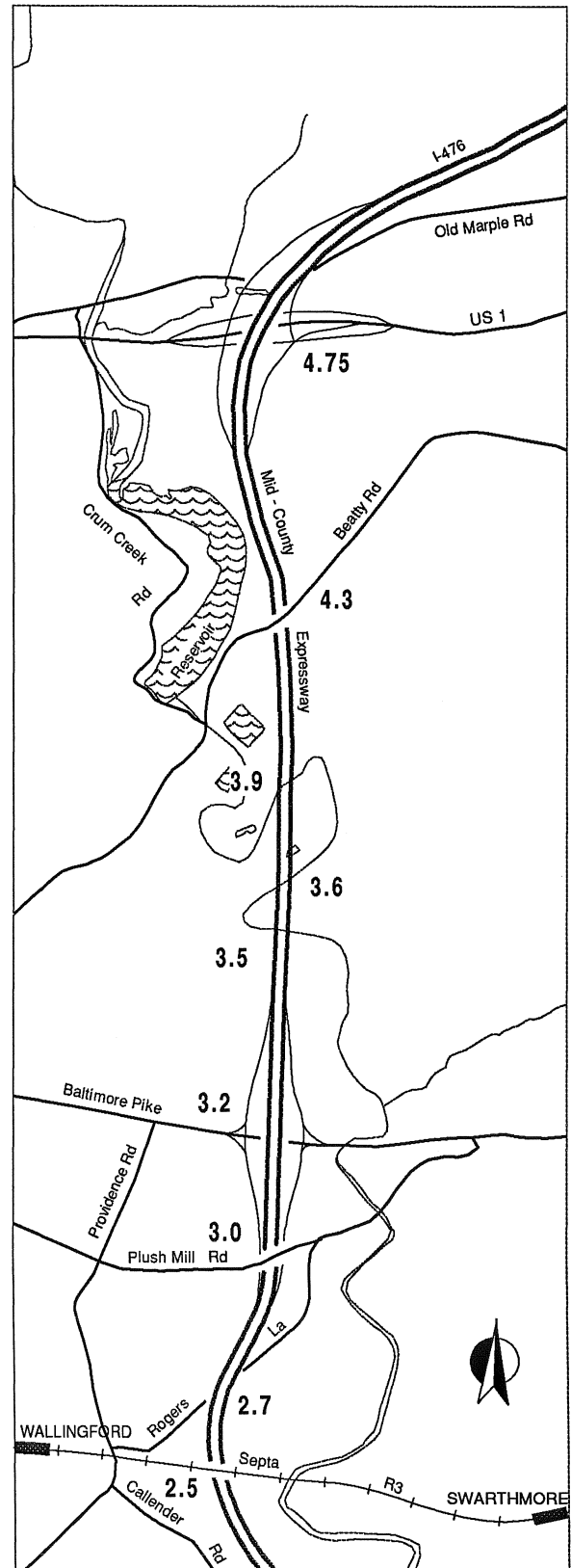
SR 1008

LR 23031;

Ramps B & C (mid-level);
and US 1/SR 001/
LR 23031 PAR

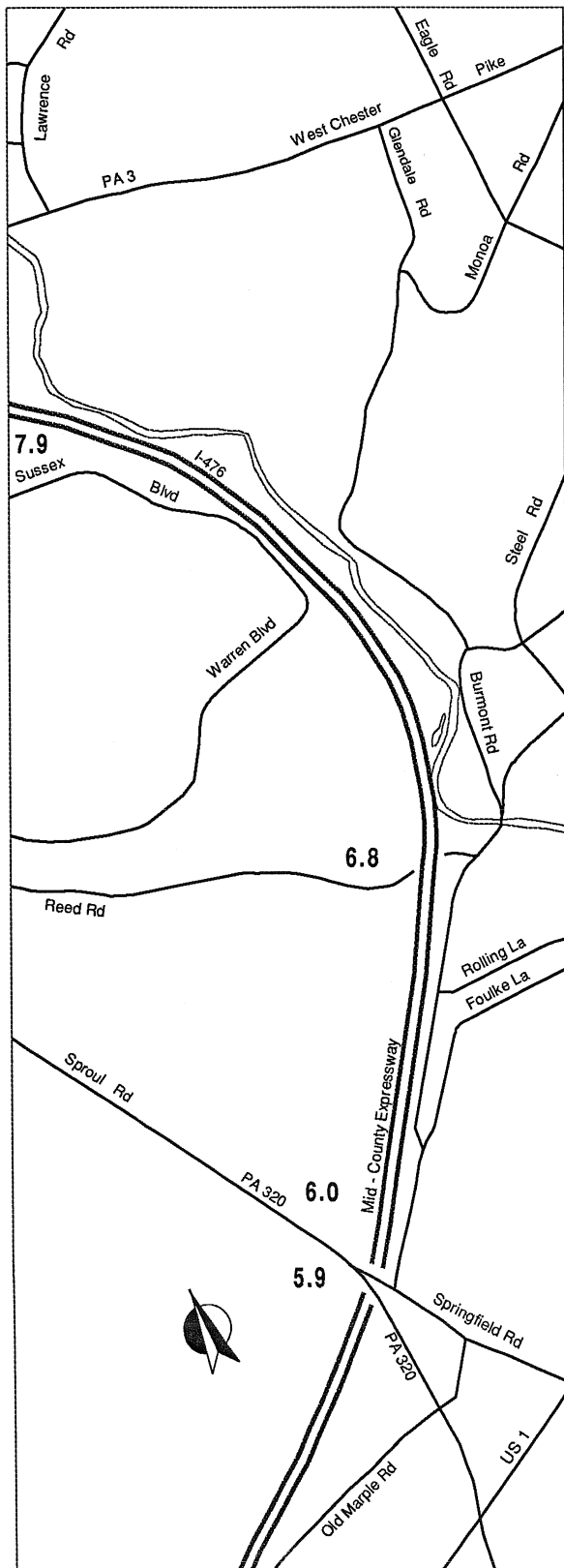
S#:16695

Structure Length: 1080'
Deck Width: 114'-0"





THE MID-COUNTY EXPRESSWAY



Map 3

Design Features

Scale: 1" = 2000'

Note: North of 5.9, two NBD and three SBD travel lanes.
All lane widths 12'. Shoulder widths 10' + (variable).

5.9 Overpass

Sproul Road PA 320

SR 320 LR 225

S#:16534

Distance Between Piers: 106'-5", 93'-9"

5.9 Overpass

Springfield Road

SR 2009 LR 580 SPUR E

S#:16535

Distance Between Piers: 86'-3", 73'-8"

6.0 Overpass

O'Hara Drive

S#:16536

Distance Between Piers: 76'-0" NBD 64'-0" SBD

6.8 Dual Bridge

Over Reed Road & Whetstone Creek

S#:16537

Span Length: 200'-0"

Deck Width: NB - 46'-6" SB - 46'-6"

Between - 21'-6"

7.9 Dual Bridge

Over Trib. Darby Creek

S#:16544

Span Length: 44'

Deck Width: NBD - 47'-0" SBD - 47'-0"

Between - 21'





THE MID-COUNTY EXPRESSWAY

Map 4

Design Features

Scale: 1" = 2000'

Note: South of 8.2, two NBD and three SBD travel lanes. North of 8.2, three lanes by direction. All lane widths 12. Shoulder widths 10' to 14.5' (variable).

8.2 Overpass

Lawrence Road
SR 1020 LR 133 Spur A
S#:16548
Distance Between Piers:
108'-6", 89'-6"

8.6 Overpass

West Chester Pike PA 3
SR 003 LR 133
S#:16545
Distance Between Piers:
110'-11", 86'-1"

9.5 Overpass

Marple Road
SR 1028 LR 23058
S#:16698
Distance Between Piers:
NBD - 91'
SBD - 97'-6"

9.6 Dual Bridge

Over Darby Creek
S#:16699
Span Length: NBD - 592'
SBD - 603'-7"
Deck Width: NBD - 60'-9"
SBD - 60'-9"

10.2 Bridge

Over Darby Road
SR 2005 LR 23047
S#:16836
Span Length: 70'
Deck Width: 110'-6"

10.4 Bridge

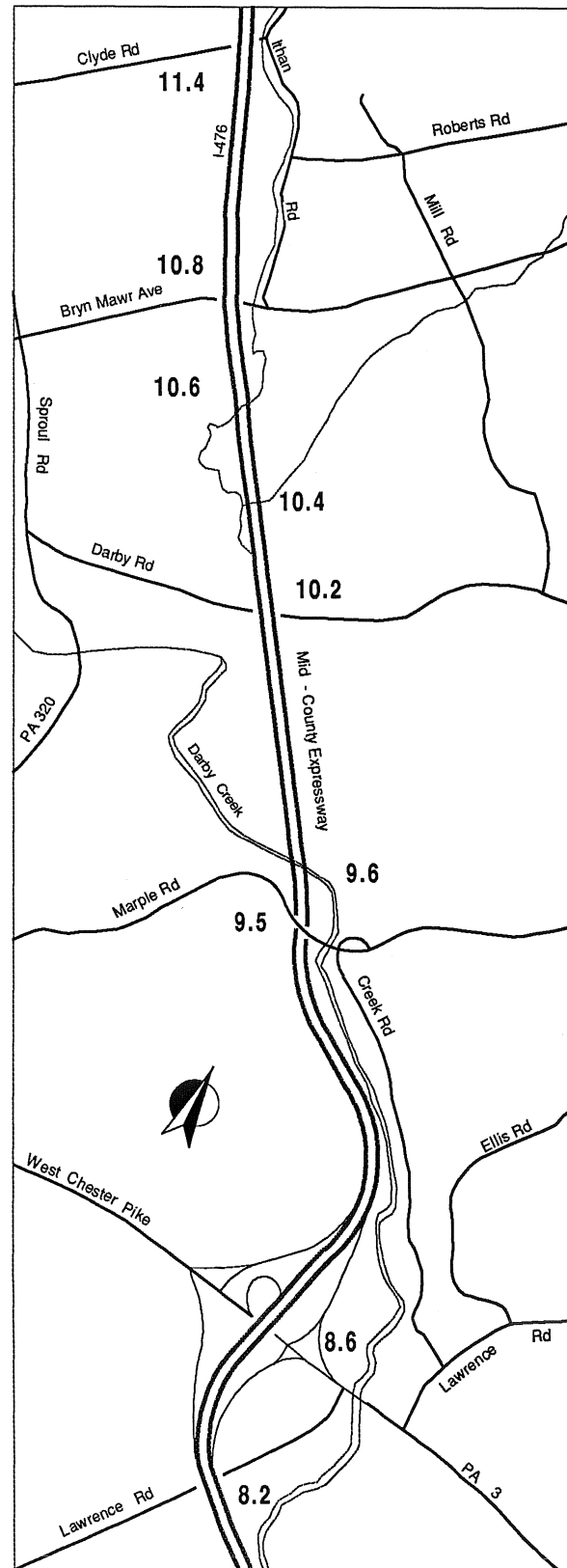
Over Meadow Brook Run
S#: 16837
Span Length: 20'-0"
Deck Width: 110'-0"

10.8 Dual Bridge

Over Bryn Mawr Avenue
SR 1032 LR 23035
S#:16839
Span Length: NBD 55'-2"
SBD 55'-2"
Deck Width: NBD 55'-5"
SBD 55'-5"

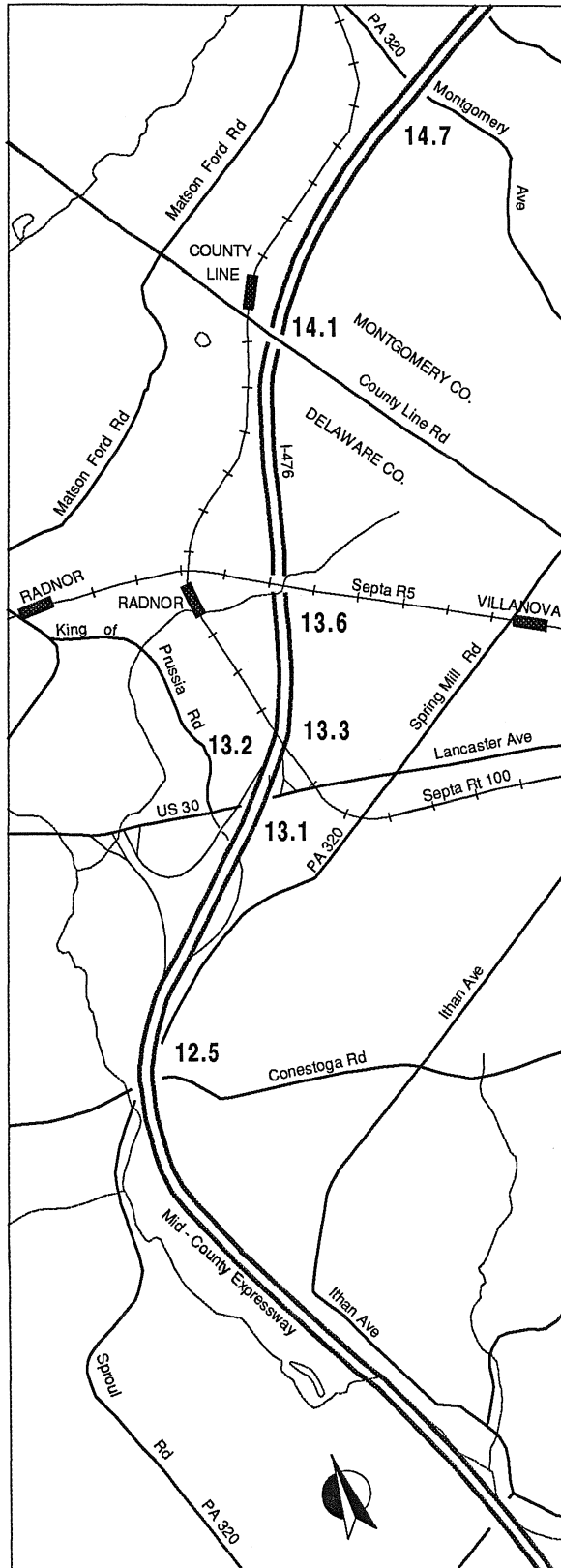
11.4 Dual Bridge

Over Clyde Road
S#:16840
Span Length: NBD 55'-2"
SBD 55'-2"
Deck Width: NBD 55'-3"
SBD 55'-3"





THE MID-COUNTY EXPRESSWAY



Map 5

Design Features

Scale: 1" = 2000'

Note: Three 12' lanes by direction. Shoulder widths 10' to 14.5' (variable).

12.5 Bridge

Over Sproul Road
SR 320 LR 225
and Conestoga Road
SR 1019 LR 577
S#:16841
Span Length: NBD - 562'
SBD - 560'
Deck Width: 62'

13.1 Dual Bridge

Over Lancaster Avenue
SR 0030 LR 142
S#:16903
Span Length: 180'-7"
Deck Width: NBD - 71'-2"
SBD - 71'-2"

13.1 Dual Bridge

Over SR 0030 LR 142
Ramp N-L
S#:16898
Span Length: NB 119'
SB 119'
Deck Width: NBD - 60'-6"
SBD - 60'-6"

13.2 Bridge

Over Browns Run
S#:16902
Span Length: 101'
Deck Width: 75.7'

13.3 Dual Bridge

Over SEPTA Norristown (100)
S#:16899
Span Length: NBD 112'
SBD 115'
Deck Width: NBD 60.5'
SBD 68.9'

13.6 Overpass

Amtrak R.R. / SEPTA R-5
S#: 16900
Distance Between Piers:
center pier

14.1 Overpass

County Line Road
S#: 8890
Distance Between Piers:
center pier

14.7 Dual Bridge

Over Montgomery Avenue
S#: 8891
Span Length: NBD 105'
SBD 105'
Deck Width: NBD 56'
SBD 56'





THE MID-COUNTY EXPRESSWAY

Map 6

Design Features

Scale: 1" = 2000'

Note: Three 12' lanes by direction. 12' shoulder width.

14.7 Dual Bridge

Over Montgomery Avenue
S#: 8891
Span Length: NBD 105'
SBD 105'
Deck Width: NBD 56'
SBD 56'

14.95 Overpass

Old Gulph Road
SR 3030 LR 225
S#: N/A
Distance Between Piers:
center pier

15.8 Dual Bridge

Over I-76
SR 0076 LR 769
S#: 8893
Structure Length: NBD 576'
SBD 570'
Deck Width: NBD 68'
SBD 56'

16.0 Dual Bridge

Over Matson Ford Road
SR 3016 LR 46140
and De Haven Street
S#: 8898
Structure Length: NBD 235'
SBD 256'
Deck Width: NBD 56.0'
SBD 56.0'

16.4 Dual Bridge

Over Balligomingo Road
SR 3037 LR 46172
and Gulph Mill Creek
S#: 8905
Span Length: NBD 605'
SBD 605'
Deck Width: NBD 67.3'
SBD 55.3'

16.7 to 17.1 Dual Bridge

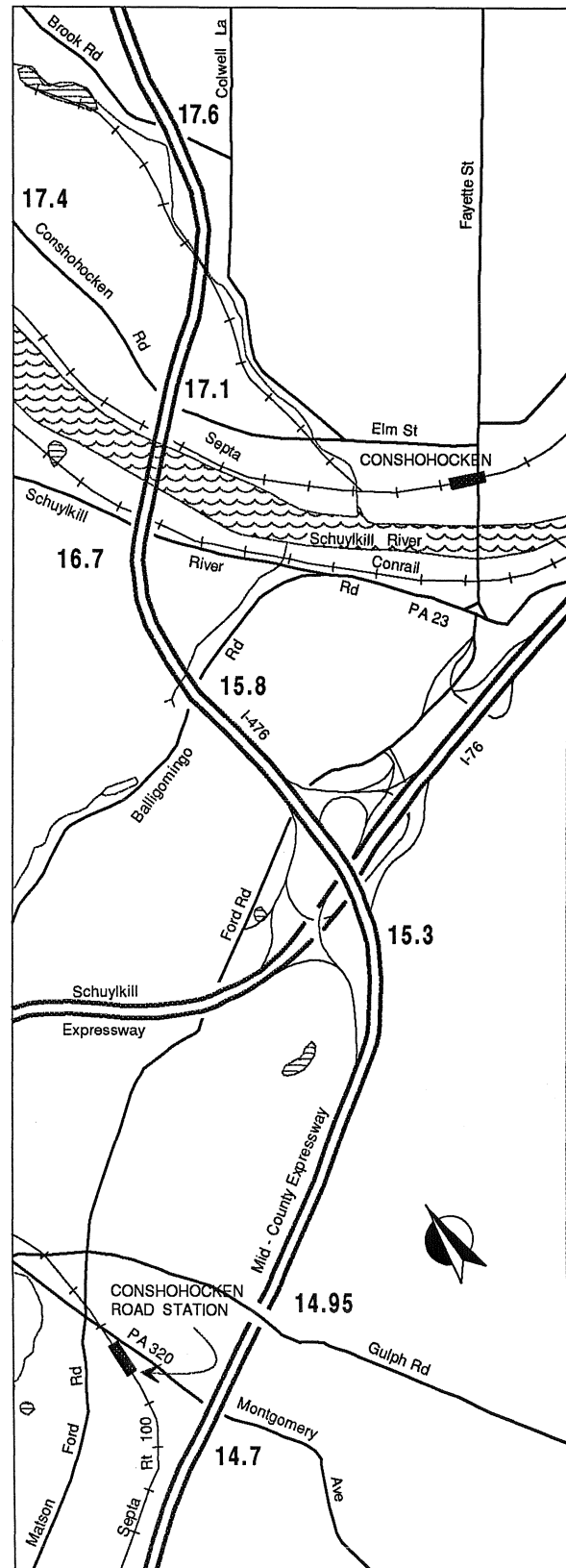
Over Schuylkill River Road
SR 0023 / LR 46138;
Conrail; Schuylkill River;
SEPTA R-6;
and Conshohocken Road
SR 3013 LR 46107
S#: 9171
Span Length: NBD 1942'
SBD 2013'
Deck Width: NBD 56' SBD 56'

17.4 Dual Bridge

Over Conrail and
Plymouth Creek
S#: 9511
Span Length: NBD 316'
SBD 424'
Deck Width: NBD 55.2'
SBD 55.2'

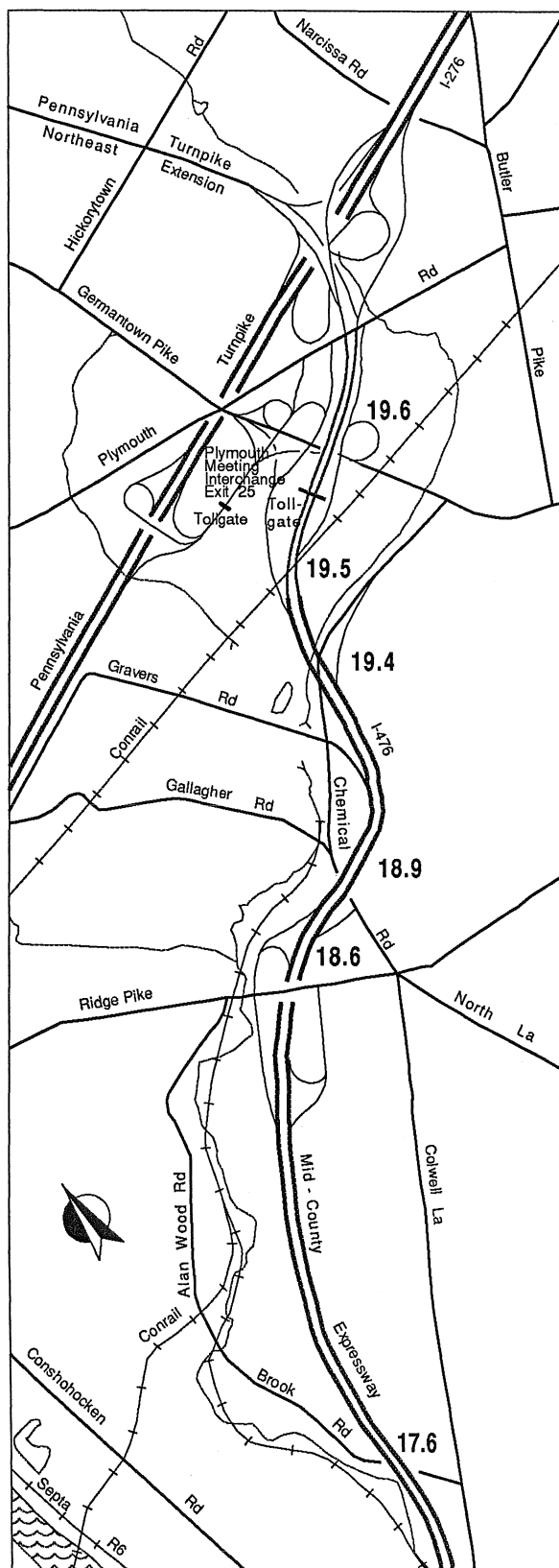
17.6 Dual Bridge

Over Brook Road
S#: 9514
Span Length: NBD 136'
SBD 140'
Deck Width: NBD 56'
SBD 56'





THE MID-COUNTY EXPRESSWAY



Map 7

Design Features

Scale: 1" = 2000'

Note: Three 12' lanes by direction. 12' shoulder width.

17.6 Dual Bridge

Over Brook Road

S#: 9514

Span Length: NBD 136' SBD 140'

Deck Width: NBD 56' SBD 56'

18.6 Overpass

Ridge Pike

S#: 9516

Distance Between Piers: center Pier

18.9 Dual Bridge

Over Chemical Road

SR 3015 LR 46110

S#: 9515

Structure Length: NBD 196' BD 202'

Deck Width: NBD 56' SBD 68'

19.4 Bridge

Over Chemical Road

SR 3015 LR 46110

S#: 16486

Structure length: 162'

Deck Width: 70.5'

19.5 Bridge

Over Conrail

S#: 16485

Span Length: 147'

Deck Width: 72'

19.6 Bridge

Over Germantown Pike

SR 3053 LR 145

S#: 16486

Span Length: 195'

Deck Width: 60.5'



Print
on
green
paper

ADJACENT LAND USE

The adjacent land use maps identify the land use on parcels immediately adjacent to the Mid-County Expressway's right of way. The purpose of this inventory is to highlight land use considerations which may impact, positively or negatively, upon large-scale roadway modifications.

Nine different land use categories are used to designate adjacent land uses. These categories are general in nature so as to facilitate their use, but sufficiently specific to allow for a meaningful designation of land use. The nine categories are:

- Agricultural
- Cemetery
- Commercial/Industrial
- Institutional
- Parkland
- Residential
- Transportation
- Vacant (parcels over 10 acres identified as such)
- Water

Adjacent land uses are identified on the map along both sides of the highway. The land use noted at the top of each map extends towards the bottom of the page until a different land use is cited under a horizontal line.

Special conditions and anecdotal information that may provide additional insight into the nature of adjacent parcels is provided where appropriate. The presence of special structures, including the proper names of identifiable places located adjacent to the roadway is also noted.

Information presented on this map was assembled from DVRPC in-house data, including aerial photographs, and field views.



THE MID-COUNTY EXPRESSWAY

Residential
(Pendle Hill; Wallingford;
Avondale Springs)

Commercial/Industrial
(Springhaven Country Club)

Vacant
(Springhaven Country Club
adjacent to creek)

Water
(Crum Creek)

Vacant
(Residential adjacent to creek)

Residential
(Including Garden City)

Vacant

Map 1

Adjacent Land Use

Scale: 1" = 2000'

Vacant
(Swarthmore College
adjacent to creek)

Vacant

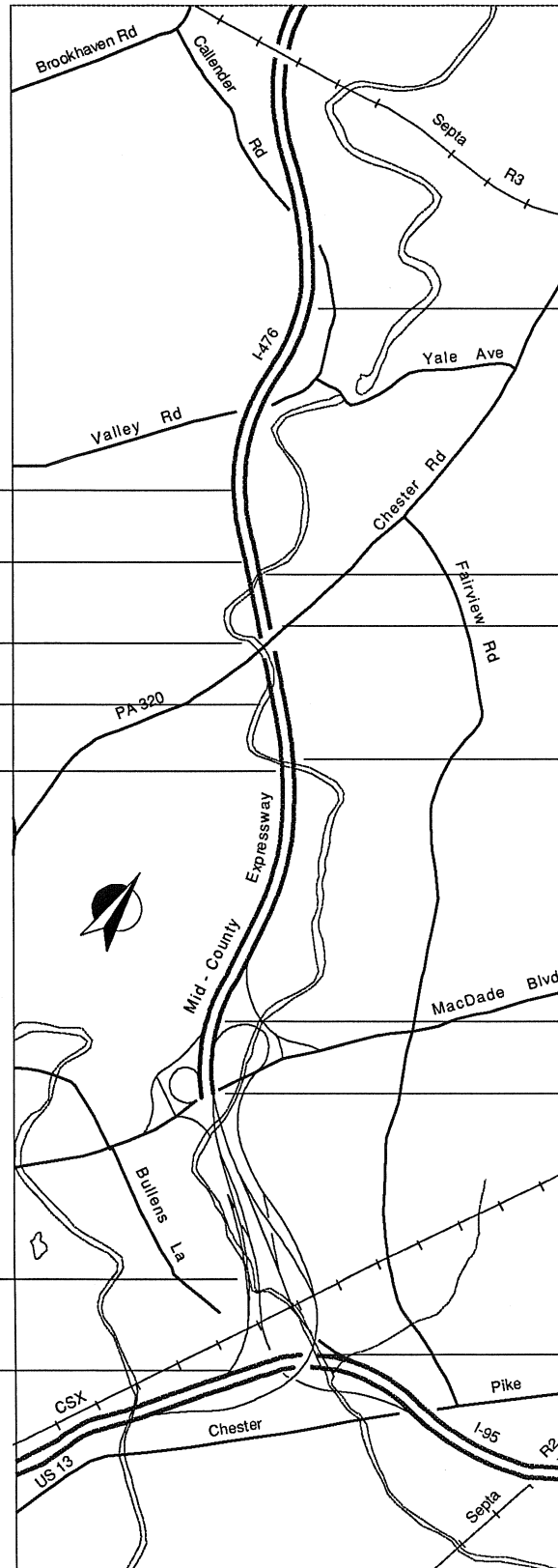
Commercial/Industrial

Residential

Vacant

Commercial/Industrial

Residential





THE MID-COUNTY EXPRESSWAY

Map 2
Adjacent Land Use
Scale: 1" = 2000'

Vacant

Water
(Crum Creek)

Commercial/Industrial
(Wastewater treatment)

Vacant

Commercial/Industrial

Parkland
(Smedley Park)

Residential

Vacant

Residential

Commercial/Industrial

Residential



Commercial/Industrial
(Marple Crossroads
Shopping Center)

Vacant

Residential

Vacant

Residential

Parkland
(Smedley Park; Springfield
Mall 1/2 mile east on
Baltimore Pike)

Vacant

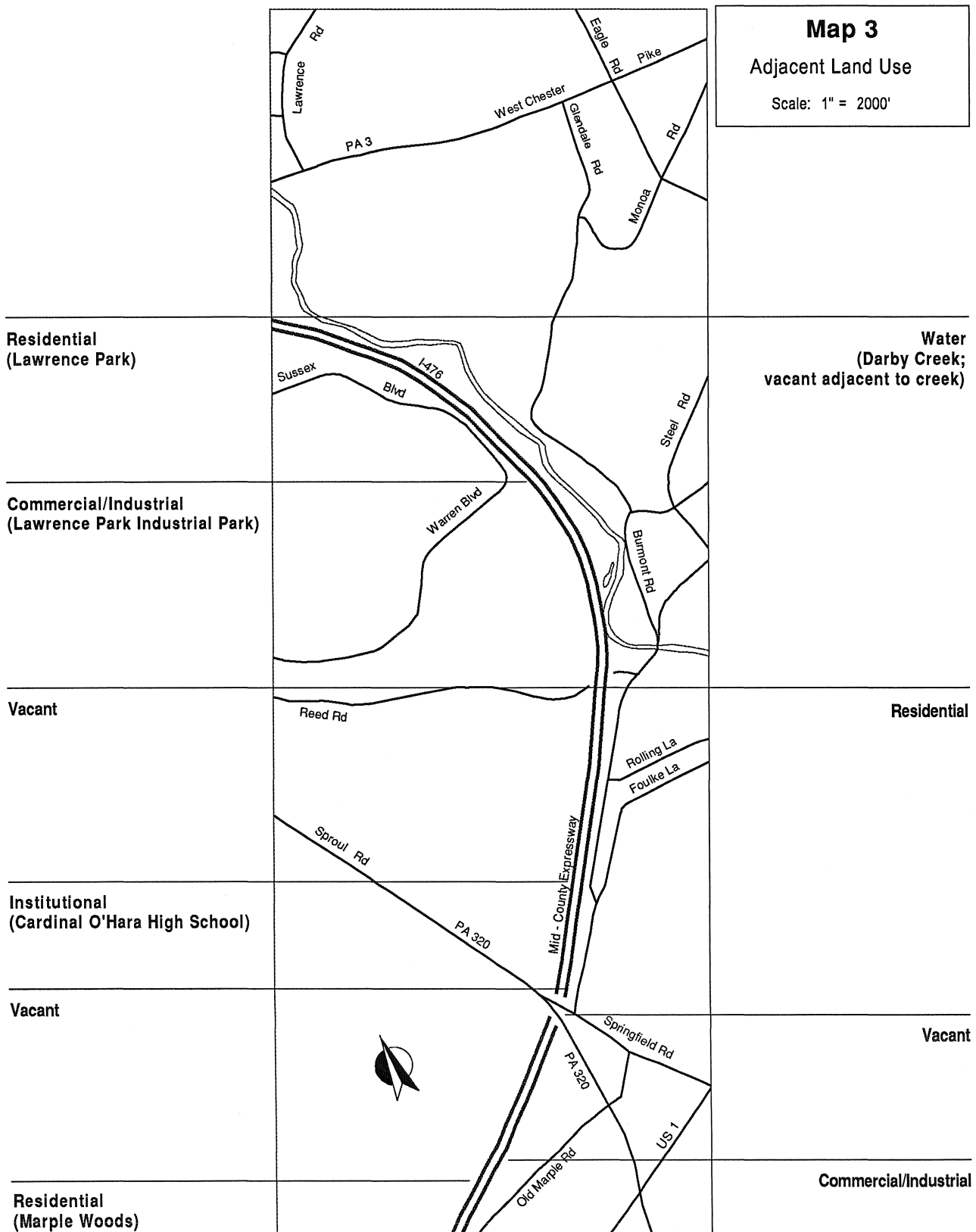
Residential

Vacant





THE MID-COUNTY EXPRESSWAY





THE MID-COUNTY EXPRESSWAY

Map 4
Adjacent Land Use
Scale: 1" = 2000'

Residential

Vacant

Parkland
(Darby Creek Valley Park)

Residential

Vacant

Residential
(Brookthorpe Hills)

Vacant

Residential

Vacant

Residential

Vacant

Water
(Haverford State Hospital
adjacent to creek)

Water
(Darby Creek; storm water
basin at interchange)

Commercial/Industrial

Vacant





THE MID-COUNTY EXPRESSWAY

Residential

Vacant

Transportation
(SEPTA; residential adjacent
to railroad tracks)

Vacant
(Archbishop Carroll High School
adjacent to railroad tracks)

Vacant
(St. David's Industrial Park
adjacent to railroad tracks)

Commercial/Industrial
(St. David's Industrial Park)

Institutional

Vacant

Residential

Commercial/Industrial
(Radnor Valley Country Club)

Residential

Map 5

Adjacent Land Use

Scale: 1" = 2000'

Residential

Vacant

Residential

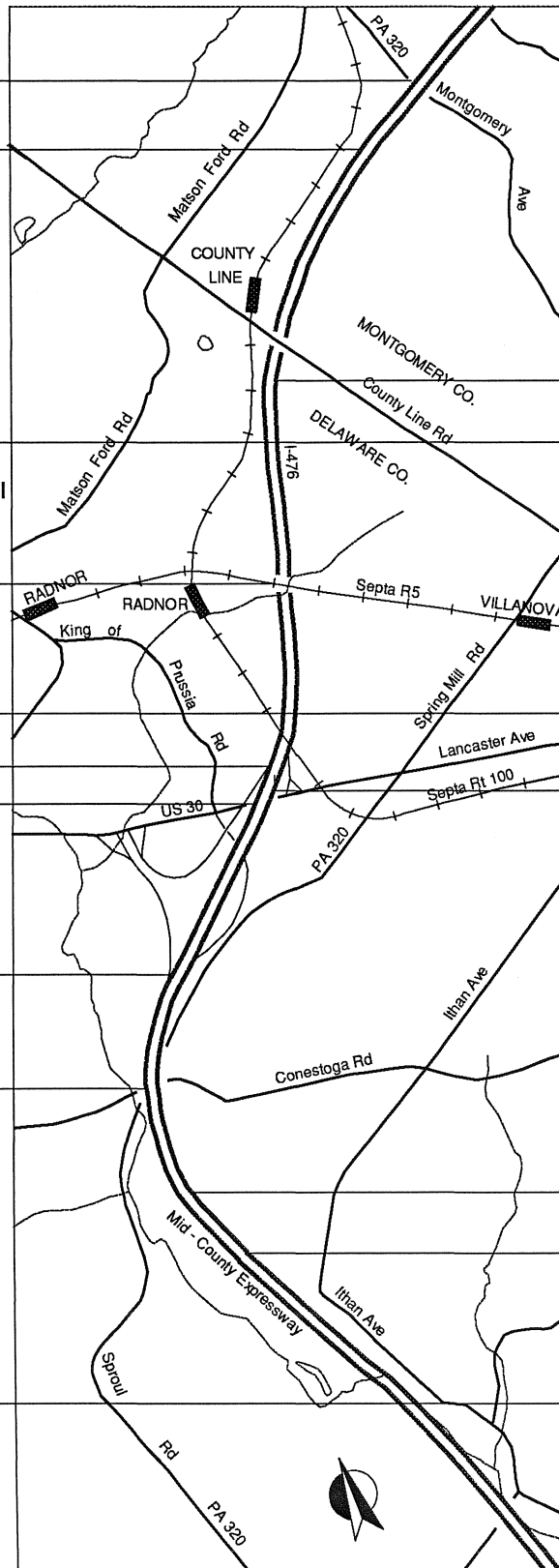
Vacant

Residential

Vacant

Residential

Vacant





THE MID-COUNTY EXPRESSWAY

Map 6

Adjacent Land Use

Scale: 1" = 2000'

Vacant
(Commercial/Industrial adjacent to
railroad tracks)

Commercial/Industrial

Water

Vacant

Commercial/Industrial

Vacant

Commercial/Industrial

Transportation
(Schuylkill Expressway;
Ford Road)

Vacant

Cemetery
(Calvary)

Residential

Commercial/Industrial

Vacant
(Commercial/Industrial adjacent to
railroad tracks)

Residential

Water

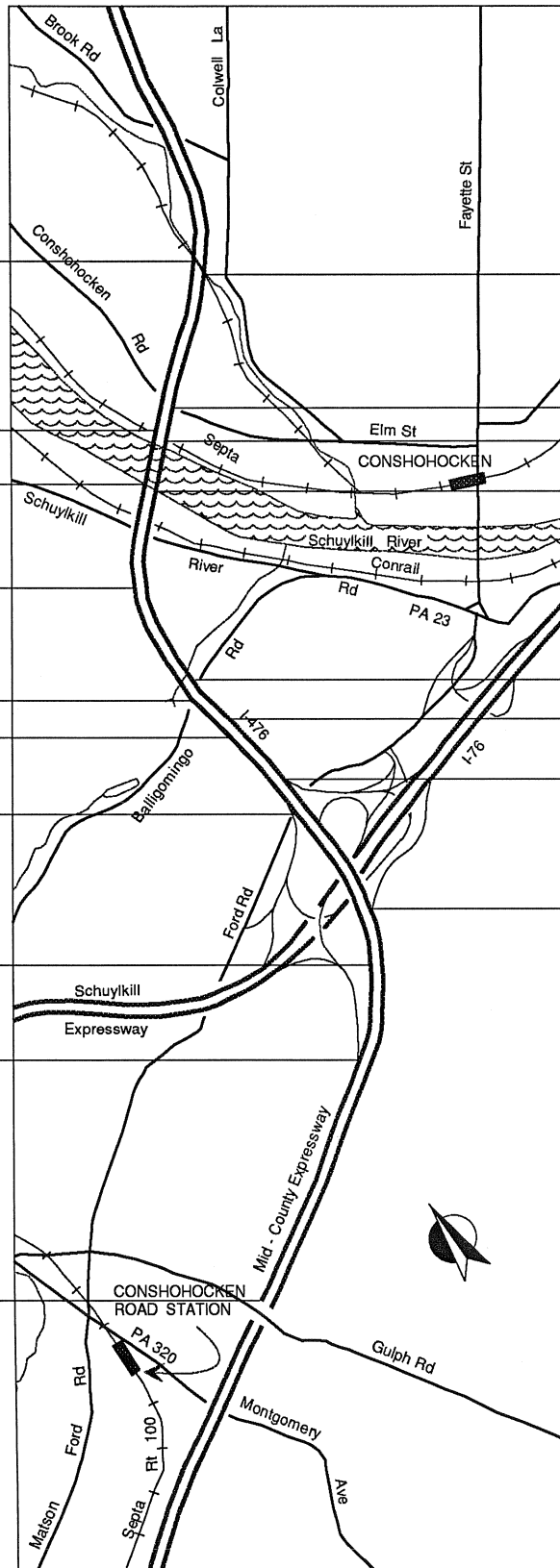
Commercial/Industrial

Vacant

Residential

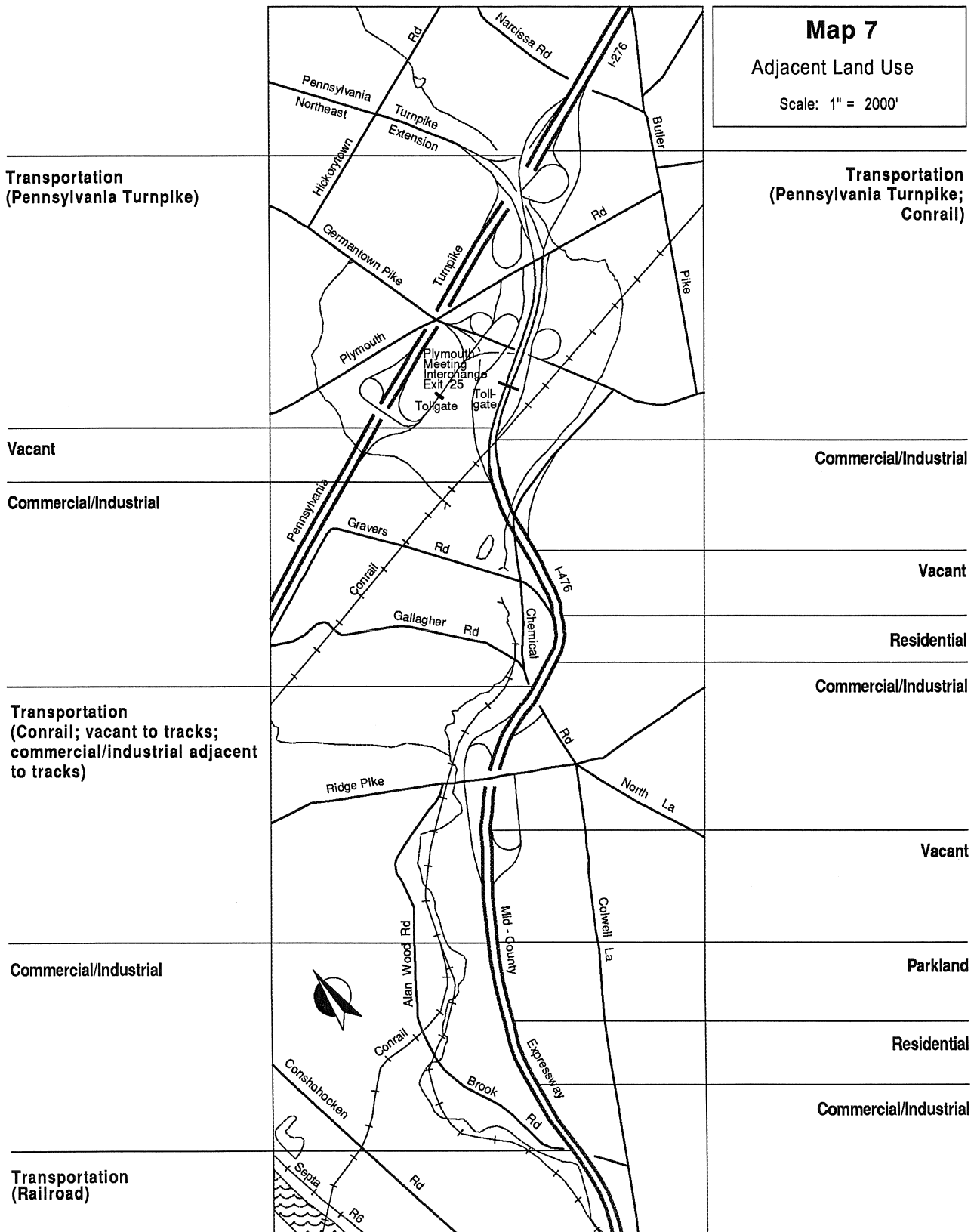
Transportation
(Schuylkill Expressway)

Residential





THE MID-COUNTY EXPRESSWAY



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yellow
Paper

PLANNED IMPROVEMENTS

This type of map summarizes transportation improvements to the Mid-County Expressway corridor which are either underway or programmed. Relevant improvements are also shown for parallel and intersecting roadways. Projects which are not yet funded are not included in this listing.

Each project can be located on the map by referencing its TIP number, which is placed as closely as possible to actual location. When a project entails a corridor, it is referenced at one point only.

Two official program numbers, the Transportation Improvement Program (TIP; source: DVRPC) number and the Program Management System (PMS; source: Penn DOT) number are provided for reference purposes. In the few cases where a TIP number is not specified, federal funding is not involved and the project is therefore not on the TIP. Cost and estimated let and completion dates are subject to change.

Each project is catalogued according to the following format:

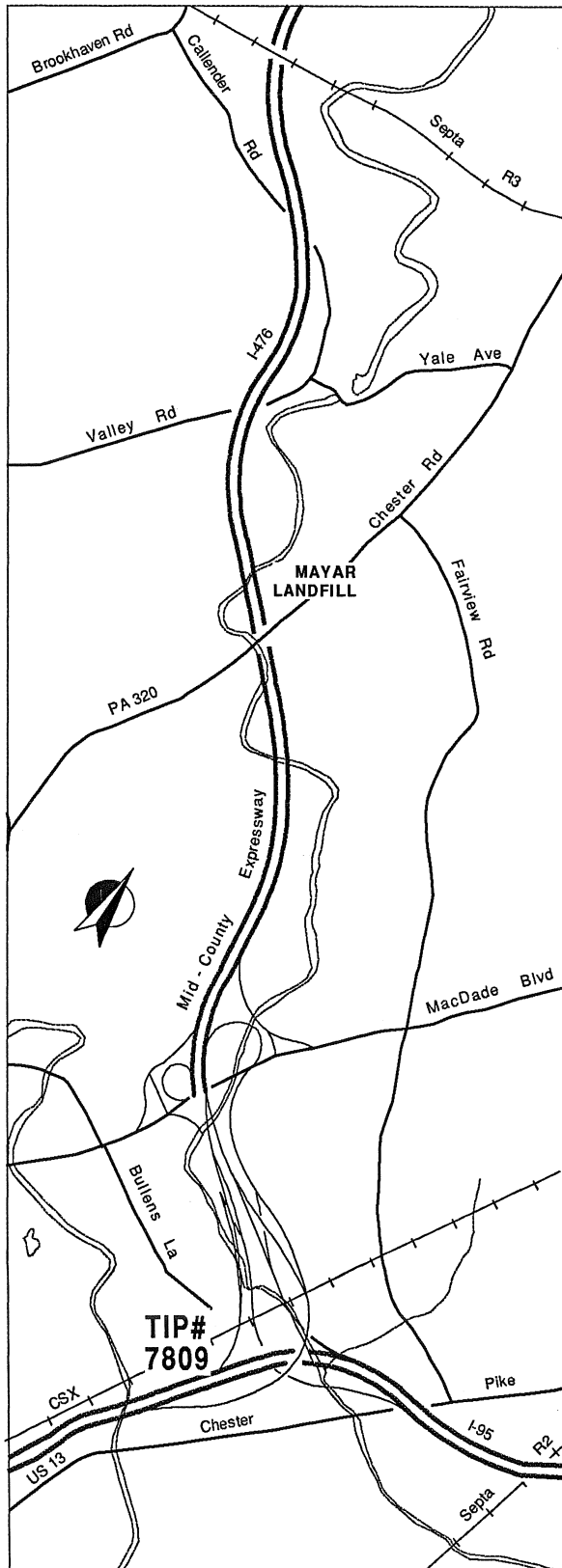
- Project Description
- Project Location
- Limits of Project (if necessary)
- Special Comments (related projects, funding, etc.)
- Transportation Improvement Program and Program Management System Numbers
- Current Estimated Cost
- Actual or Estimated Let Date
- Estimated Completion Date

Even though the road was only opened to traffic in late 1991, there are six projects along the length of I-476. In this series, they can be found on Maps 2, 4, and 5.

Information was gathered for this section from DVRPC's Transportation Improvement Program, and the PennDOT 12-year plan.



THE MID-COUNTY EXPRESSWAY



Map 1

Planned Improvements

Scale: 1" = 2000'

Installation of Noise Barrier Walls

I-476, Mid-County Expressway at I-95,

Delaware Expressway

Ridley Township

TIP # 7809

PMS # 063C500

\$2,768,000

Late date: Mid 1992

Estimated Completion: 6/93





THE MID-COUNTY EXPRESSWAY

Map 2

Planned Improvements

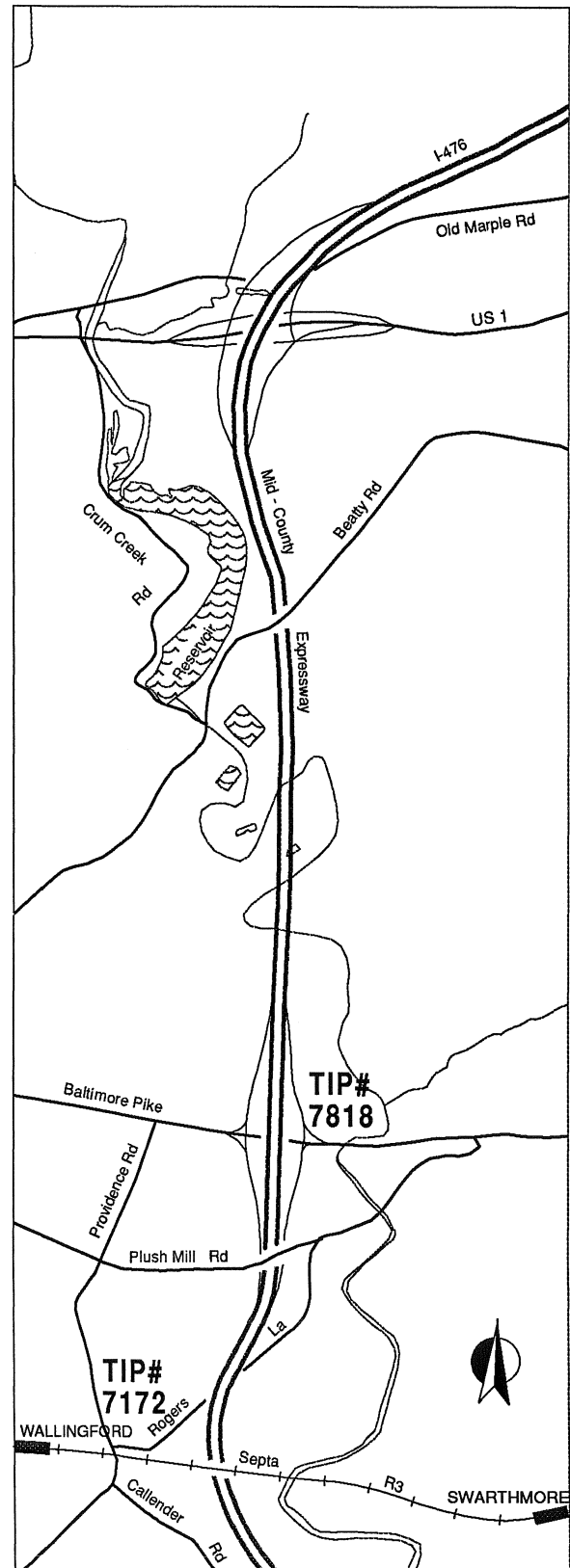
Scale: 1" = 2000'

Park & Ride Lot

I-476 Wallingford Fringe Parking Lot
at SEPTA Elwyn Commuter Rail Line
Nether Providence Township
TIP # 7172
PMS # 063C036
\$11,170,000
Let date: early 1995
Estimated completion: 10/96

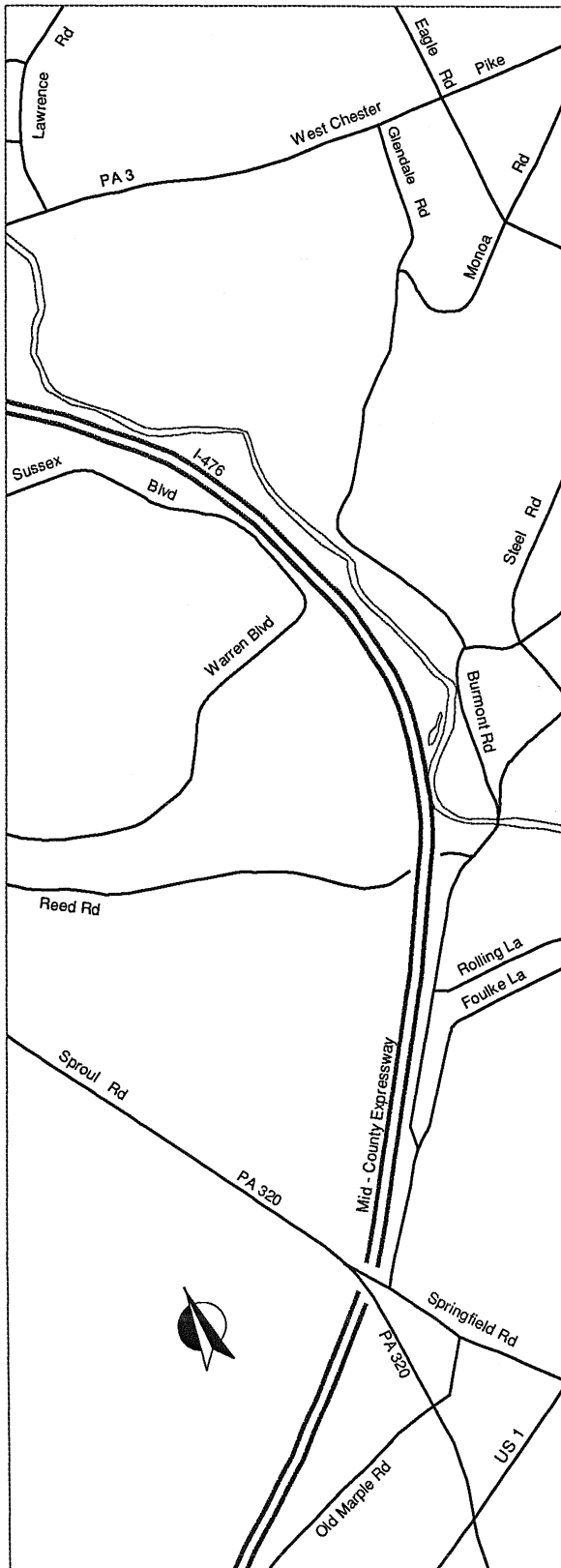
Park and Ride Lot

I-476, Mid-County Expressway at Baltimore Pike
Nether Providence Township
TIP # 7818
PMS # 063C101
\$15,350,000
Let date: Late 1993
Estimated Completion: 12/94





THE MID-COUNTY EXPRESSWAY



Map 3 *Planned Improvements*

Scale: 1" = 2000'

NO PROJECTS





THE MID-COUNTY EXPRESSWAY

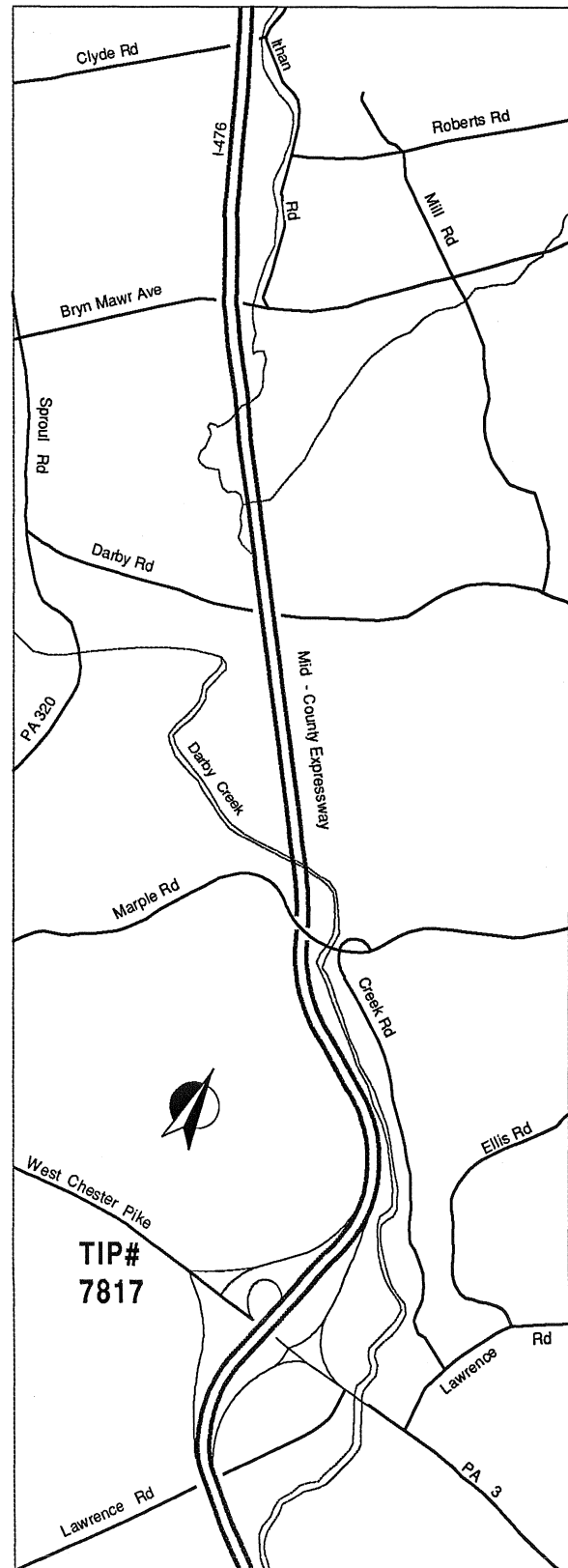
Map 4

Planned Improvements

Scale: 1" = 2000'

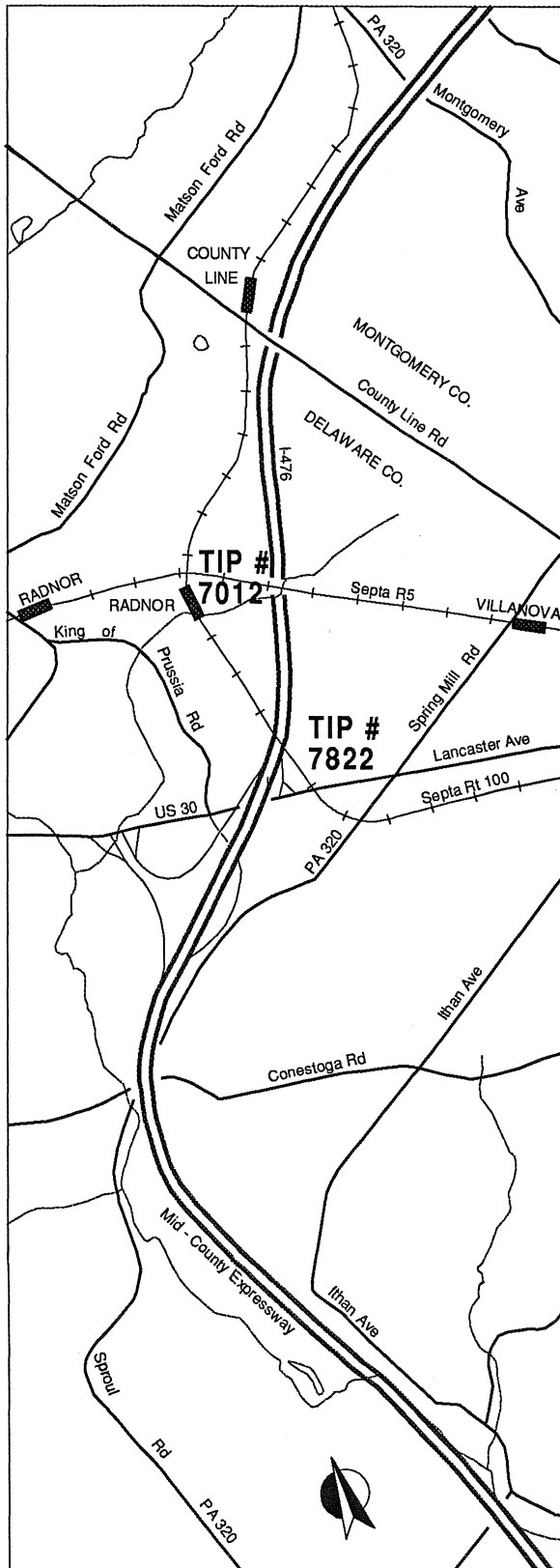
Park and Ride Lot

I-476, Mid-County Expressway at PA 3,
West Chester Pike
Marple Township
TIP # 7817
PMS # 063C100
\$8,150,000
Late date: Mid 1993
Estimated Completion: 12/93





THE MID-COUNTY EXPRESSWAY



Map 5

Planned Improvements

Scale: 1" = 2000'

I-476 Radnor Park & Ride

at SEPTA Parkesburg Commuter Rail Line
and SEPTA Route 100 Norristown High
Speed Line
Radnor Township
TIP # 7012
PMS # 063C041
\$11,670,000
Let Date: Mid 1996
Estimated Completion: 12/97

Drainage Improvement

I-476, Mid-County Expressway at US 30/
SEPTA Norristown
High Speed Line Bridge/Villanova University
Radnor Township
TIP # 7822
PMS # 063M5402
\$1,435,000
Let date: Mid 1992
Estimated Completion: 6/93

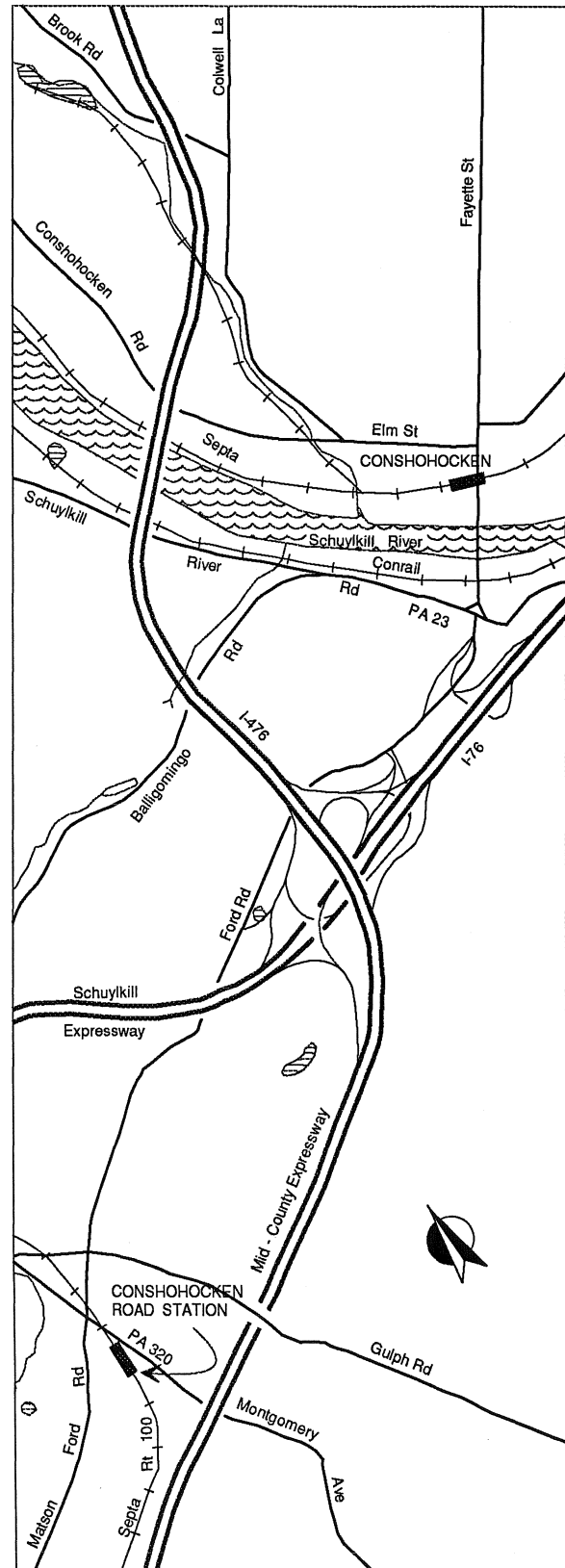




THE MID-COUNTY EXPRESSWAY

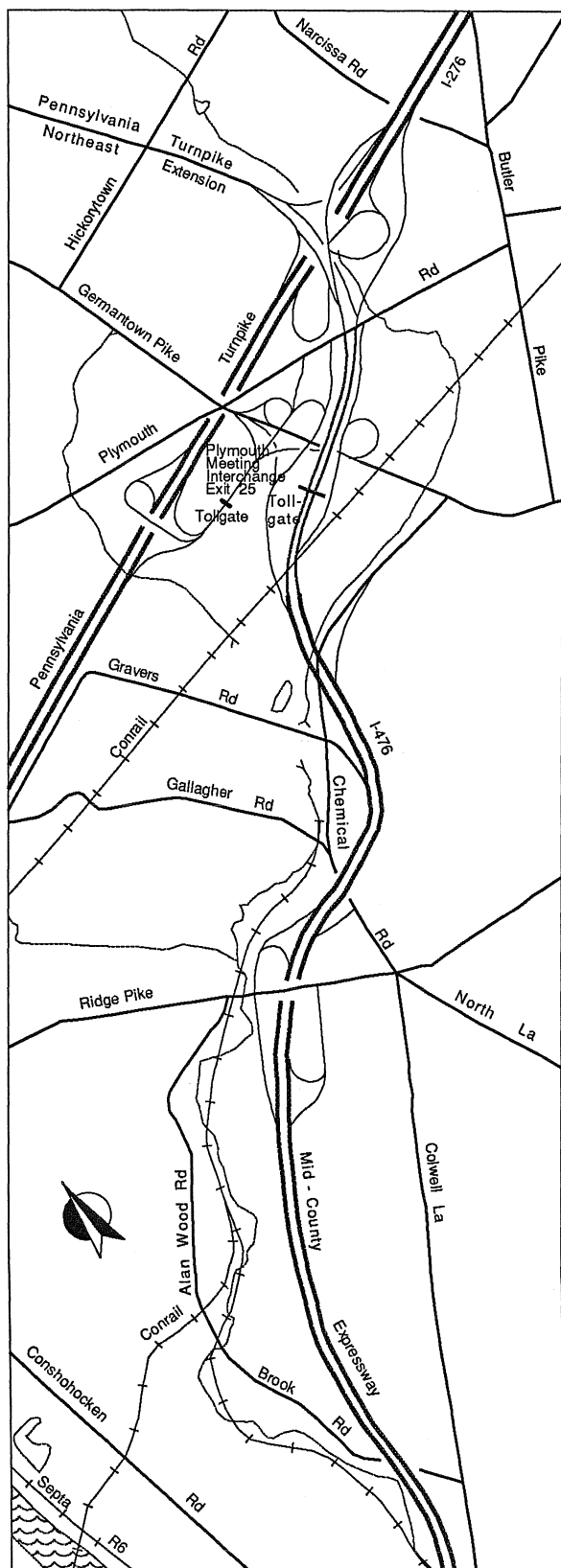
Map 6
Planned Improvements
Scale: 1" = 2000'

NO PROJECTS





THE MID-COUNTY EXPRESSWAY



Map 7
Planned Improvements

Scale: 1" = 2000'

NO PROJECTS



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NATURAL AND CULTURAL ENVIRONMENT

The natural and cultural environment maps identify noteworthy features located within close proximity to the roadway. Natural features displayed on the map include wetlands and flood plains. Cultural features are primarily historic.

Most of the wetlands information, including location and general configuration, was derived from the National Wetlands Inventory (NWI) Maps, issued by the United States Department of the Interior. Wetlands are identified by a pattern of horizontal lines. No distinction is made between the various classifications of wetlands (such as estuarine, palustrine, riverine, marine, or lacustrine).

It should be noted that the NWI maps are compiled on a very large scale and are therefore very general. An indication of wetlands on an NWI map suggests that wetlands are probably present to some extent on that particular site. Conversely, an indication that wetlands are not present suggests that the site is probably free of wetlands. However, any specific site which is proposed for development must be surveyed individually before a final determination can be made as to whether or not wetlands are actually present.

The flood plains delineated on the map represent the 100-year flood plain boundaries as identified on the Flood Insurance Rate Maps distributed by the Federal Emergency Management Agency. Flood plain areas are indicated by a pattern of vertical lines. Areas where flood plains and wetlands overlap are indicated by intersecting vertical and horizontal lines. Wetlands which are actually bodies of standing water are already depicted on the map.

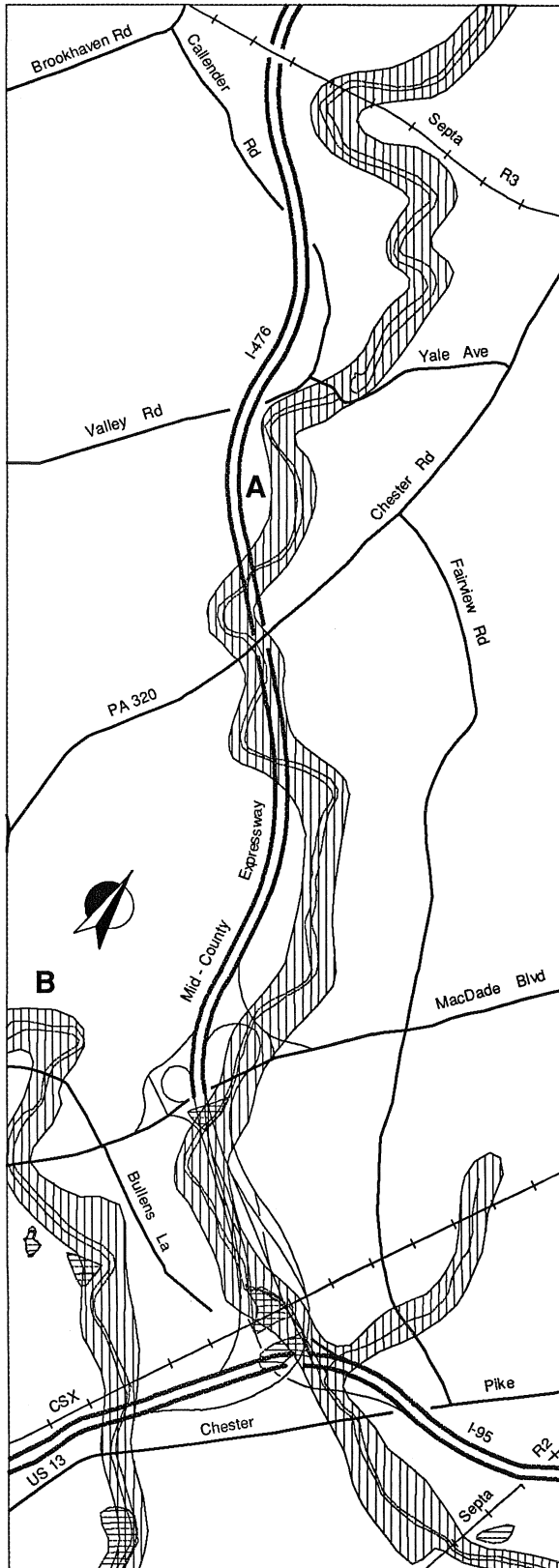
Historical features are also identified. These features include sites, structures, or districts which are significant in terms of American history, architecture, or culture. The general locations of the features are indicated on the map by large, upper-case letters. The site name, the year built, a general description and the type of ownership are provided in the margin text. Sites which are listed on the national Register of Historic Places are identified as such.

Other sites or buildings which have been determined to be eligible for the National Register are also identified. All public agencies are required to safeguard properties on the National Register as well as those which are or may be eligible for it.

Information for this section was gathered from DVRPC reports and field observations.



THE MID-COUNTY EXPRESSWAY



Map 1

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

A Thomas Leiper House (Avondale)

521 Avondale Road between East Rose Valley Road and Route 320.

Built 1785. Listed on the National Register.

Municipal ownership.

B Lapidea (Governor Sproul Estate)

Brent Drive near intersection of Routes 252 and Route 320.

Main house built 1818; carriage house built circa 1734.

Eligible for National Register. Private.





THE MID-COUNTY EXPRESSWAY

Map 2

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

A Fell Home

501 Beatty Road. Built circa 1744. Eligible for National Register. Private.

B Springfield Country Club

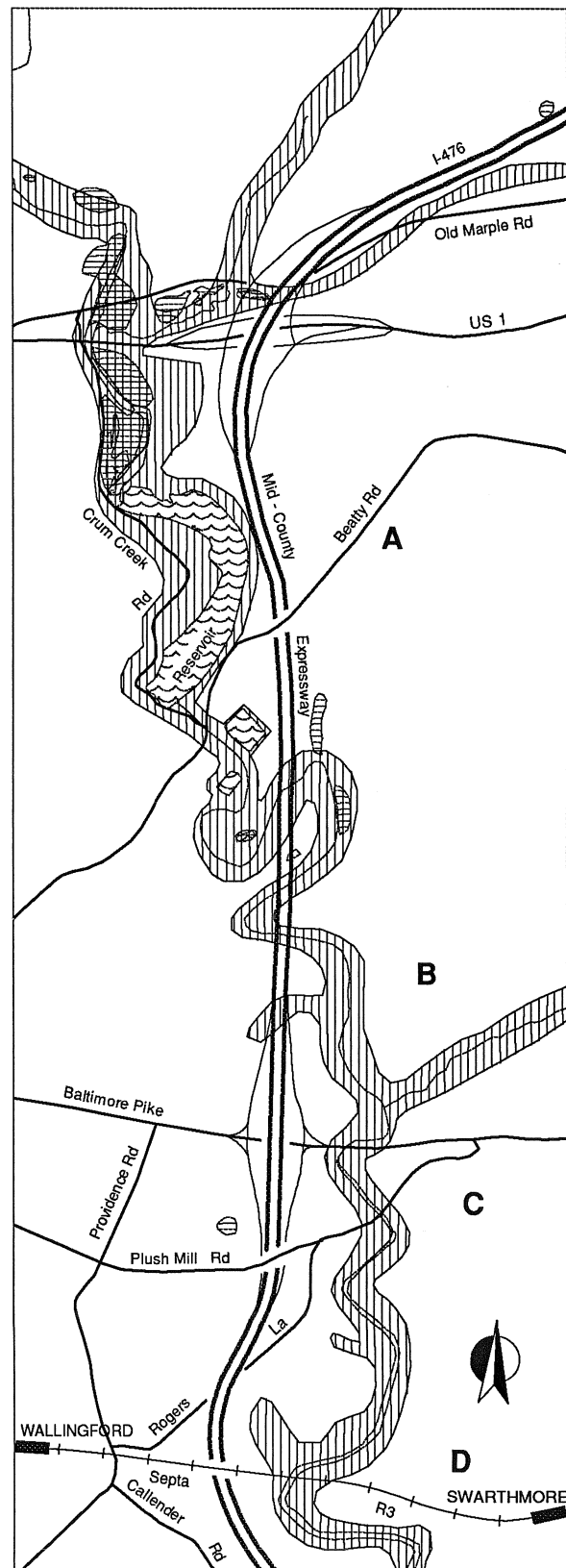
Route 320, Springfield Township. House built 1890 (currently used as restaurant). Also site of ancient Indian village. Municipal ownership.

C Clapboard House

538 Walnut Lane. Built 1683; first floor on right side encloses log cabin. Private.

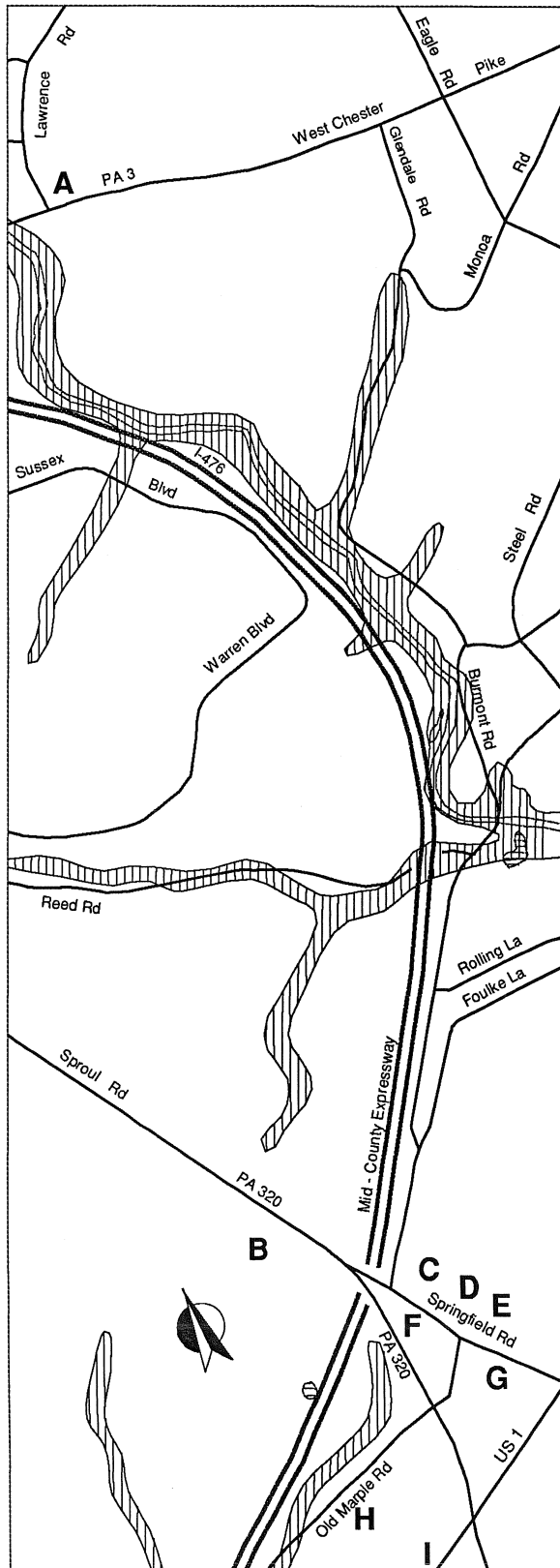
D Parrish Hall

Swarthmore College campus. Determined eligible for National Register by Delaware County Planning Department. Institutional.





THE MID-COUNTY EXPRESSWAY



Map 3

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

A Joseph Powell House

West Chester Pike and Lawrence Road. Built circa 1792. Municipal ownership.

B Worrall House

2105 Sproul Road. Built 1720. Eligible for National Register Private.

C Lamb Tavern

Springfield and Eagle Roads. Built circa 1808. Eligible for National Register. Private.

D Marple Store and Antique Shop

816 Springfield Road. Original land grant dated 1684; store built circa 1832. Eligible for National Register. Private.

E Reynolds-Pancoast Houses

801, 807-809 and 815 West Springfield Road. Original brick house built circa 1752. Private.

F Springfield Meeting House and Burial Grounds

Old Sproul and Springfield Roads. Site donated to Society of Friends 1686; original meetinghouse erected 1701. Existing building built circa 1851. Society of Friends.

G McCullough House

954 Old Sproul Road. Built late 1700's. Eligible for National Register. Private.

H Fillingame House

900 Old Marple Road. Built circa 1831. Eligible for the National Register. Private.

I Gibbons House

Northwest corner at Sproul Road and U.S. Route 1. Built 1830. Private.





THE MID-COUNTY EXPRESSWAY

Map 4

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

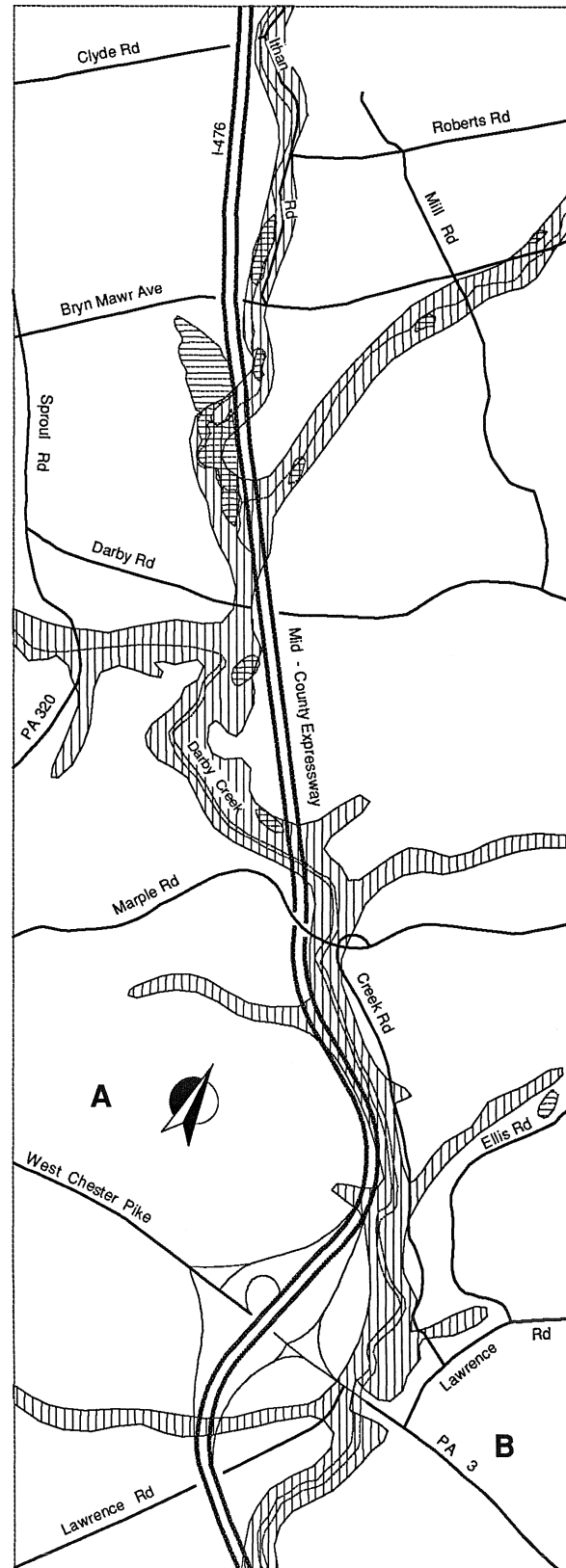
Vertical pattern: 100 - year flood plain

A Moore House (Stony Lonesome)

2321 Anthony Avenue. Built circa 1718; renovated to present condition 1848. Private.

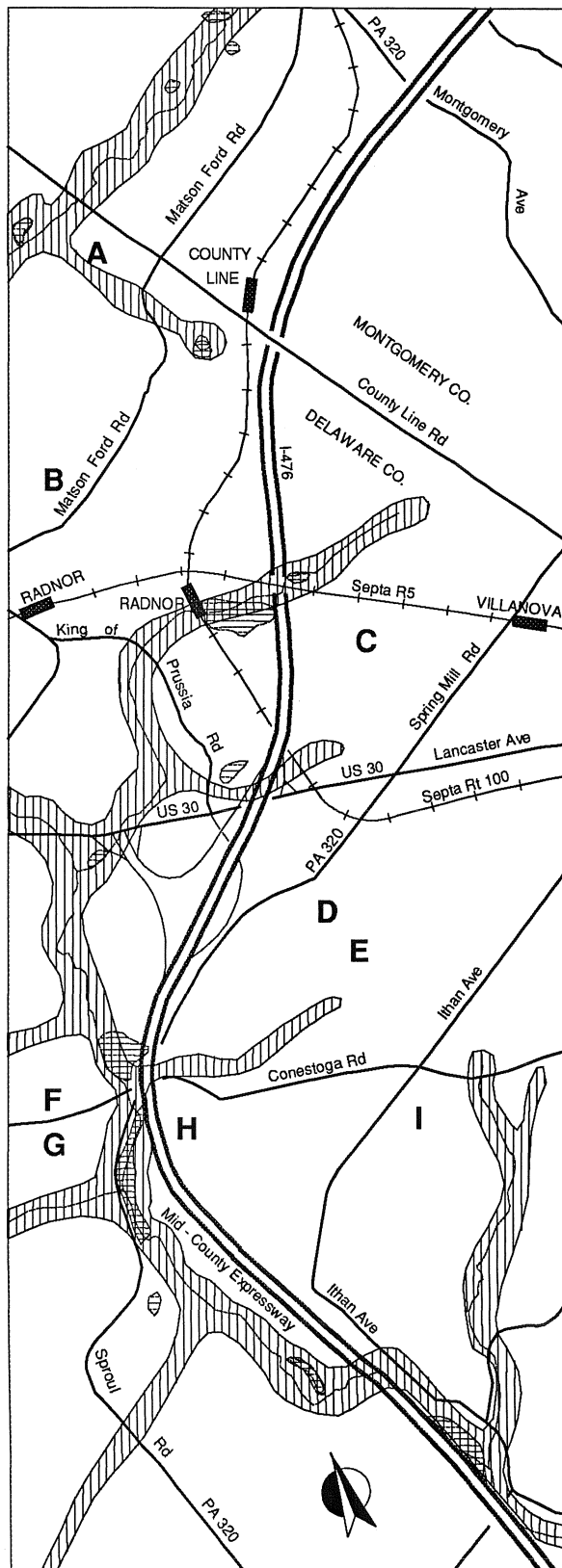
B Joseph Powell House

Chester Pike and Lawrence Road. Built circa 1792. Eligible for National Register. Municipal ownership.





THE MID-COUNTY EXPRESSWAY



Map 5

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

- A Bel Orme**
Junction of County Line Road and Matsons Ford Road. Two-story fieldstone house built early 19th century. Eligible for National Register. Private.
- B Morgan Farmhouse and Barn**
Matsons Ford Road. Built circa 1760. Private.
- C Ashwood (Penn-Gaskell House) and Gatehouse**
208 Ashwood Road. Built circa 1800; post-1883 additions. Eligible for National Register. Private.
- D Woodstock Barn**
South Spring Mill Road. Built circa 1804; converted to residence in 1926. Private.
- E Chuckswood**
South Spring Mill Road. Built circa 1807. Private.
- F Green Hedges**
581 Conestoga Road. Built circa 1800. Eligible for National Register. Private.
- G Wayside**
569 Conestoga Road. Built circa 1830. Private.
- H Radnor Friends Meeting House**
Sproul and Conestoga Roads. Organized 1684. Oldest structure built circa 1718; eastern section added in 1722. Society of Friends.
- I Sorrel Horse Inn (Barclay Farm)**
Conestoga Road and Ithan Avenue. Built circa 1756. Used as inn from 1756 to late 19th century; currently Kindergarten and Lower School of Saint Agnes Irwin School. Institutional.





THE MID-COUNTY EXPRESSWAY

Map 6

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

A Gulph Mills Encampment

Site of December, 1777, encampment by the Continental army (before moving on to Valley Forge). County.

B Old Gulph School House

Matsons Ford Road, Gulph Mills. Built 1696. Church-owned.

C Gulph Christian Church

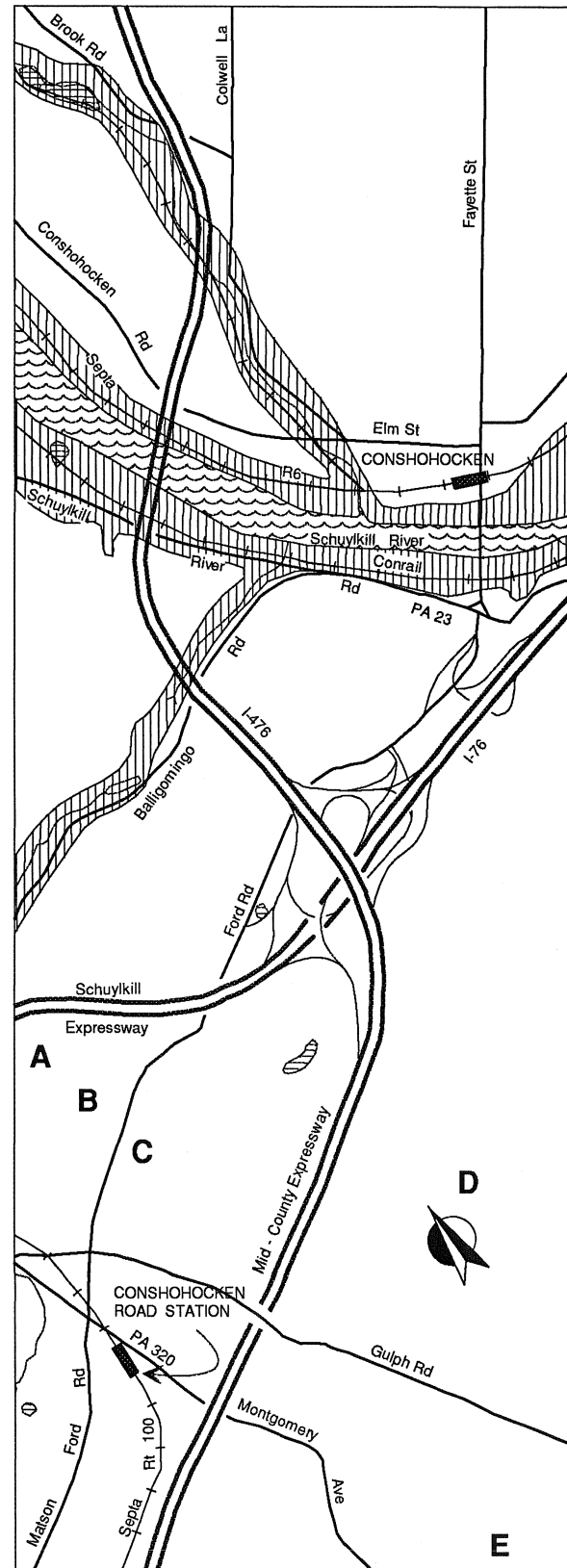
Gulph Mills and Matsons Ford Roads. Built 1835; enlarged 1890. Church-owned.

D The Meadows

1635 Lark Lane. Farmhouse and barn built circa 1745. Private.

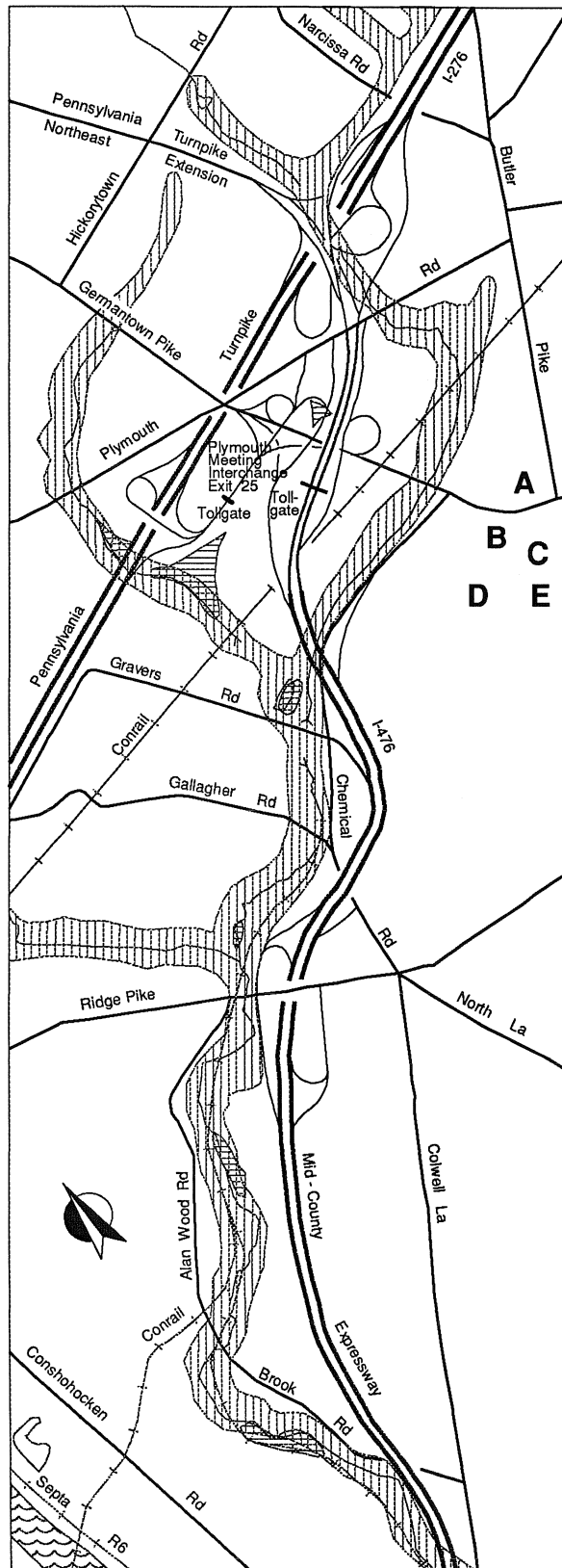
E Stoke Poges House

1425 Mount Pleasant Road. Built circa 1780; addition built 1830. Private.





THE MID-COUNTY EXPRESSWAY



Map 7

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

A Post Office and Country Store

Intersection of Germantown and Butler Pikes. Built circa 1827. Private.

B Residence

127 West Meeting House Lane. Built circa 1851. Private.

C Hovenden House

Intersection of Germantown and Butler Pikes. Built circa 1794. Private.

D Plymouth Library (William Jeanes Memorial Library)

Butler Pike. Built 1935. Plymouth Monthly Meeting.

E Plymouth Friends Meeting House

Butler Pike. Built circa 1710; eastern wing added 1780. Society of Friends.



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TRANSIT AND TRAFFIC DATA

These maps illustrate those transit routes which either cross over or under the Mid-County Expressway. Reference is made in the accompanying text to the route number, the origin and destination points, the road it travels at the point of intersection, and whether the crossing is over or under the highway. All points of intersection are shown. There are no bus routes currently operating along the Mid-County Expressway.

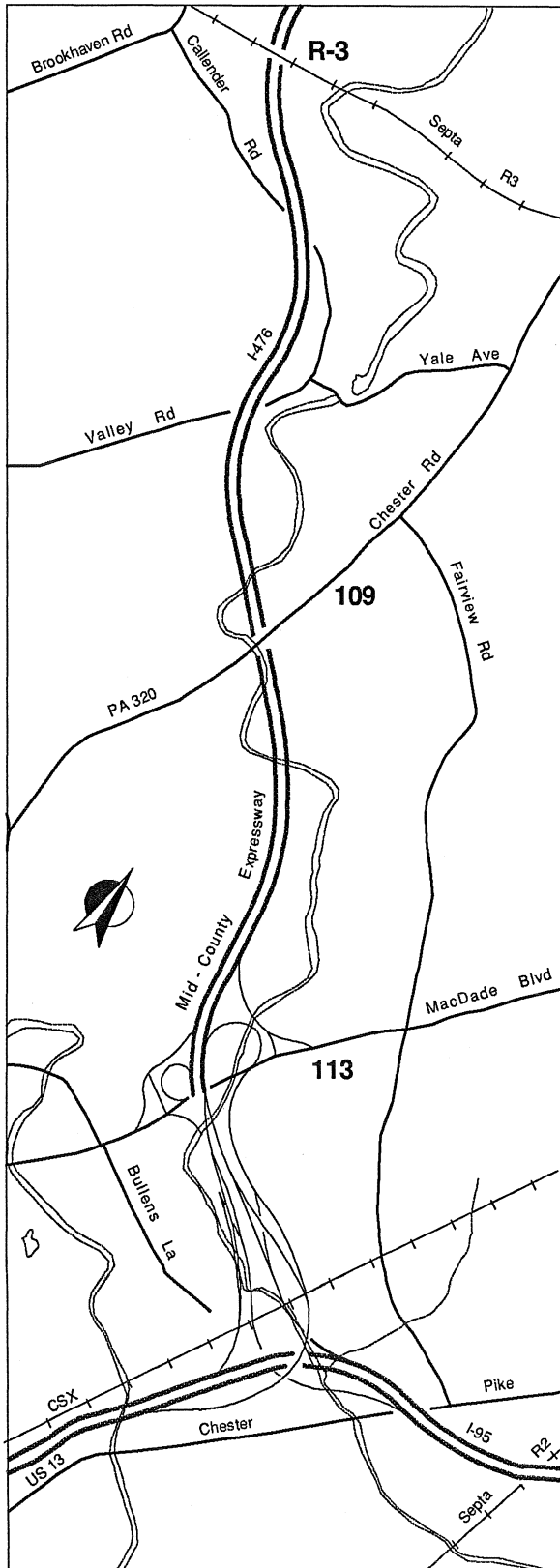
Given that this road was only recently opened to traffic, actual Average Annual Daily Traffic (AADT) counts are not available at this time.

Information for this section was provided by Southeastern Pennsylvania Transportation Authority.





THE MID-COUNTY EXPRESSWAY



Map 1

Transit and Traffic Data

Scale: 1" = 2000'

- 113** 69th Street to Marcus Hook
Crosses under I-476 at MacDade Boulevard
- 109** 69th Street to Chester
Crosses over I-476 at Chester Road PA 420
- R-3** Regional Rail to Elwyn
Crosses over I-476 north of Rogers Lane





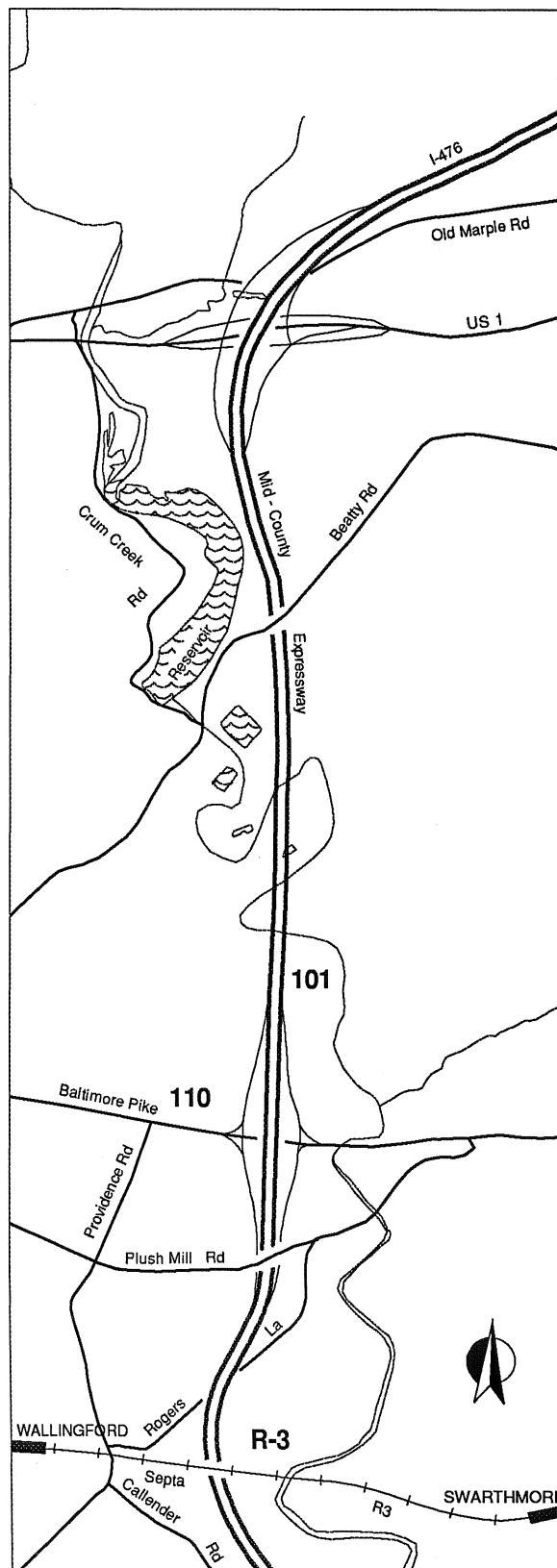
THE MID-COUNTY EXPRESSWAY

Map 2

Transit and Traffic Data

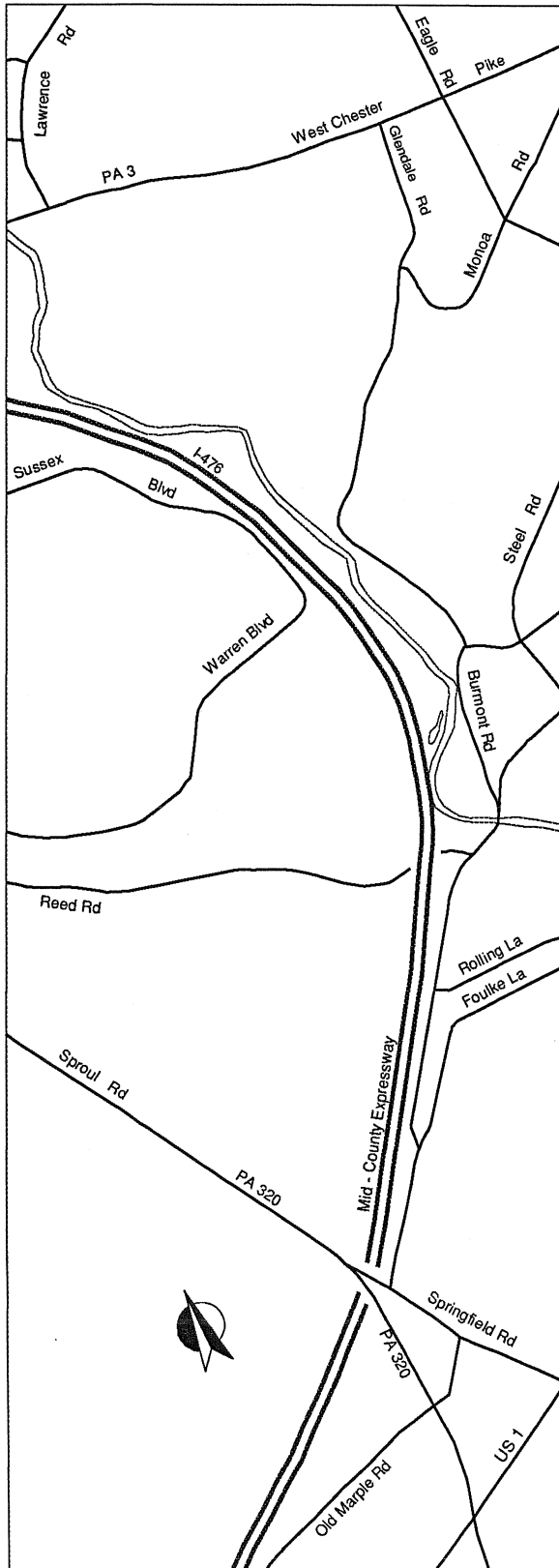
Scale: 1" = 2000'

- R-3** Regional Rail to Elwyn
Crosses over I-476 north of Rogers Lane
- 110** 69th Street to Penn State Lima Campus
Crosses under I-76 at Baltimore Pike
- 101** Route 101 (not shown)
Trolley from 69th Street to Media
Crosses under I-476 north of Baltimore Pike





THE MID-COUNTY EXPRESSWAY



Map 3

Transit and Traffic Data

Scale: 1" = 2000'

NO TRANSIT





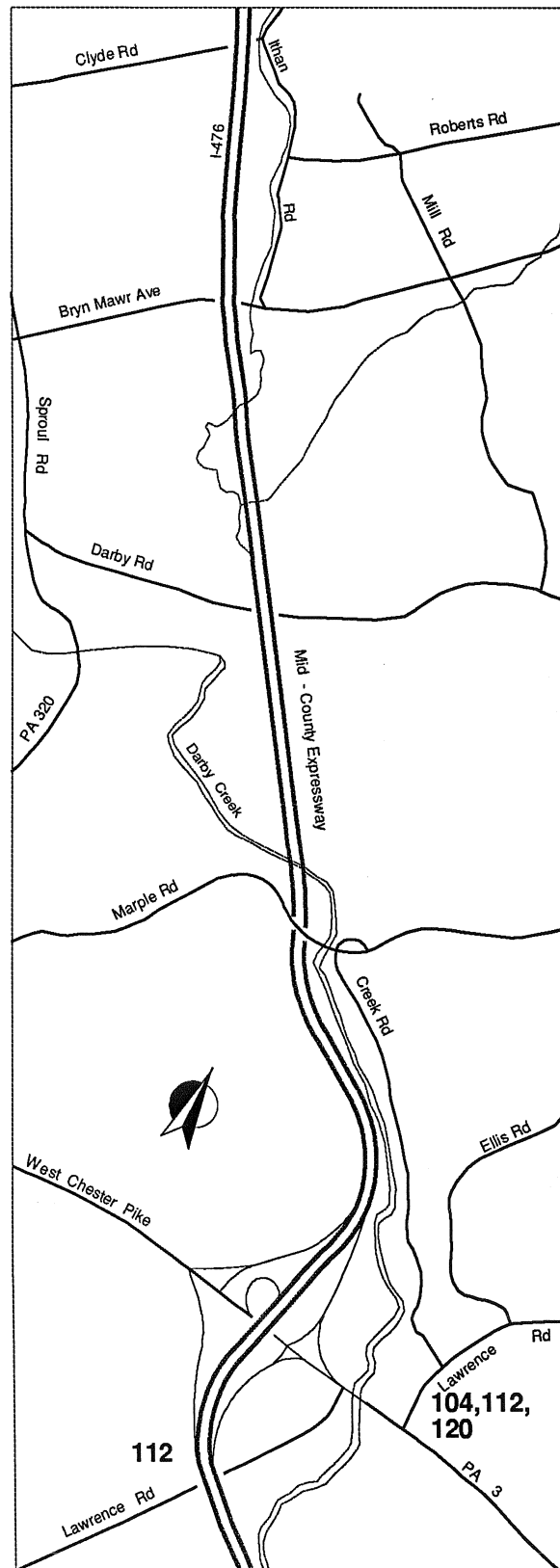
THE MID-COUNTY EXPRESSWAY

Map 4

Transit and Traffic Data

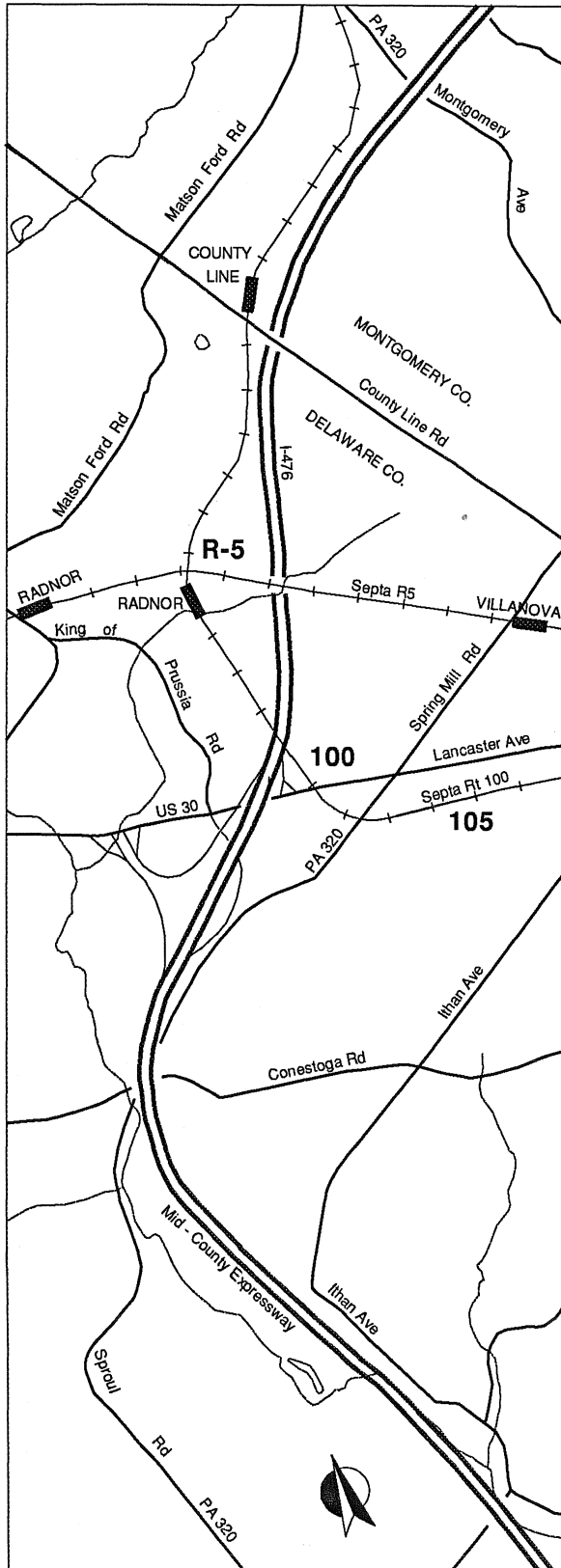
Scale: 1" = 2000'

- 104** 69th Street to West Chester
Crosses over I-476 at West Chester Pike PA 3
- 112** 69th Street to Delaware County Community Coll.
Crosses over I-476 at Lawrence Road and West Chester Pike PA 3
- 120** 69th Street to Cheyney University
Crosses over I-476 at West Chester Pike PA 3





THE MID-COUNTY EXPRESSWAY



Map 5

Transit and Traffic Data

Scale: 1" = 2000'

- 105** 69th Street to Paoli
Crosses under I-476 at US 30
- R-5** Regional Rail to Downingtown Amtrak to Harrisburg
and points west
Crosses over I-476 north of US 30
- 100** Light rail from 69th Street to Norristown
Crosses under I-476 north of US 30





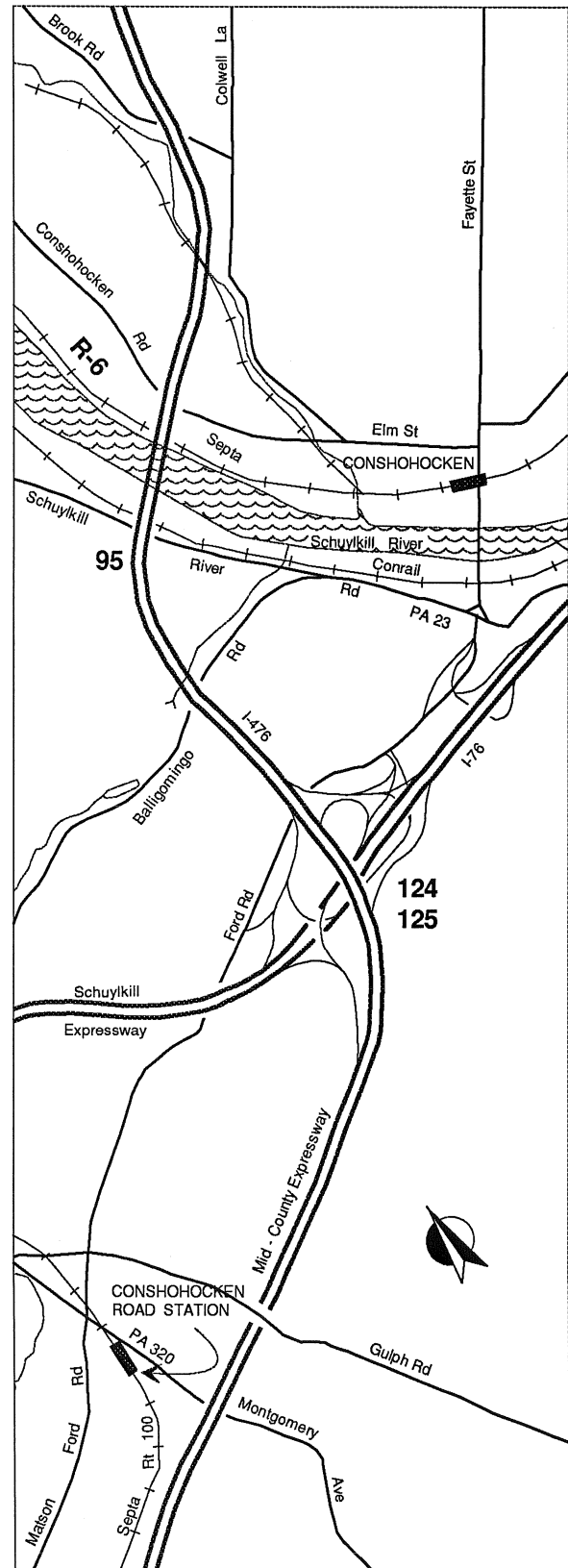
THE MID-COUNTY EXPRESSWAY

Map 6

Transit and Traffic Data

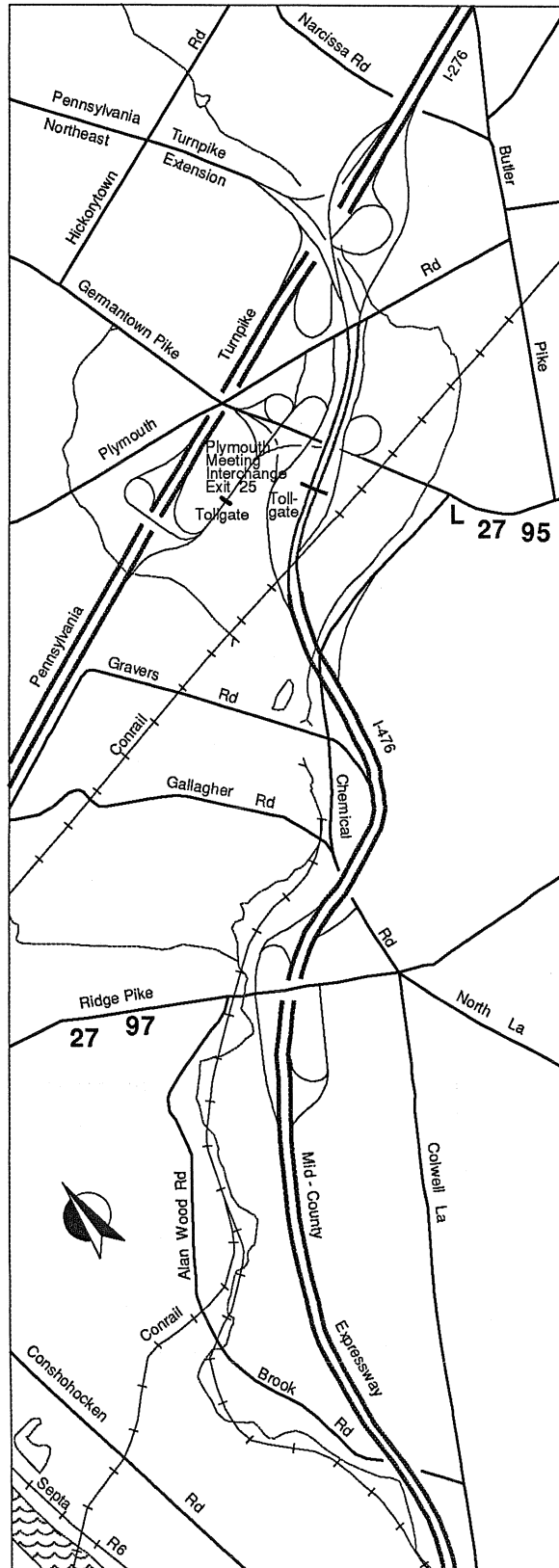
Scale: 1" = 2000'

- 124** Center City to King of Prussia
Crosses under I-476 at I-76
- 125** Center City to King of Prussia
Crosses under I-476 at I-76
- 95** Plymouth Meeting Mall to King of Prussia
Crosses under I-476 at River Road PA 23
- R-6** Regional rail to Norristown
Crosses under I-476 north of the Schuylkill River





THE MID-COUNTY EXPRESSWAY



Map 7
Transit and Traffic Data

Scale: 1" = 2000'

- 27** Center City to Plymouth Meeting Mall
crosses over I-476 at Germantown Pike
- 97** Penn Square to Spring Mill
Crosses over I-476 at Ridge Pike
- L** Olney to Plymouth Meeting Mall
Crosses under I-476 at Germantown Pike
- 95** King of Prussia to Plymouth Meeting Mall
Crosses under I-476 at Germantown Pike



SECTION FIVE

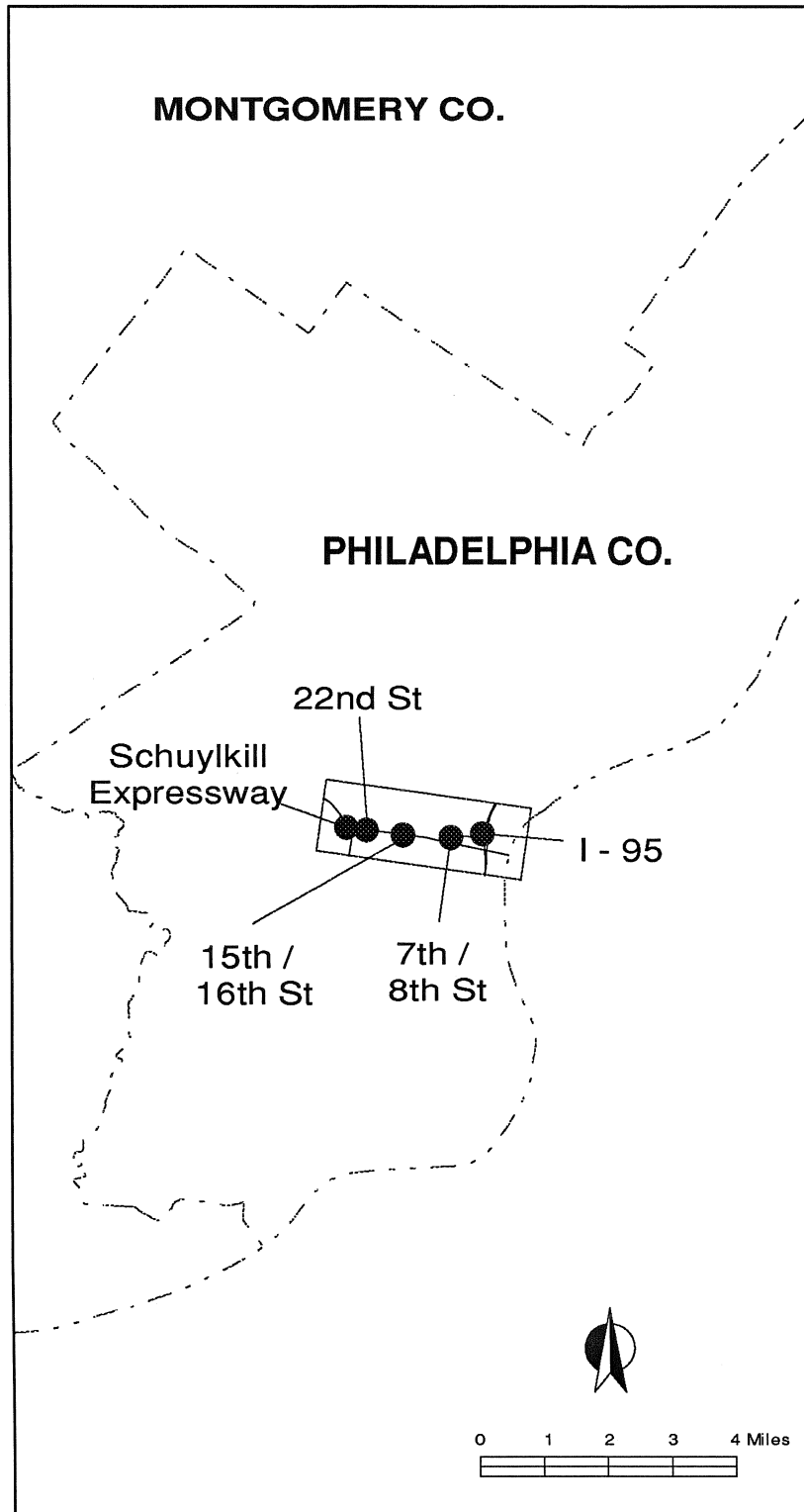


THE VINE EXPRESSWAY



THE VINE EXPRESSWAY

Figure 6



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White Paper

DESIGN FEATURES

This series of maps inventories the current physical attributes of the Vine Expressway. Also highlighted are physical characteristics which might limit improvements to the roadway. It should be noted that for most of its length, this road is below grade-level, surrounded by retaining walls. Travel lanes and entrance ramps are 12' in width.

Cross streets are shown on this map instead of segment/offset postings in order to more clearly show location of bridges. Information about the road shown in each map is located underneath the title block.

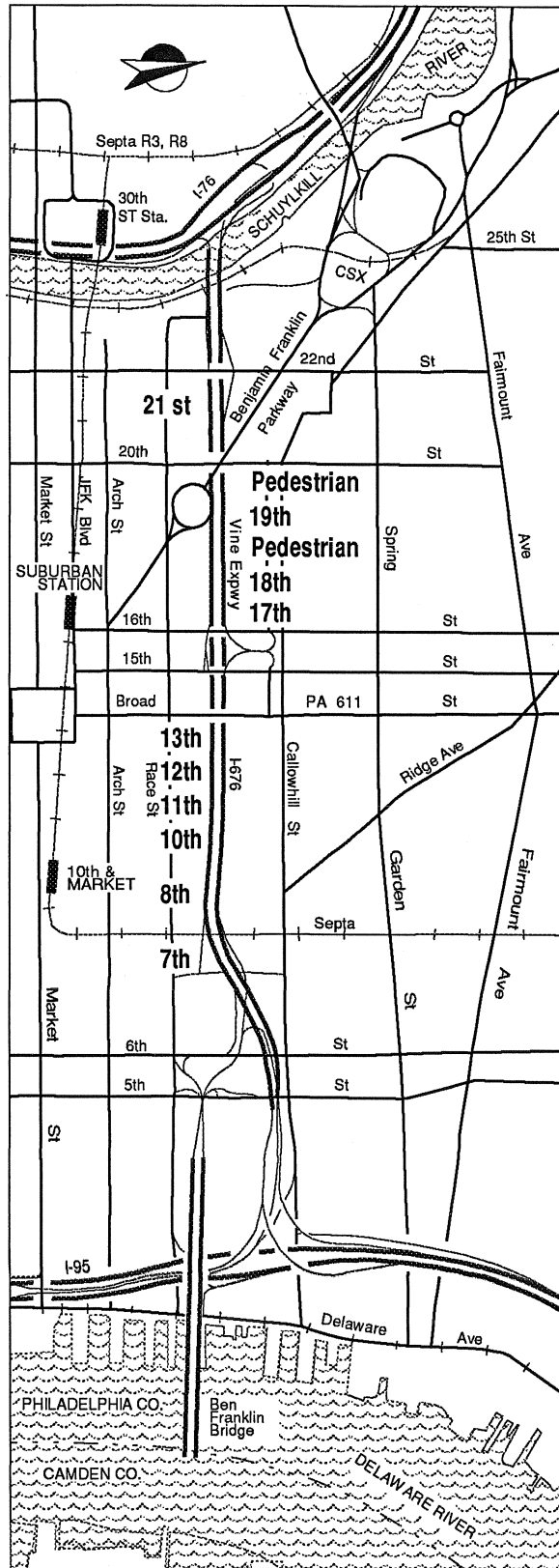
Specific information regarding structures is provided where available. Citations are made for either bridges or overpasses. For the bridge over 8th Street, structure number, span length, and deck width are given. For structures overpassing the Vine Expressway, the distance between piers is given in exact measurements (where available) or is listed according the location of the piers. Structure number and description of facility carried are also given. Due to the Interstate status of this highway, there are no sub-standard weight or clearance structures.

Information reported in this section was gathered from a combination of construction drawings, PennDOT Bridge Unit's structure lists, and field observations.





THE VINE EXPRESSWAY



Map 1

Design Features

Scale: 1" = 2000'

Note: Two Eastbound and Two Westbound travel lanes with various access and egress ramps. Shoulder width varies.

Bridge

over Schuylkill River, CSX,
and I-676 ramp
S# N/A
Span Length: N/A
Deck Width: N/A

Overpass

22nd Street
S# N/A
Distance Between Piers: 59', 47"

Overpass

21st Street
S# N/A
Distance Between Piers: N/A

Overpass

20th Street
and Benjamin Franklin Parkway
S# N/A
Distance Between Piers: N/A

Pedestrian Overpass

Between 19th and 20th Streets
S# N/A
Distance Between Piers: 96'-11"

Overpass

19th Street
S# N/A
Distance Between Piers: 96'-11"

Pedestrian Overpass

Between 18th and 19th Streets
S# N/A
Distance Between Piers: N/A

Overpass

18th Street
S# N/A
Distance Between Piers: 97'

Overpass

17th Street
S# 15812
Distance Between Piers: 121'-6"

Overpass

16th Street
S# 15813
Distance Between Piers: 120'-10"

Overpass

15th Street
S# 15814
Distance Between Piers: 122'-7"

Overpass

Broad Street
S# 15816
Distance Between Piers: 87'-7"

Overpass

13th Street
S# 15841
Distance Between Piers: 110'-10"

Overpass

12th Street
S# 15842
Distance Between Piers: 94'-5"

Overpass

11th Street
S# 15843
Distance Between Piers: 91'-8"

Overpass

10th Street
S# 15844
Distance Between Piers: 99'-3"

Bridge

over 8th Street S# 15902
Span Length: 521'-7"
Deck Width: 75'-6"



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ADJACENT LAND USE

The adjacent land use map identifies the land use on parcels immediately adjacent to the Vine Expressway's right of way. The purpose of this inventory is to highlight land use considerations which may impact, positively or negatively, upon large-scale roadway improvements.

Nine different land use categories are used to designate adjacent land uses. These categories are general in nature so as to facilitate their use, but sufficiently specific to allow for a meaningful designation of land use. The nine categories are:

- Agricultural
- Cemetery
- Commercial/Industrial
- Institutional
- Parkland
- Residential
- Transportation
- Vacant (parcels over 10 acres identified as such)
- Water

Adjacent land uses are identified on the map along both sides of the highway. The land use noted at the top of the map extends towards the bottom of the page until a different land use is cited under a horizontal line.

Special conditions and anecdotal information that may provide additional insight into the nature of adjacent parcels is provided where appropriate. The presence of special structures, including the proper names of identifiable places located adjacent to the roadway is also noted.

Information presented on this map was assembled from DVRPC in-house data, including aerial photographs, and field views.



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Paper

PLANNED IMPROVEMENTS

This type of map summarizes transportation improvements to the Vine Expressway corridor which are either underway or programmed. Relevant improvements are also shown for parallel and intersecting roadways. Projects which are not yet funded are not included in this listing.

Each project can be located on the map by referencing its TIP number, which is placed as closely as possible to actual location. When a project entails a corridor, it is referenced at one point only.

Two official program numbers, the Transportation Improvement Program (TIP; source: DVRPC) number and the Program Management System (PMS; source: Penn DOT) number are provided for reference purposes. In the few cases where a TIP number is not specified, federal funding is not involved and the project is therefore not on the TIP. Cost and estimated let and completion dates are subject to change.

Each project is catalogued according to the following format:

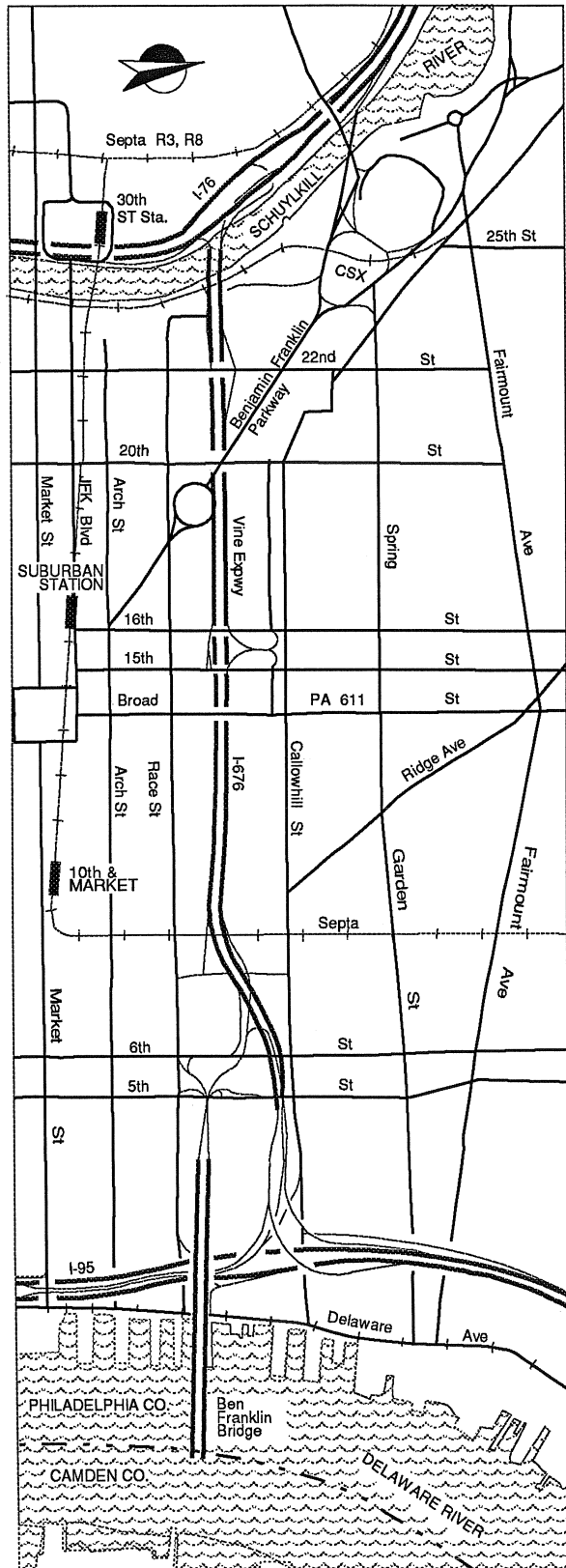
- Project Description
- Project Location
- Limits of Project (if necessary)
- Special Comments (related projects, funding, etc.)
- Transportation Improvement Program and Program Management System Numbers
- Current Estimated Cost
- Actual or Estimated Let Date
- Estimated Completion Date

It should be noted that the Vine Expressway was opened to traffic in 1991, and there are, accordingly, no planned improvements at this time.

Information was gathered for this section from DVRPC's Transportation Improvement Program, the City of Philadelphia Streets Department, and the PennDOT 12-year plan.



THE VINE EXPRESSWAY



Map 1

Planned Improvements

Scale: 1" = 2000'

NO PROJECTS



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NATURAL AND CULTURAL ENVIRONMENT

The natural and cultural environment map identifies noteworthy features located within close proximity to the roadway. Natural features displayed on the map include wetlands and flood plains. Cultural features are primarily historic.

Most of the wetlands information, including location and general configuration, was derived from the National Wetlands Inventory (NWI) Maps, issued by the United States Department of the Interior. Wetlands are identified by a pattern of horizontal lines. No distinction is made between the various classifications of wetlands (such as estuarine, palustrine, riverine, marine, or lacustrine).

It should be noted that the NWI maps are compiled on a very large scale and are therefore very general. An indication of wetlands on an NWI map suggests that wetlands are probably present to some extent on that particular site. Conversely, an indication that wetlands are not present suggests that the site is probably free of wetlands. However, any specific site which is proposed for development must be surveyed individually before a final determination can be made as to whether or not wetlands are actually present.

The flood plains delineated on the map represent the 100-year flood plain boundaries as identified on the Flood Insurance Rate Maps distributed by the Federal Emergency Management Agency. Flood plain areas are indicated by a pattern of vertical lines. Areas where flood plains and wetlands overlap are indicated by intersecting vertical and horizontal lines. Wetlands which are actually bodies of standing water are already depicted on the map.

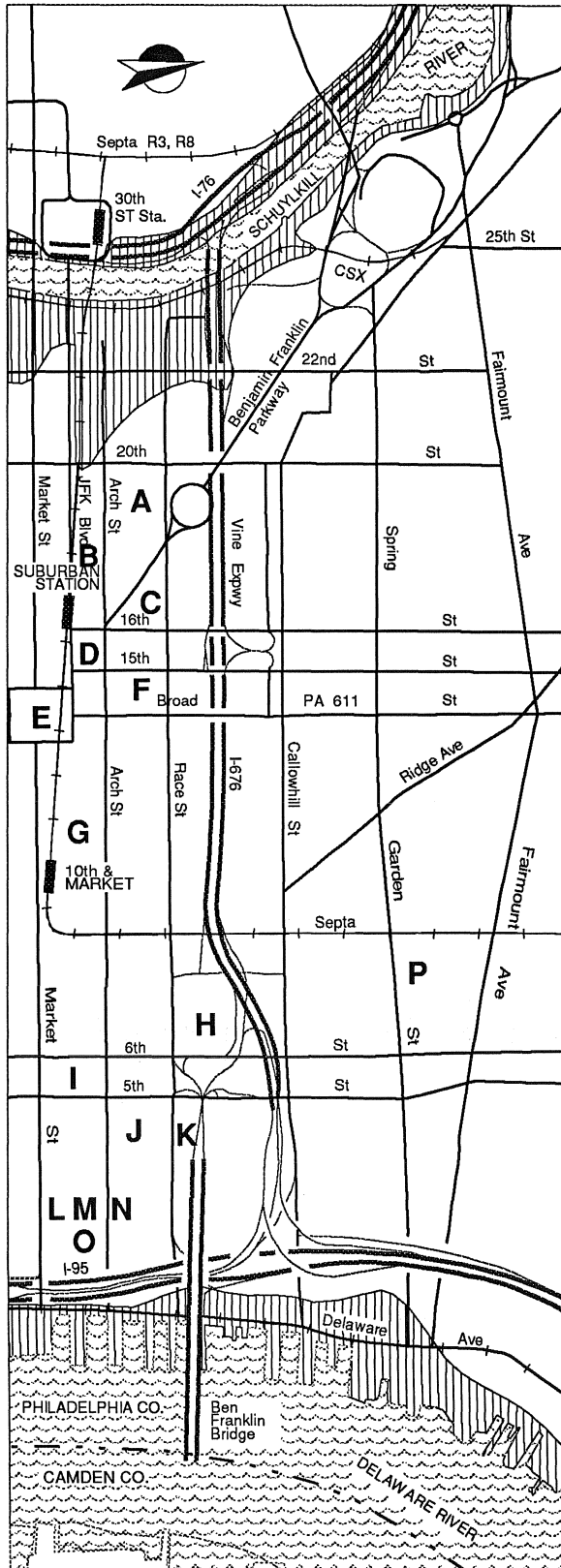
Historical features are also identified. These features include sites, structures, or districts which are significant in terms of American history, architecture, or culture. The general locations of the features are indicated on the map by large, upper-case letters. The site name, the year built, a general description and the type of ownership are provided in the margin text. Sites which are listed on the national Register of Historic Places are identified as such.

Other sites or buildings which have been determined to be eligible for the National Register are also identified. All public agencies are required to safeguard properties on the National Register as well as those which are or may be eligible for it.

Information for this section was gathered from DVRPC reports and field observations.



THE VINE EXPRESSWAY



Map 1

Natural and Cultural Environment

Scale: 1" = 2000'

Horizontal pattern: wetlands

Vertical pattern: 100 - year flood plain

- A St. Clement's Episcopal Church** 20th and Cherry. Built 1859. Church.
- B Arch Street Presbyterian Church** 18th and Arch. Built 1850 to 1855. Church.
- C SS Peter and Paul Cathedral** 18th and Race. Built 1846 to 1864. Church.
- D Masonic Temple** Broad and Filbert. Built 1868 to 1873. Masons.
- E Philadelphia City Hall** Broad and Market. Built 1871 to 1901. City of Philadelphia.
- F Pennsylvania Academy of the Fine Arts** Broad and Cherry. Built 1870's. Academy of the Fine Arts.
- G Arch Street Methodist Church** Broad and Arch. Built 1860's. Church.
- H St. Michael's (Zion) Lutheran Church** West side of Franklin Square. Church.
- I Free Quakers Meeting House** 5th and Arch. Built 1783. Commonwealth of Pennsylvania.
- J Christ Church Burial Ground** 5th and Arch. In existence since 1719 Church.
- K St George's Methodist Church** 235 North 4th Street. Built 1763. Church.
- L Christ Church** 2nd Street north of Market. Present building (National Shrine) built 1727. Church.
- M Arch Street Friends Meetinghouse** Arch Street between 3rd and 4th Streets. Built 1804. Church.
- N Betsy Ross House** 239 Arch Street. Built 1740. City of Philadelphia.
- O Elfret's Alley** North of Arch between Front and Second Sts. 18th century houses. Private.
- P Edgar Allen Poe House** 530 N. 7th Street. Built circa 1830. Listed on the National Register. Private.



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TRANSIT AND TRAFFIC DATA

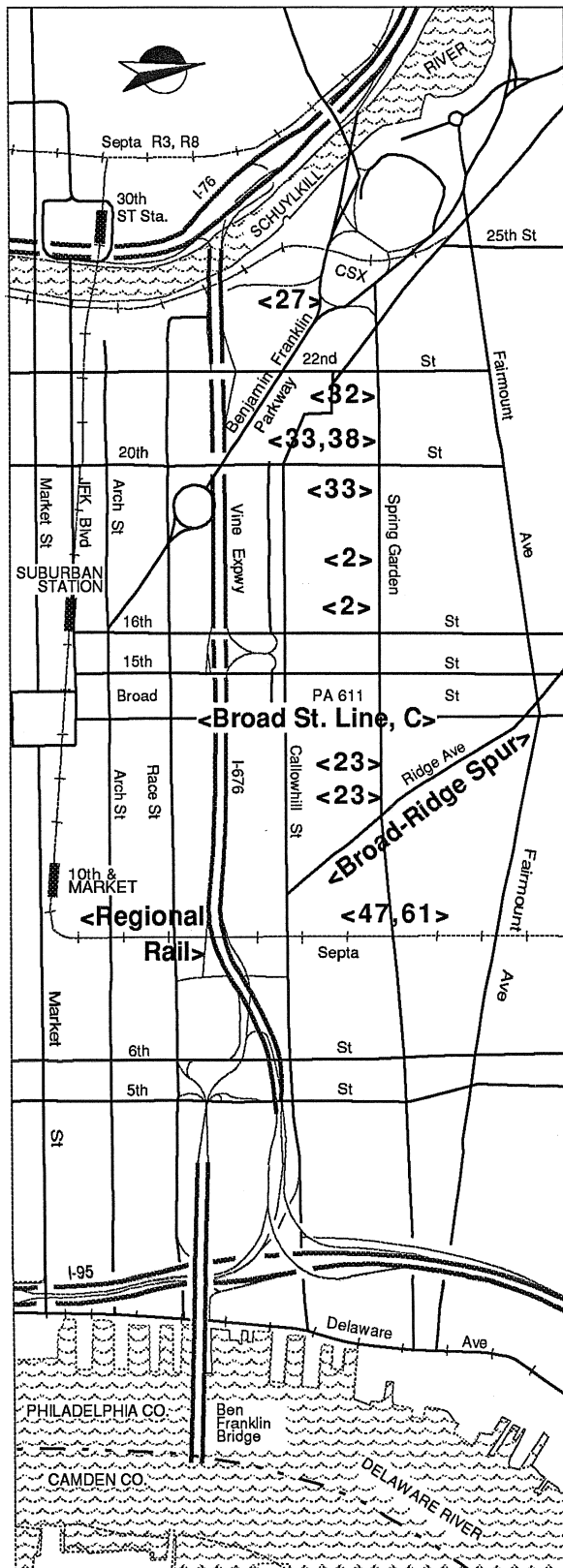
This map illustrates those transit routes which either cross or travel on the Vine Expressway. Reference is made in the accompanying text to the route number, the origin and destination points, the road it travels at the point of intersection, and whether the crossing is over or under I-676. All points of intersection are shown.

Given that this road was only recently opened to traffic, actual Average Annual Daily Traffic (AADT) counts are not available at this time.

Information for this section was provided by Southeastern Pennsylvania Transportation Authority.



THE VINE EXPRESSWAY



Map 1

Transit and Traffic Data

Scale: 1" = 2000'

Note: 1990 AADT: 80,423

- 2** Nicetown to South Philadelphia
Crosses over at 16th and 17th Streets
- 23** Chestnut Hill to South Philadelphia
Crosses over at 11th and 12th Streets
- 27** Plymouth Meeting Mall to Center City
via Vine Street Expressway
- 32** Andorra to Center City
Crosses over at Benjamin Franklin Parkway
and at 21st Street
- 33** Tioga to Penn's Landing
Crosses over at 19th and 20th Streets
- 38** East Falls to Independence Mall
crosses over at 20th Street
- 47** Olney to South Philadelphia
Crosses under at 8th Street
- 48** Tioga to Penn's Landing
Crosses over at 21st and 23rd Streets
- 61** Manayunk to Center City
Crosses over at 11th Street and under
at 8th Street

Regional Rail (all Routes)
Commuter Tunnel
Crosses under west of 8th Street

Broad Street Line
Broad Street Subway
Crosses under at Broad Street

- C** West Oak Lane to Center City
Crosses over at Broad Street

Broad Street Line
Broad-Ridge Spur
Crosses under east of 12th Street

